

CITY OF ARCATA

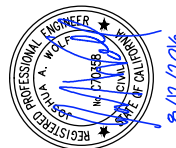
HUMBOLDT BAY TRAIL NORTH PROJECT

FEDERAL PROJECT NUMBER ATPL-5021(020)

AUGUST 2016



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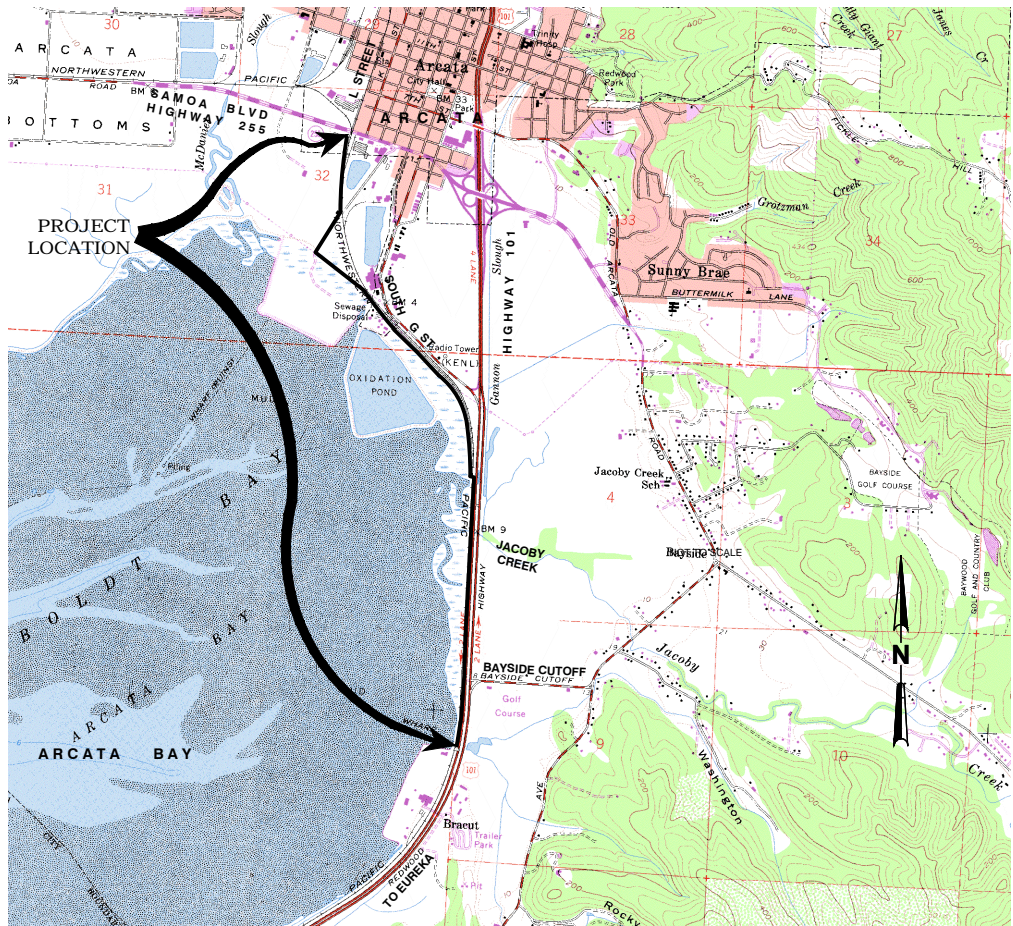
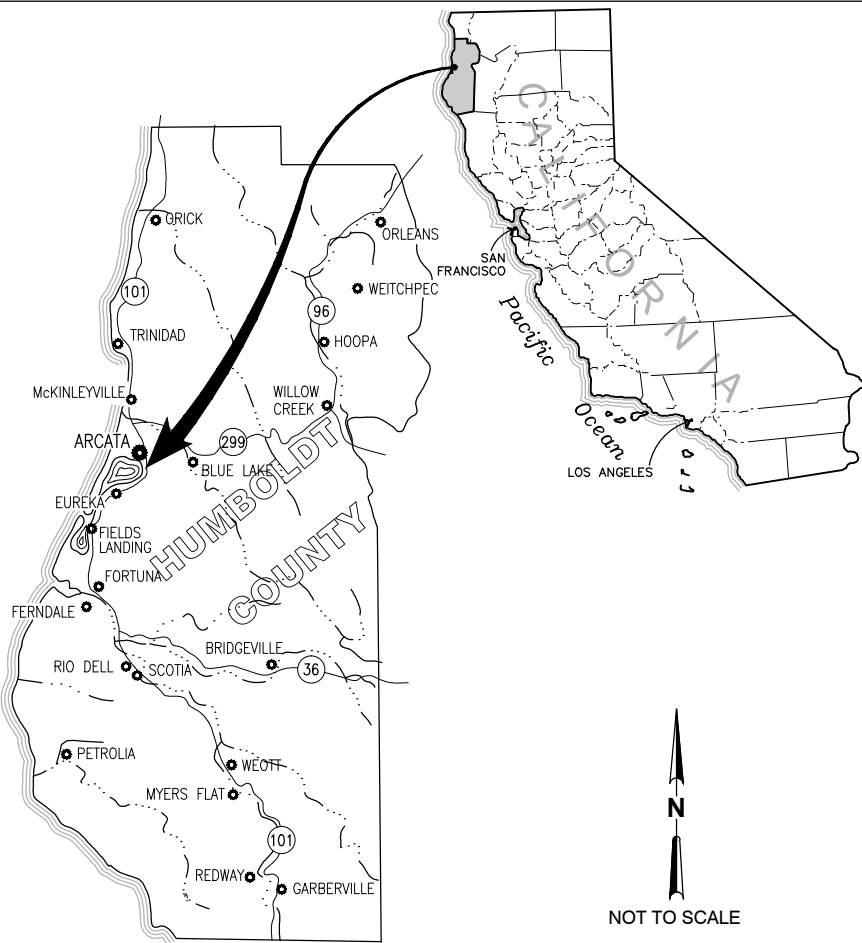


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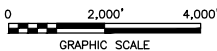
AREA MAP

LOCATION MAP

APPROVALS



SOURCE: USGS QUADRANGLE MAP



CITY OF ARCATA
ASSISTANT CITY ENGINEER:
NETRA KHATRI, PE

Netra Khatri
8/30/16

CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH

GENERAL
COVER SHEET

PROJ NO: 8411982
DRWN: OFG CHKD: JW

G-1.0

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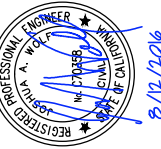
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8/12/2016

ORIGINAL DRAWING

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**CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH**

GENERAL DRAWING INDEX

PROJ NO: 8411982

DRWN: OFG

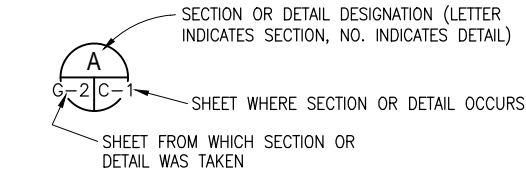
CHKD: JW

G-2.0

SHEET 2 OF 52

SYMBOLS:

GENERAL:	
	CENTERLINE
	DIAMETER
	APPROXIMATE
	AT



TOPOGRAPHIC:

NEW	EXISTING	
		LIMITS OF DISTURBANCE
		PROPERTY LINE (APPROXIMATE)
		RIGHT OF WAY LINE (APPROXIMATE)
		FENCE
		ROAD/TRAIL CENTERLINE
		AC PAVEMENT
		ROADSIDE GUARDRAIL
		RAILROAD TRACKS
		CONTOUR LINE
		LIMITS OF EARTHWORK CUT/FILL
		TOP OF BANK
		TOE OF BANK
		DRIVEWAY
		CONCRETE SLAB
		DRAINAGE/CREEK /DITCH/SWALE
		BRUSH /VEGETATION LINE
		TREE
		WET AREA / WETLAND
		TEMPORARY BENCH MARK
		SURVEY MONUMENT
		ROAD SIGN
		MONITORING WELL

UTILITY:

NEW	EXISTING	
		UNKNOWN UTILITY
		WATER MAIN
		STORM DRAIN/CULVERT
		SANITARY SEWER MAIN
		SANITARY SEWER FORCE MAIN
		UNDERGROUND ELECT CONDUIT
		OVERHEAD ELECTRICAL LINE
		OVERHEAD ELECTRICAL & TELEPHONE
		CABLE TELEVISION (COMM) LINE
		GAS MAIN

	SS MANHOLE
	SS CLEANOUT
	FIRE HYDRANT
	GATE VALVE
	BLOW-OFF ASSEMBLY
	WATER METER
	GAS VALVE
	GAS METER
	STORM DRAINAGE MANHOLE
	STORM DRAINAGE INLET
	UTILITY POLE (PP=POWER), (TP=TELEPHONE), (JP=JOINT POLE)
	GUY POLE
	ELECTRICAL MANHOLE
	LIGHT POLE
	PULL BOX
	ELECT METER BOX
	TRANSFORMER PAD
	TELE-COMM MANHOLE
	ELECTRICAL PANEL

ABBREVIATIONS:

AB	ANCHOR BOLT	FP	FIRE PROTECTION	PVC	POLYVINYL CHLORIDE PLASTIC PIPE
AB	AGGREGATE BASE	FS	FINISHED SURFACE	PVI	POINT OF VERTICAL INTERSECT
AC	ASPHALT CONCRETE	FT	FOOT OR FEET		
AGG	AGGREGATE	FTG	FOOTING	R, RAD	RADIUS
APE	AREA OF POTENTIAL EFFECT			RC	RELATIVE COMPACTION
ARV	AIR-VACUUM RELEASE VALVE	G	GAS LINE	RCP	REINFORCED CONCRETE PIPE
ASS'Y.	ASSEMBLY	GAL	GALLON	RD	ROAD
AVE	AVENUE	GALV	GALVANIZED	RDWD	REDWOOD
AWWA	AMERICAN WATER WORKS ASSOCIATION	GB	GRADE BREAK	REQ'D	REQUIRED
		GR	GRADE	REQ'T	REQUIREMENT
		GRD	GROUND	RPP	REDUCED PRESSURE PRINCIPAL
		GV	GATE VALVE	RR	RAILROAD
				RSP	ROCK SLOPE PROTECTION
B-	BORING			RT	RIGHT
BC	BEGIN CURVE	HB	HOSE BIBB	R/W	RIGHT-OF-WAY
BFP	BACK FLOW PREVENTER	HORZ	HORIZONTAL		
BM	BENCH MARK	HPS	HIGH PRESSURE SODIUM		
BLDG	BUILDING	HWY	HIGHWAY	S	SLOPE
BLVD	BOULEVARD	HYD	HYDRANT	SAT	SATURATED
BO	BLOW OFF			SCH,	SCHEDULE
BOT	BOTTOM	IP	IRON PIPE	SCH,	SCHEDULE
BVC	BEGIN VERTICAL CURVE	INV	INVERT	SD	STORM DRAIN
BVCE	BEGIN VERTICAL CURVE ELEVATION			SDMH	STORM DRAIN MANHOLE
BVCS	BEGIN VERTICAL CURVE STATION	JCT	JUNCTION	SHT	SHEET
		JP	JOINT UTILITY POLE	SIM	SIMILAR
				SO	SOUTH
C	CONDUIT	K	RATE OF CURVATURE	SS	SANITARY SEWER
CITY	CITY OF ARCATA			S STL	STAINLESS STEEL
CL,	CENTERLINE	L	LENGTH	STA	STATION
CLR	CLEAR, CLEARANCE	LAT	LATERAL	STD	STANDARD
CO	CLEANOUT	LS	LIFT STATION	STL	STEEL
CMP	CORRUGATED METAL PIPE	LT	LEFT		
CMU	CONCRETE MASONRY UNIT	LVC	LENGTH VERTICAL CURVE	T	TANGENT
CONC	CONCRETE			T, TEL	TELEPHONE
CONT	CONTINUOUS	MAX	MAXIMUM	TC	TOP OF CURB
CONT'D	CONTINUED	M	METER	THK	THICK
COORD	COORDINATE	MFR	MANUFACTURER	TG	TOP GRATE
COR	CORNER	MH	MAN HOLE	TP	TEST PIT
CU	CUBIC	MIN	MINIMUM	TS	TOP OF SLAB
CV	CHECK VALVE	MISC	MISCELLANEOUS	TW	TOP OF WALK
		MG	MILLION GALLON	TYP	TYPICAL
d	PENNY (NAIL SIZE)			UBC	UNIFORM BUILDING CODE
DIA,	DIAMETER	N	NORTH	UNO	UNLESS OTHERWISE NOTED
DTL	DETAIL	(N)	NEW		
DI	DROP (DRAINAGE) INLET	NCRA	NORTH COAST RAILROAD AUTHORITY	V	VOLT(S)
DR	DRIVE	NIC	NOT IN CONTRACT	VERT	VERTICAL
DW	DOMESTIC WATER LINE	NO	NUMBER		
DWG	DRAWING	NTS	NOT TO SCALE	W/ W WD	WITH WATER WIDE
(E)	EXISTING			XING	CROSSING
E	EAST	OC	ON CENTERS		
EA	EACH	OPNG	OPENING	YD	YARD
EC	END CURVE				
EF	EACH FACE	PC	POINT OF CURVE		
EP	EDGE PAVING	PCC	PORTLAND CONCRETE CEMENT		
EQ	EQUAL	PE	POLYETHYLENE		
ER	EDGE ROAD	PI	POINT OF INTERSECTION		
EL/ELEV	ELEVATION	PL	PLATE		
ELEC	ELECTRIC	P/L,	PROPERTY LINE		
ENGR	ENGINEER	PLCS	PLACES		
ETW	EDGE OF TRAVELED WAY	PLWD	PLYWOOD		
EVC	END VERTICAL CURVE	POC	POINT OF CONNECTION		
EVCE	END VERTICAL CURVE ELEVATION	PP	POWER POLE		
EVCS	END VERTICAL CURVE STATION	PRC	POINT OF REVERSE CURVE		
EW	EACH WAY	PRV	PRESSURE REDUCING VALVE		
		PSI	POUNDS PER SQUARE INCH		
FIN	FINISH	PT	POINT		
FF	FINISH FLOOR	PT	POINT OF TANGENT BEGINING		
FH	FIRE HYDRANT				
FL,	FLOW LINE				
FLR	FLOOR				

NOTE: CONTACT ENGINEER FOR ABBREVIATIONS NOT LISTED.

CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH

GENERAL
GENERAL SYMBOLS
AND ABBREVIATIONS

PROJ NO: 8411982
DRWN: OFG CHKD: JW

G-3.0

SHEET 3 OF 52

1. ALL WORKMANSHIP MATERIALS SHALL CONFORM TO STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION (CALTRANS) STANDARD PLANS & SPECIFICATIONS, AND THE CONTRACT SPECIAL PROVISIONS FOR THIS PROJECT, UNLESS SHOWN OTHERWISE.
2. PROJECT REQUIRES A CLASS A GENERAL ENGINEERING CONTRACTOR'S LICENSE IN THE STATE OF CALIFORNIA.
3. QUANTITIES OF ITEMS, LENGTH OF PROJECT, AND SITE CONDITIONS SHOWN IN THE PLANS ARE APPROXIMATE. ALL MATERIALS SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR UNLESS OTHERWISE NOTED.
4. CONTRACTOR AGREES TO ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR THE JOB SITE DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY; THAT THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS; AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY, AND HOLD THE CITY, GHD, NCRA, US FISH & WILDLIFE AND THEIR REPRESENTATIVES HARMLESS FROM ANY AND ALL LIABILITY, REAL AND/OR ALLEGED, IN CONJUNCTION WITH THE PERFORMANCE OF THIS PROJECT.
5. CONTRACTOR SHALL BE HELD RESPONSIBLE FOR ANY AND ALL DAMAGES TO EXISTING STRUCTURES, ROADS, RAILROAD TRACK AND TRACK BED, AND UTILITIES DURING CONSTRUCTION. ALL DAMAGE SHALL BE RESTORED TO EQUAL OR BETTER CONDITION AT THE CONTRACTOR'S EXPENSE.
6. CONTRACTOR SHALL VERIFY LOCATIONS, ELEVATIONS, DISTANCES, AND FEATURES THAT MAY AFFECT THE WORK. SHOULD EXISTING CONDITIONS DIFFER FROM THOSE SHOWN OR INDICATED, OR IF IT APPEARS THAT THESE PLANS, STANDARD SPECIFICATIONS, AND SPECIAL PROVISIONS DO NOT ADEQUATELY DETAIL THE WORK TO BE DONE, CONTRACTOR SHALL NOTIFY THE CITY PRIOR TO CONTINUING WITH ANY RELATED WORK. NO ALLOWANCE WILL BE MADE ON HIS BEHALF FOR ANY EXTRA EXPENSE RESULTING FROM FAILURE OR NEGLECT IN DETERMINING THE CONDITIONS UNDER WHICH WORK IS TO BE PERFORMED. NOTED DIMENSIONS TAKE PRECEDENCE OVER SCALE.
7. A SET OF SIGNED WORKING DRAWINGS AND A SET OF SPECIFICATIONS WILL BE KEPT AT ALL TIMES AT THE JOB SITE ON WHICH ALL CHANGES OR VARIATIONS IN THE WORK, INCLUDING ALL EXISTING UTILITIES, ARE TO BE RECORDED AND/OR CORRECTED DAILY AND SUBMITTED TO THE CITY WHEN THE WORK TO BE DONE IS COMPLETED.
8. CONTRACTOR SHALL PROVIDE AND MAINTAIN SUFFICIENT TEMPORARY BARRIERS TO PROVIDE FOR THE SAFETY OF THE STAFF AND PUBLIC TO THE SATISFACTION OF THE CITY, CALTRANS AND NCRA.
9. SHOULD GRADING OPERATIONS ENCOUNTER HAZARDOUS MATERIALS, OR WHAT APPEAR TO BE HAZARDOUS MATERIALS, STOP WORK IN THE AFFECTED AREA IMMEDIATELY AND CONTACT 911 OR THE APPROPRIATE AGENCY FOR FURTHER INSTRUCTION. IF SAID MATERIALS ARE DISCOVERED ON RAILROAD RIGHT-OF-WAY, NCRA SHALL BE NOTIFIED.
10. CONTRACTOR SHALL NOTIFY THE CITY AT LEAST 72 HOURS IN ADVANCE OF COMMENCEMENT OF ANY PART OF THE WORK AND SHALL COORDINATE CONSTRUCTION SCHEDULE WITH THE CITY TO MINIMIZE IMPACT TO RESIDENTS. NCRA SHALL BE NOTIFIED AT LEAST 5 BUSINESS DAYS BEFORE THE START OF CONSTRUCTION.
11. UNSUITABLE EXCAVATED MATERIAL SHALL BE REMOVED FROM SITE AND DISPOSED OF IN A MANNER CONSISTENT WITH APPLICABLE REGULATIONS SUCH AS CITY OR COUNTY GRADING ORDINANCES. THE CONTRACTOR SHALL ASSUME FULL RESPONSIBILITY AND EXPENSE FOR PROPER DISPOSAL OF UNSUITABLE MATERIALS TAKEN FROM SITE AND PROVIDE SUITABLE DOCUMENTATION OF PERMISSION AND ENVIRONMENTAL DOCUMENTATION FOR USE OF ANY DISPOSAL SITE AS REQUESTED BY THE CITY.
12. THE DESIGN FEATURES INCLUDING HORIZONTAL AND VERTICAL ALIGNMENTS, TYPICAL SECTIONS, APPROACHES, AND OTHER DESIGN DETAILS SHOWN ON THESE DESIGN PLANS SHALL NOT BE ALTERED OR MODIFIED IN ANY WAY DURING CONSTRUCTION WITHOUT THE EXPRESSED, WRITTEN DIRECTION AND APPROVAL OF THE CITY AND/OR CONTRACTING OFFICER. DRAINAGE STRUCTURES SHALL BE INSTALLED AS SHOWN THE PLANS WITH ONLY MINOR CORRECTIONS IN LOCATION SKEW AND/OR ELEVATIONS AS NEEDED TO FIT FIELD CONDITIONS AS DETERMINED BY THE CITY'S REPRESENTATIVE.
13. THE CONTRACTOR SHALL READ AND MAKE CAREFUL EXAMINATION OF THE PLANS, SPECIFICATIONS, QUANTITIES AND MATERIAL ESTIMATES AND VISIT THE SITE OF THE PROPOSED CONSTRUCTION TO BECOME FAMILIAR WITH THE SITE CONDITIONS AND LIMITATIONS BEFORE MAKING A BID. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY AND ALL ERRORS RESULTING FROM THE FAILURE TO MAKE SUCH AN EXAMINATION. ANY INFORMATION DERIVED FROM THE MAPS, PLANS, SPECIFICATIONS, PROFILES, DRAWINGS OR FROM THE CITY WILL NOT RELIEVE THE CONTRACTOR FROM ANY RISK OR FROM FULFILLING THE TERMS OF THE CONTRACT.
14. NO WORK SHALL BE PERFORMED OUTSIDE OF THE DESIGNATED CONSTRUCTION LIMITS WITHOUT THE APPROVAL OF THE CITY'S REPRESENTATIVE.
15. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING STAGING AREAS WITH THE CITY OF ARCATA. ALL STAGING AREAS WITHIN THE RIGHT-OF-WAY SHALL BE PRE-APPROVED BY THE CITY. ANY PROPOSED STAGING AREA WITHIN RAILROAD RIGHT-OF-WAY SHALL BE APPROVED BY NCRA.
16. UPON COMPLETION OF THE CONSTRUCTION PROJECT, THE CONTRACTOR SHALL LEAVE THE PROJECT AREA FREE OF DEBRIS AND UNUSED MATERIAL. ALL DAMAGE CAUSED BY THE CONTRACTOR SHALL BE RESTORED TO AN "AS GOOD OR BETTER" CONDITION.
21. EXISTING FENCING SHALL REMAIN IN PLACE AND UNDISTURBED BY CONSTRUCTION OPERATIONS UNLESS OTHERWISE SHOWN ON THE PLANS. CONTRACTOR SHALL BE HELD RESPONSIBLE FOR ANY AND ALL DAMAGES TO FENCES DURING CONSTRUCTION. ALL DAMAGE SHALL SHALL BE RESTORED TO EQUAL OR BETTER CONDITION AT THE CONTRACTOR'S EXPENSE.
22. ALL WORK WITHIN NCRA RIGHT-OF-WAY SHALL FOLLOW NRCA'S BEST MANAGEMENT PRACTICES (BMP).
23. THE CONTRACTOR SHALL NOTIFY THE CITY A MINIMUM OF 72 HOURS IN ADVANCE OF ANY EXCAVATION THAT WILL EXTEND 2 FEET OR DEEPER, AND WILL NOT PROCEED WITH ANY EXCAVATION WORK UNTIL CLEARED TO DO SO BY THE CITY. A PROJECT ARCHAEOLOGIST MAY BE ON SITE DURING CONSTRUCTION ACTIVITIES. THE CONTRACTOR IS ADVISED THAT IF ANY ARCHAEOLOGICAL FINDINGS ARE DISCOVERED DURING CONSTRUCTION THAT THE MONITOR OR ARCHAEOLOGIST HAS THE AUTHORITY TO SLOW OR STOP CONSTRUCTION ACTIVITIES AS THEY DEEM NECESSARY.
24. EXISTING SIGN SYSTEMS SHALL BE PROTECTED THROUGHOUT CONSTRUCTION ACTIVITY UNLESS NOTED OTHERWISE ON PLANS. CONTRACTOR SHALL BE HELD RESPONSIBLE FOR ANY AND ALL DAMAGES TO SIGNS DURING CONSTRUCTION. ALL DAMAGE SHALL BE RESTORED TO EQUAL OR BETTER CONDITION AT THE CONTRACTOR'S EXPENSE.
25. STRUCTURAL EXCAVATION AND BACKFILL OF CULVERTS AND OTHER DRAINAGE STRUCTURES SHALL BE CONSIDERED INCIDENTAL TO INSTALLATION OF THE STRUCTURE. EXCESS MATERIAL REMOVED MAY BE USED TO REBUILD APPROACHES OR MAY BE PLACED ALONG ROADWAY/TRAIL SHOULDERS IN AREAS AS DIRECTED BY THE CITY'S REPRESENTATIVE.
26. CONTRACTOR SHALL OBTAIN ENCROACHMENT PERMITS FROM CALTRANS AND NCRA PRIOR TO BEGINNING WORK.

1. ALL PERMANENT AND TEMPORARY ROADSIDE SIGNS SHALL BE PLACED IN ACCORDANCE WITH THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND IN ACCORDANCE WITH THE DETAILS IN THESE PLANS. CONTRACTOR SHALL USE MUTCD FOR TRAFFIC CONTROL. SEE SPECIFICATIONS FOR OTHER TRAFFIC CONTROL REQUIREMENTS.
2. THE CONTRACTOR IS RESPONSIBLE FOR PREPARING AND SUBMITTING A TRAFFIC CONTROL PLAN IN ACCORDANCE WITH THESE DETAILS, THE CONTRACTOR'S SEQUENCING PLAN, MUTCD, AND THE PROJECT SPECIFICATIONS.
3. IF CONTRACTOR SUBMITS A TRAFFIC CONTROL PLAN OTHER THAN A STANDARD FROM MUTCD OR CALTRANS STANDARD PLAN, THEN THE TRAFFIC CONTROL PLAN MUST BE SIGNED BY A LICENSED ENGINEER.

1. THE CONTRACTOR IS RESPONSIBLE FOR FIELD VERIFICATION OF ALL SURVEY DATA. CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING THE LIMITS OF DISTURBANCE, LIMITS OF WETLAND IMPACTS (AS SHOWN ON THE PLANS), TRAIL ALIGNMENT AND GRADE, RIGHT-OF-WAY LINES, SLOPE EASEMENTS, AND ALL HORIZONTAL AND VERTICAL CONTROL PRIOR TO CONSTRUCTION.
2. CONTRACTOR IS RESPONSIBLE FOR CONSTRUCTION STAKING AND SHALL ARRANGE FOR STAKING WITH A CALIFORNIA LICENSED SURVEYOR. TWO-DIMENSIONAL COMPUTER AIDED DESIGN (CAD) FILES WILL BE AVAILABLE AND PROVIDED TO THE CONTRACTOR PRIOR TO CONSTRUCTION. THESE PRINTED PLANS ARE THE CONTRACT DOCUMENTS AND THE CONTRACTOR ASSUMES ALL LIABILITY FOR THE USE OF ANY ELECTRONIC FILES. STAKING WILL BE REVIEWED BY OWNER FOR CONFORMANCE TO DESIGN PRIOR TO CONSTRUCTION.

1. THE LOCATION OF UTILITIES SHOWN IN THE PLANS IS APPROXIMATE AND IS ONLY SHOWN TO ASSIST THE CONTRACTOR IN COMPLETING THE WORK. THE CONTRACTOR SHALL VERIFY ALL UTILITIES WITH OWNERS PRIOR TO CONSTRUCTION. ANY DAMAGE TO EXISTING UTILITIES BY THE CONTRACTOR SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE.
2. IF ANY WORK IS IN CLOSE PROXIMITY TO EXISTING UTILITIES AND COORDINATION IS REQUESTED BY THE UTILITY COMPANY, THE CONTRACTOR SHALL COORDINATE UTILITY WORK WITH THE RESPONSIBLE UTILITY COMPANY (UTILITY OWNER).
3. THE ACTUAL LOCATION OF EXISTING UTILITIES MAY VARY FROM THAT SHOWN ON THE PLANS. EXACT LOCATION AND COMPLETENESS ARE NOT GUARANTEED. CONTRACTOR SHALL POTHOLE AND LOCATE ALL EXISTING UTILITIES VARIATIONS IN LOCATION AND DEPTH SHALL BE BROUGHT TO THE ATTENTION OF THE CITY IMMEDIATELY SO THAT THE LOCATION OF UTILITIES MAY BE CHECKED WITH THE PROPOSED DESIGN. CONTRACTOR SHALL NOTIFY UNDERGROUND SERVICES ALERT (800) 227-2600 A MIN. OF 48 HOURS PRIOR TO ANY EXCAVATION AND POTHOLE FOR EXACT LOCATION.
4. ALL LOCATIONS FOR WORK SHALL BE CHECKED AND COORDINATED WITH EXISTING CONDITIONS IN THE FIELD BEFORE BEGINNING CONSTRUCTION.
5. ALL EXISTING UTILITIES AND TIE-IN POINTS SHOULD BE CONSIDERED ACTIVE UTILITIES UNLESS OTHERWISE INDICATED.
6. CONSTRUCTION ACTIVITY WILL TAKE PLACE IN THE VICINITY OF OVERHEAD ELECTRIC TRANSMISSION LINES. IT IS THE CONTRACTOR'S RESPONSIBILITY TO BE AWARE OF, AND OBSERVE, THE MINIMUM CLEARANCES FOR WORKERS AND EQUIPMENT OPERATING NEAR HIGH VOLTAGE ELECTRIC LINES AS SET OUT IN THE HIGH VOLTAGE SAFETY ORDERS OF THE CALIFORNIA DIVISION OF INDUSTRIAL SAFETY AS WELL AS OTHER APPLICABLE SAFETY REGULATIONS.

1. CONTRACTOR SHALL BE RESPONSIBLE FOR THE SAFETY OF ALL CONSTRUCTION. ADEQUATE SHORING BRACING, TIES, AND SUPPORTS SHALL BE USED TO PROVIDE PROPER TEMPORARY INTEGRITY DURING ALL PHASES OF CONSTRUCTION.
2. ALL EXISTING LANDSCAPED AND UNPAVED AREAS WHICH ARE DISTURBED BY CONSTRUCTION OR EARTHWORK OPERATIONS SHALL BE HAND RAKED SMOOTH AND RETURNED TO ORIGINAL EXISTING CONDITIONS.
3. ALL DITCHES, SWALES, GUTTERS, ETC. SHOULD BE CONSIDERED ACTIVE STORM CONVEYANCES UNLESS OTHERWISE INDICATED. CONTRACTOR IS RESPONSIBLE FOR ADDRESSING STORM WATER DRAINAGE AND DEWATERING OF WORK AREAS DURING CONSTRUCTION.

1. CONTRACTOR IS RESPONSIBLE FOR PREPARING THE SWPPP AND FOR EROSION AND SEDIMENT CONTROL. CONTRACTOR SHALL USE CALTRANS STORMWATER QUALITY HANDBOOKS, CONSTRUCTION CONTRACTOR'S GUIDE AND SPECIFICATIONS. CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH THE PROVISIONS IN THE SWPPP AND EROSION CONTROL PLAN.
2. DURING WET WEATHER PERIODS, CONTRACTOR IS RESPONSIBLE FOR SEQUENCING CONSTRUCTION IN A MANNER TO MINIMIZE IMPACT ON OPEN EARTHWORK AND COMPACTION OPERATIONS.
3. ALL NEWLY GRADED AREAS WITHIN THE PROJECT LIMITS, INCLUDING AREAS COVERED WITH EROSION CONTROL BLANKETS ARE TO BE SEEDED. THE PERMANENT PLANTINGS/SEED MIXTURE SHALL BE AS DIRECTED BY THE CITY AND IN ACCORDANCE TO THE PROJECT PERMITS.
4. AFTER SEEDING, A MULCH CONSISTING OF GRASS, HAY OR STRAW PER CALTRANS SPECIAL PROVISIONS SHALL BE BLOWN ON AND PUNCHED INTO ALL NEWLY SEEDED AREAS AT A RATE OF TWO TONS PER ACRE. IMMEDIATELY FOLLOWING APPLICATION, THE MULCH SHALL BE PUNCHED INTO THE SOIL BY A TILLER CONSISTING OF A SERIES OF DULL PLAT DISKS WITH NOTCHED OR CUTOUT EDGES.
5. IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO FIX ANY DEFICIENCIES INDICATED BY THE CITY TO PREVENT EROSION AND CONTROL SEDIMENT.
6. PRIOR TO FINAL ACCEPTANCE TEMPORARY SEDIMENT CONTROL MEASURES SHALL BE REMOVED AND ALL AREAS OF SOIL DISTURBANCE, INCLUDING BUT NOT LIMITED TO CUT AND FILL SLOPES, SWALES AND DITCHES SHALL BE STABILIZED.
7. THE PROPOSED EROSION AND SEDIMENT CONTROL MEASURES ARE A MINIMUM BEST MANAGEMENT PRACTICE. THE CONTRACTOR SHALL INSPECT THE SITE DAILY, IDENTIFYING DEFICIENCIES AND PROVIDE ADDITIONAL EROSION CONTROL MEASURES AS REQUIRED TO ENSURE THAT NO SEDIMENT LADEN WATER EXISTS THE SITE, ENTERS THE EXISTING STORMWATER SYSTEM OR ENTERS SENSITIVE AREAS. ADJUSTMENTS MAY BE MADE TO THE BMP'S AND SWPPP IN THE FIELD SUBJECT TO APPROVAL OF OR AT THE DIRECTION OF THE OWNERS REPRESENTATIVE. IF NECESSARY, SECTION 3, EROSION AND SEDIMENT CONTROL BMP'S, OF THE CALIFORNIA STORMWATER BMP HANDBOOK SHALL BE REFERENCED TO IDENTIFY THE VARIOUS FIELD CONDITIONS.
8. THE EROSION AND SEDIMENT CONTROL MEASURES IDENTIFIED ON THESE PLANS OR THE SWPPP SITE MAPS ARE APPROPRIATE TO MINIMIZE EROSION AND PREVENT SEDIMENT DISCHARGE IN THE EVENT OF SUMMER RAINSTORMS (APRIL 15TH THROUGH OCTOBER 15TH). IN THE EVENT THERE IS A DELAY IN THE CONSTRUCTION SCHEDULE BMP'S/ OR FINAL STABILIZATION MEASURES (INCLUDING SEED MIX AND METHOD OF APPLICATION) MAY NEED TO BE ADJUSTED.
9. ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL REMAIN IN PLACE AND MAINTAINED IN ACCORDANCE WITH THEIR RESPECTIVE BMP FACT SHEETS UNTIL THE DISTURBED AREAS ARE PERMANENTLY STABILIZED. FINAL, PERMANENT STABILIZATION SHALL BE 70% ESTABLISHED VEGETATIVE GROWTH AS DETERMINED BY THE OWNERS REPRESENTATIVE.

NOTE: THE EARTHWORK VOLUMES SHOWN ARE BASED ON THE DIFFERENCE BETWEEN EXISTING GROUND AND PAVEMENT SECTION SUBGRADE. THE VOLUMES ARE UNADJUSTED FOR COMPACTION, EXPANSION, DRAINAGE DITCH AND STRUCTURE EXCAVATION, TOPSOIL OR TRENCH SPOILS. THE COMPLETENESS AND ACCURACY OF THE DATA IS NOT GUARANTEED. IT IS THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL EARTHWORK QUANTITIES.



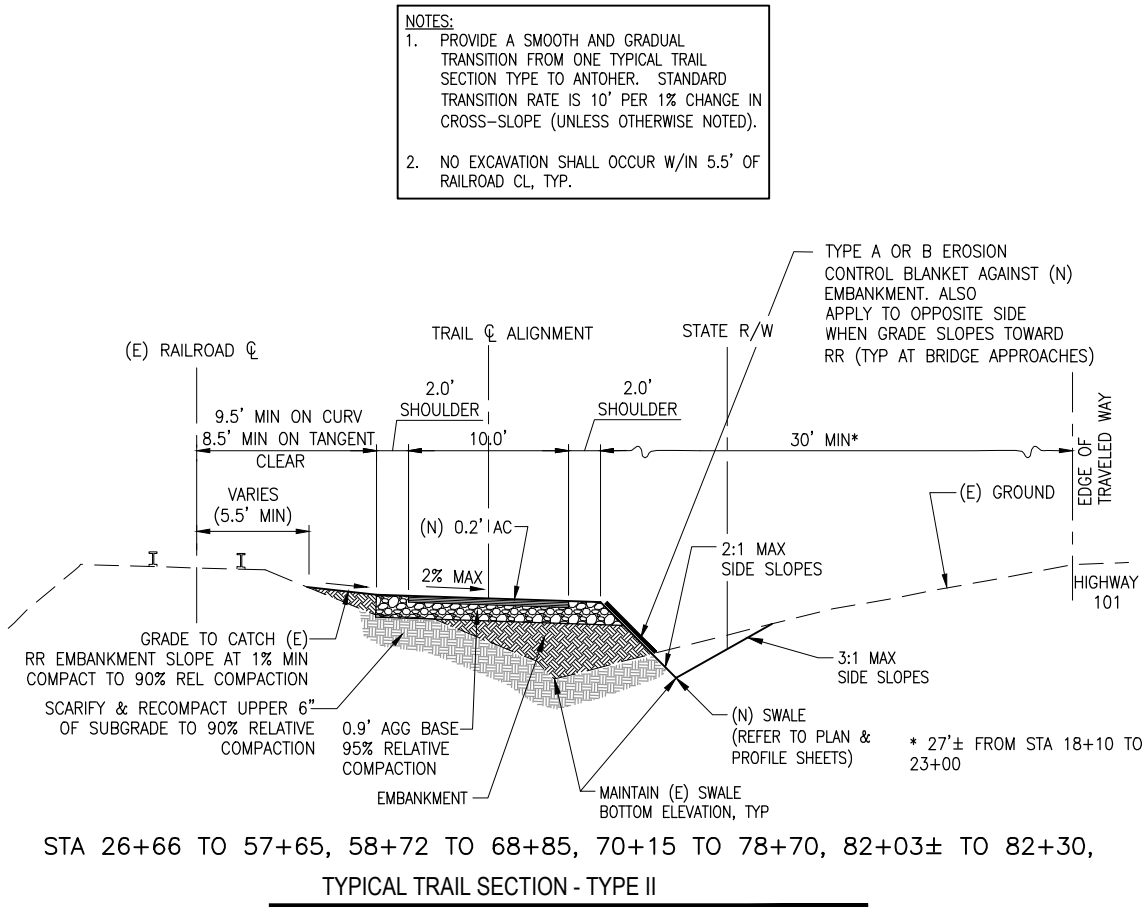
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GENERAL CONSTRUCTION NOTES

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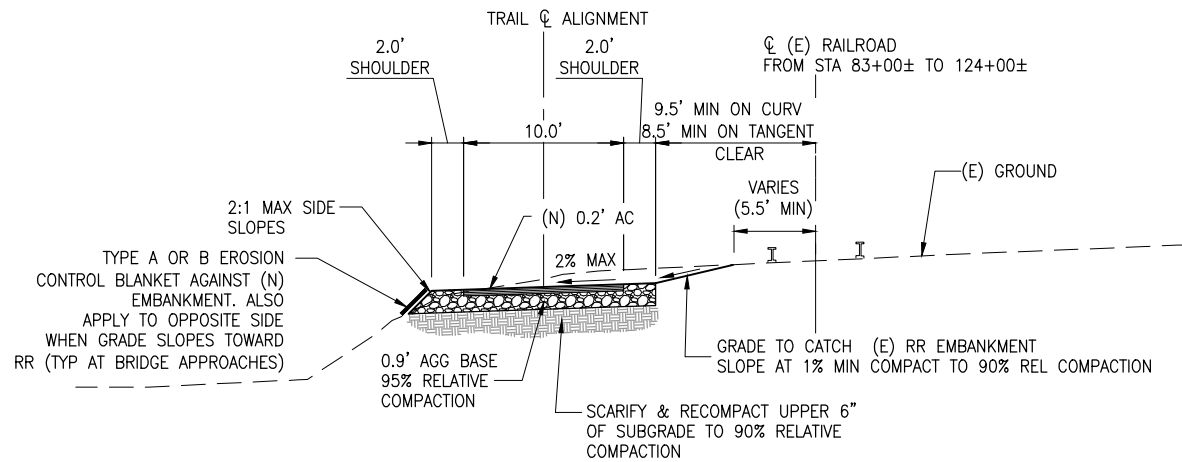


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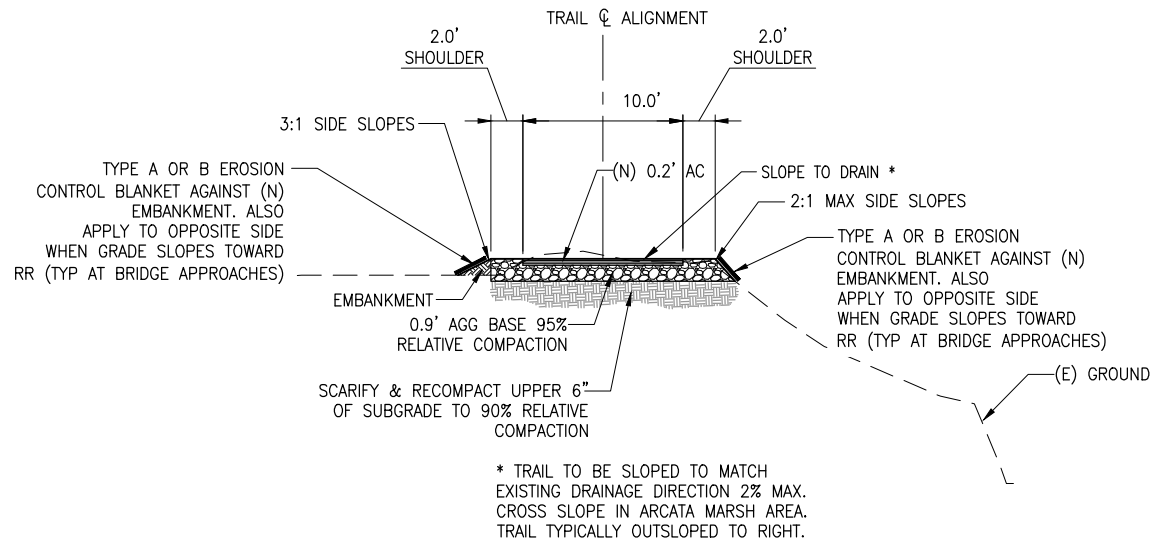
CITY OF ARCATA HUMBOLDT BAY TRAIL NORTH	TYPICAL TRAIL SECTIONS
--	------------------------

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DRWN: OFG	CHKD: JW
G-5.0	
SHEET 5	OF 52



STA 83+00 TO 121+30, 121+60 TO 124+00, 160+40 TO 169+47
TYPICAL TRAIL SECTION - TYPE III

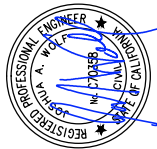
- NOTES:
1. PROVIDE A SMOOTH AND GRADUAL TRANSITION FROM ONE TYPICAL TRAIL SECTION TYPE TO ANOTHER. STANDARD TRANSITION RATE IS 10' PER 1% CHANGE IN CROSS-SLOPE (UNLESS OTHERWISE NOTED).
 2. NO EXCAVATION SHALL OCCUR W/IN 5.5' OF RAILROAD CL, TYP.



STA 82+30 TO 83+00, 124+00 TO 125+40, 126+20 TO 140+50
TYPICAL TRAIL SECTION - TYPE IV



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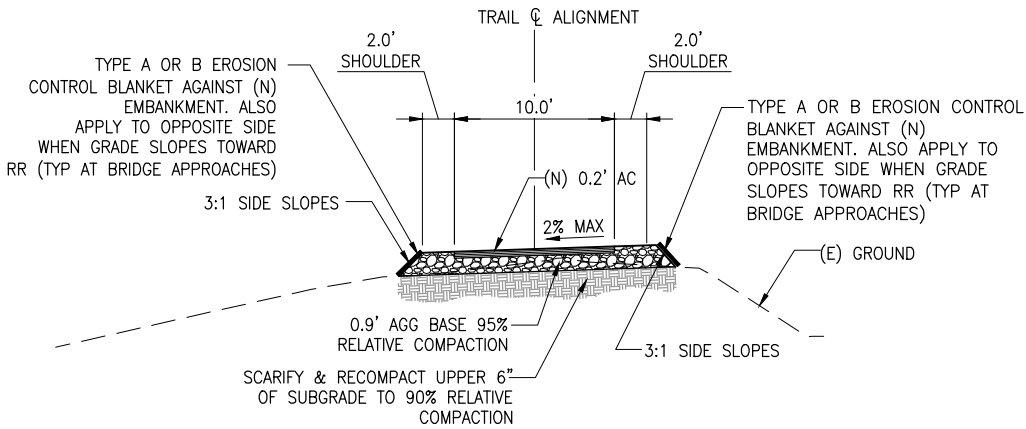


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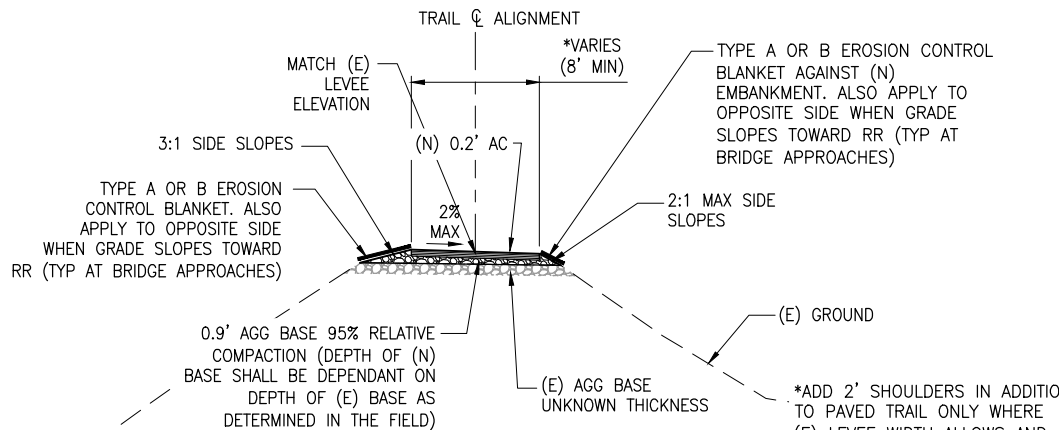
CITY OF ARCATA HUMBOLDT BAY TRAIL NORTH	TYPICAL TRAIL SECTIONS
--	------------------------

PROJ NO: 8411982
DRWN: OFG CHKD: JW
G-6.0
SHEET 6 OF 52



STA 140+50 TO 150+72
TYPICAL TRAIL SECTION - TYPE V

NOTE: PROVIDE A SMOOTH AND GRADUAL TRANSITION FROM ONE TYPICAL TRAIL SECTION TYPE TO ANOTHER. STANDARD TRANSITION RATE IS 10' PER 1% CHANGE IN CROSS-SLOPE (UNLESS OTHERWISE NOTED)



STA 151+04 TO 159+45
TYPICAL TRAIL SECTION - TYPE VI

NOTE:
REMOVE TOP 6" OF RED LAVA ROCK TRAIL SURFACING. STOCKPILE IN CITY CORP YARD AND UTILIZE FOR TRAIL SHOULDER SURFACING (4" THICK) AT FOLLOWING STATIONS: STA 126+18 TO 159+45.



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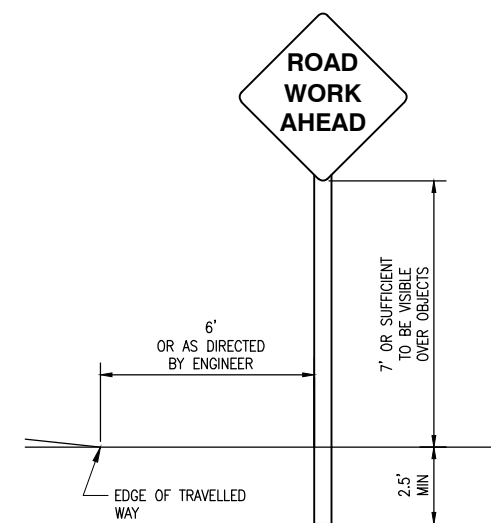
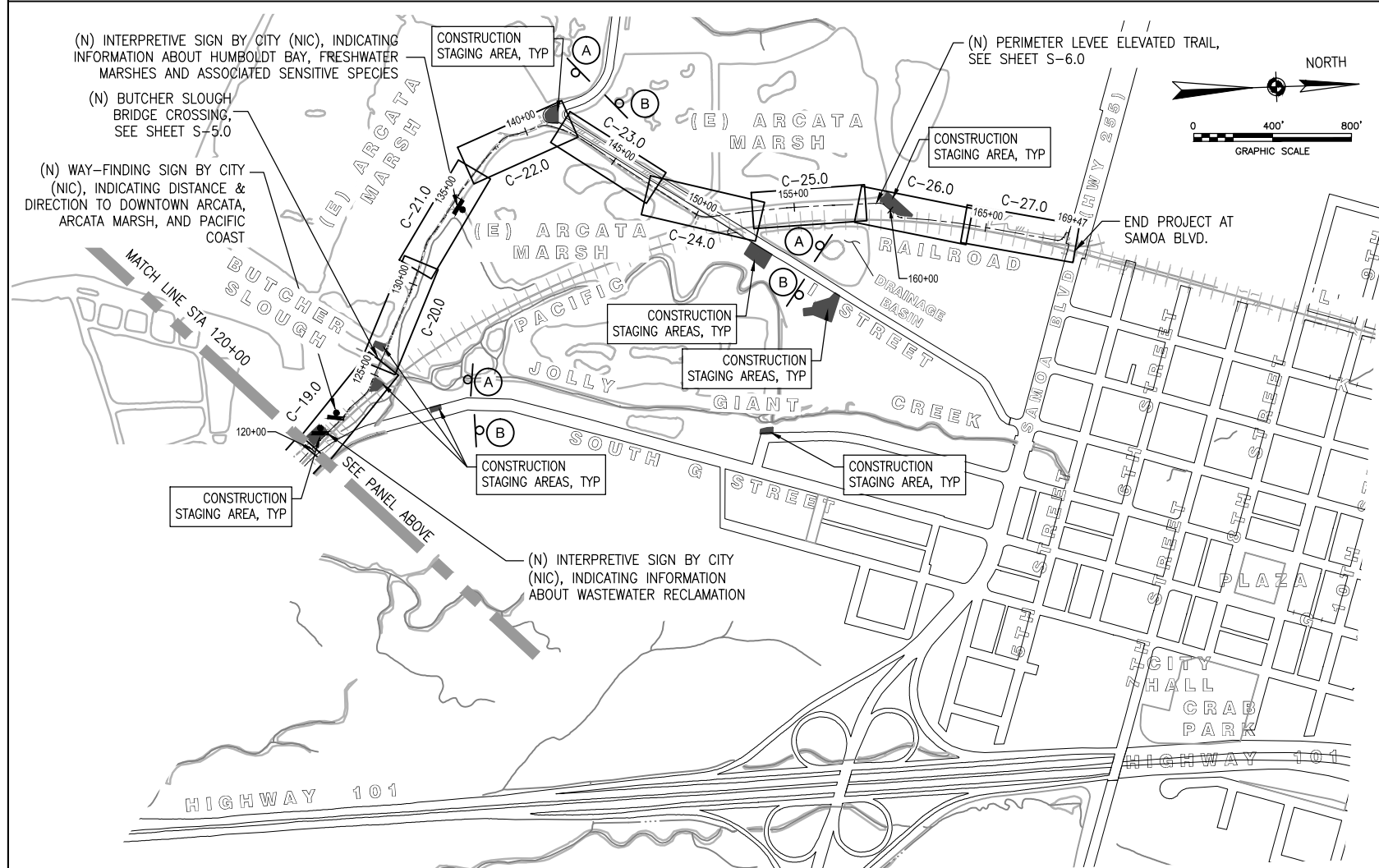
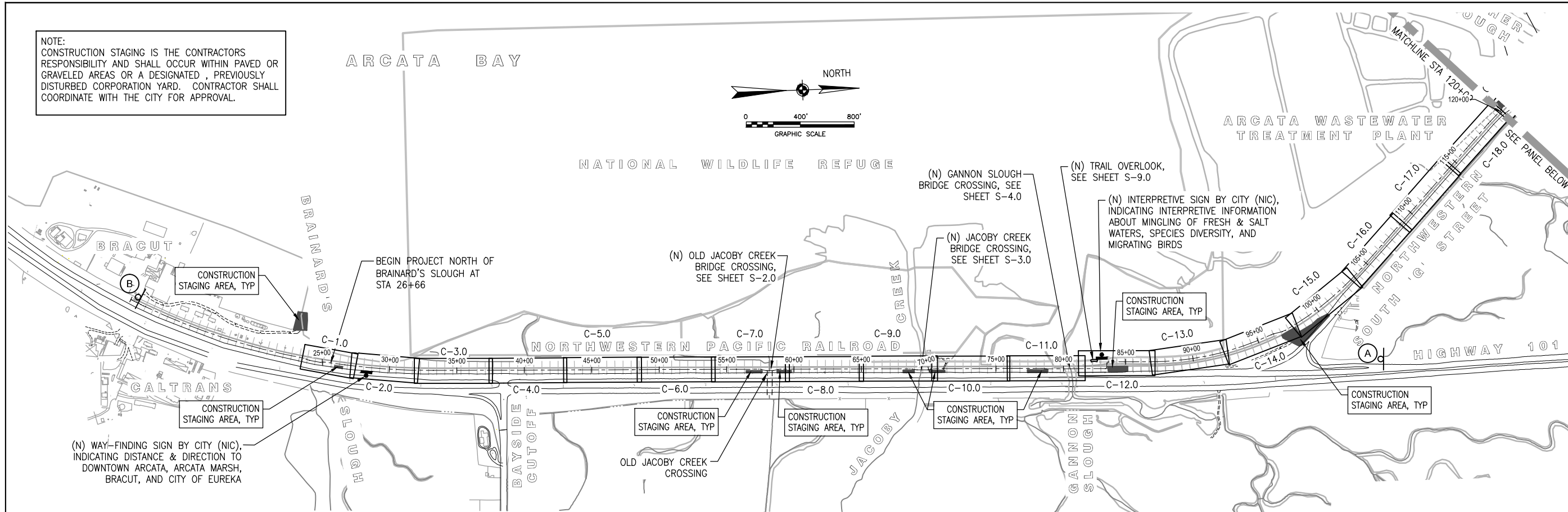
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CITY OF ARCATA HUMBOLDT BAY TRAIL NORTH	TYPICAL TRAIL SECTIONS
--	------------------------

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DRWN: OFG	

G-7.0

NOTE:
CONSTRUCTION STAGING IS THE CONTRACTORS RESPONSIBILITY AND SHALL OCCUR WITHIN PAVED OR GRAVELED AREAS OR A DESIGNATED , PREVIOUSLY DISTURBED CORPORATION YARD. CONTRACTOR SHALL COORDINATE WITH THE CITY FOR APPROVAL.



TYPICAL STATIONARY
CONSTRUCTION AREA SIGNS

STATIONARY MOUNTED CONSTRUCTION AREA SIGNS

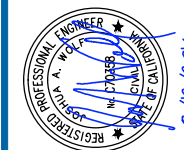
SIGN LABEL	SIGN CODE	PANEL SIZE	SIGN MESSAGE	NUMBER & POST SIZE	NO. OF SIGNS
A	W20-1	36"x36"	ROAD WORK AHEAD	(1) 4"x4"	4
B	G20-2	36"x18"	END ROAD WORK	(1) 4"x4"	4

CONSTRUCTION SIGN NOTES:

- EXACT SIGN LOCATIONS AND POSITIONS TO BE APPROVED BY THE ENGINEER.
- ALL WARNING SIGNS SHALL HAVE A BLACK LEGEND AND BORDER ON ORANGE BACKGROUND.
- ALTERNATIVE POST CONFIGURATION TO BE APPROVED BY ENGINEER.



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CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH

GENERAL
PLAN SHEET INDEX,
STAGING AREAS, SIGNAGE

PROJ NO: 8411982

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G-8.0

SHEET 8 OF 52

LINE TABLE (TRAIL CL)			
LINE #	LENGTH	DIRECTION	
L#1	1088.93'	N01°47'42"E	
L#2	391.36'	N01°46'32"E	
L#3	311.29'	N01°42'30"E	
L#4	456.66'	N01°48'20"E	
L#5	163.69'	N01°48'16"E	
L#6	33.93'	N15°20'36"E	
L#7	55.00'	N01°48'32"E	
L#8	33.43'	N11°43'31"W	
L#9	550.25'	N01°49'25"E	
L#10	406.38'	N01°47'01"E	
L#11	36.33'	N13°12'03"E	
L#12	80.00'	N01°47'57"E	
L#13	22.53'	N13°12'03"W	
L#14	424.31'	N01°53'46"E	
L#15	370.67'	N01°48'48"E	
L#16	14.29'	N08°40'51"E	
L#17	440.27'	N01°47'17"E	
L#18	25.33'	N88°12'50"W	
L#19	311.67'	N01°51'10"E	
L#20	64.88'	N12°17'13"W	
L#21	228.36'	N46°39'47"W	
L#22	172.07'	N46°54'20"W	
L#23	192.77'	N46°43'00"W	
L#24	151.13'	N46°48'03"W	
L#25	140.12'	N46°55'22"W	
L#26	118.95'	N46°36'37"W	
L#27	177.05'	N46°44'46"W	
L#28	245.24'	N46°32'31"W	
L#29	188.58'	N44°50'35"W	
L#30	97.46'	N42°11'38"W	

LINE TABLE (TRAIL CL)			
LINE #	LENGTH	DIRECTION	
L#31	29.27'	N54°54'48"W	
L#32	97.06'	N39°46'53"W	
L#33	28.72'	N68°12'37"W	
L#34	153.33'	N63°30'24"W	
L#35	37.08'	N66°56'45"W	
L#36	209.83'	N64°50'51"W	
L#37	61.11'	N67°37'48"W	
L#38	81.00'	N71°20'36"W	
L#39	47.25'	N50°57'20"W	
L#40	49.90'	N54°18'22"W	
L#41	117.52'	N55°50'30"W	
L#42	193.35'	N50°44'53"W	
L#43	70.84'	N46°44'35"W	
L#44	26.33'	N08°02'29"W	
L#45	65.39'	N20°20'38"W	
L#46	62.39'	N18°46'43"E	
L#47	42.85'	N30°50'47"E	
L#48	140.79'	N36°52'09"E	
L#49	453.69'	N34°49'48"E	
L#50	144.47'	N06°12'58"W	
L#51	196.78'	N03°28'31"W	
L#52	190.09'	N01°02'18"W	
L#53	233.12'	N00°46'50"W	
L#54	97.35'	N41°39'50"E	
L#55	353.22'	N12°12'31"E	
L#56	11.13'	N42°58'43"W	
L#57	6.48'	N17°20'19"E	

CURVE TABLE (TRAIL CL)				
CURVE #	RADIUS	LENGTH	DELTA	
C#1	4900.00'	658.33'	N5° 38' 39"E	
C#2	90.00'	21.27'	N8° 34' 26"E	
C#3	90.00'	21.28'	N4° 57' 03"W	
C#4	90.00'	17.93'	N7° 29' 32"E	
C#5	90.00'	23.71'	N5° 39' 08"W	
C#6	90.00'	10.79'	N5° 14' 49"E	
C#7	90.00'	10.83'	N5° 14' 04"E	
C#8	12.00'	18.85'	N43° 12' 47"W	
C#9	12.00'	18.86'	N43° 10' 50"W	
C#10	2115.00'	922.46'	N10° 38' 32"W	
C#11	100.00'	18.94'	N17° 42' 43"W	
C#12	100.00'	26.84'	N19° 58' 35"W	
C#13	2200.00'	729.43'	N37° 09' 52"W	
C#14	1200.00'	55.48'	N43° 31' 06"W	
C#15	90.00'	19.98'	N48° 33' 13"W	
C#16	90.00'	23.77'	N47° 20' 50"W	
C#17	100.00'	49.62'	N53° 59' 45"W	
C#18	200.00'	16.42'	N65° 51' 30"W	
C#19	500.00'	30.01'	N65° 13' 35"W	
C#20	750.00'	27.47'	N65° 53' 48"W	
C#21	750.00'	36.42'	N66° 14' 20"W	
C#22	1500.00'	97.21'	N69° 29' 12"W	
C#23	90.00'	32.02'	N61° 08' 58"W	
C#24	500.00'	29.24'	N52° 37' 51"W	
C#25	750.00'	20.10'	N55° 04' 26"W	
C#26	500.00'	44.45'	N53° 17' 42"W	
C#27	500.00'	34.95'	N48° 44' 44"W	
C#28	115.00'	77.68'	N27° 23' 32"W	
C#29	200.00'	42.94'	N14° 11' 33"W	
C#30	100.00'	68.28'	N0° 46' 57"W	

CURVE TABLE (TRAIL CL)				
CURVE #	RADIUS	LENGTH	DELTA	
C#31	200.00'	42.12'	N24° 48' 45"E	
C#32	750.00'	78.84'	N33° 51' 28"E	
C#33	1000.00'	35.59'	N35° 50' 58"E	
C#34	115.00'	82.38'	N14° 18' 25"E	
C#35	1000.00'	47.84'	N4° 50' 45"W	
C#36	1000.00'	42.53'	N2° 15' 25"W	
C#37	2000.00'	9.01'	N0° 54' 34"W	
C#38	22.00'	16.30'	N20° 26' 30"E	
C#39	30.00'	18.99'	N23° 31' 40"E	
C#40	3770.00'	448.56'	N8° 48' 00"E	
C#41	25.00'	24.08'	N15° 23' 06"W	
C#42	40.00'	20.34'	N57° 32' 54"W	
C#43	10.00'	15.61'	N27° 23' 23"W	

CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH

GENERAL
LINE & CURVE TABLES

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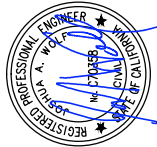
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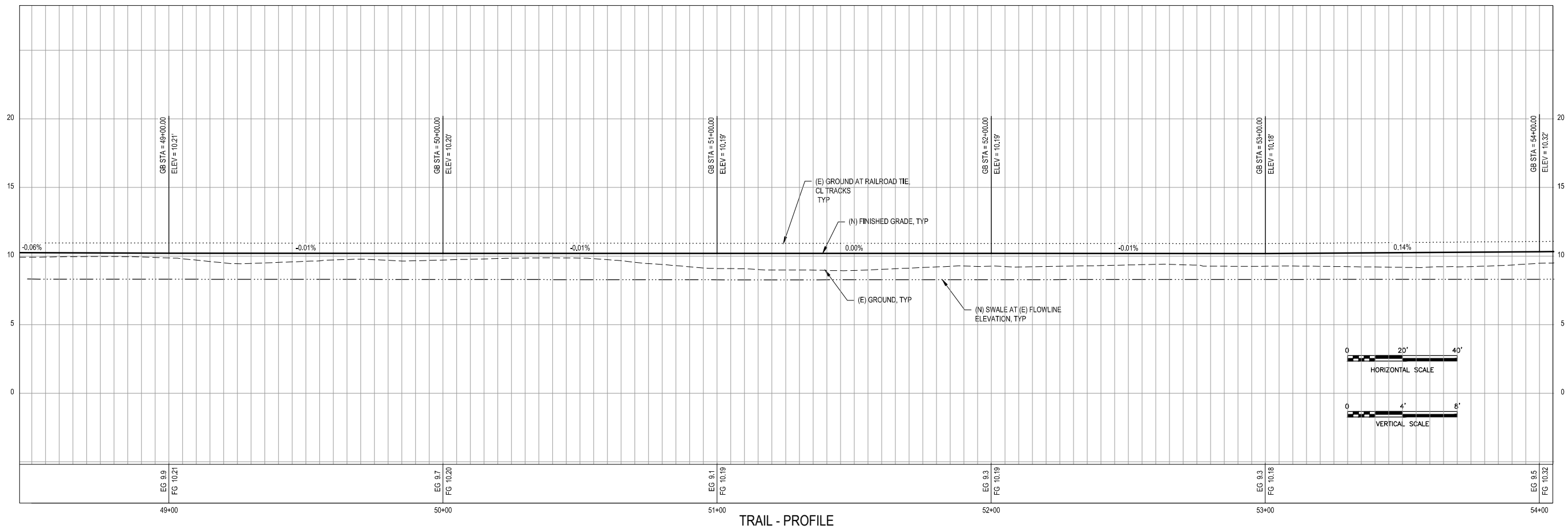
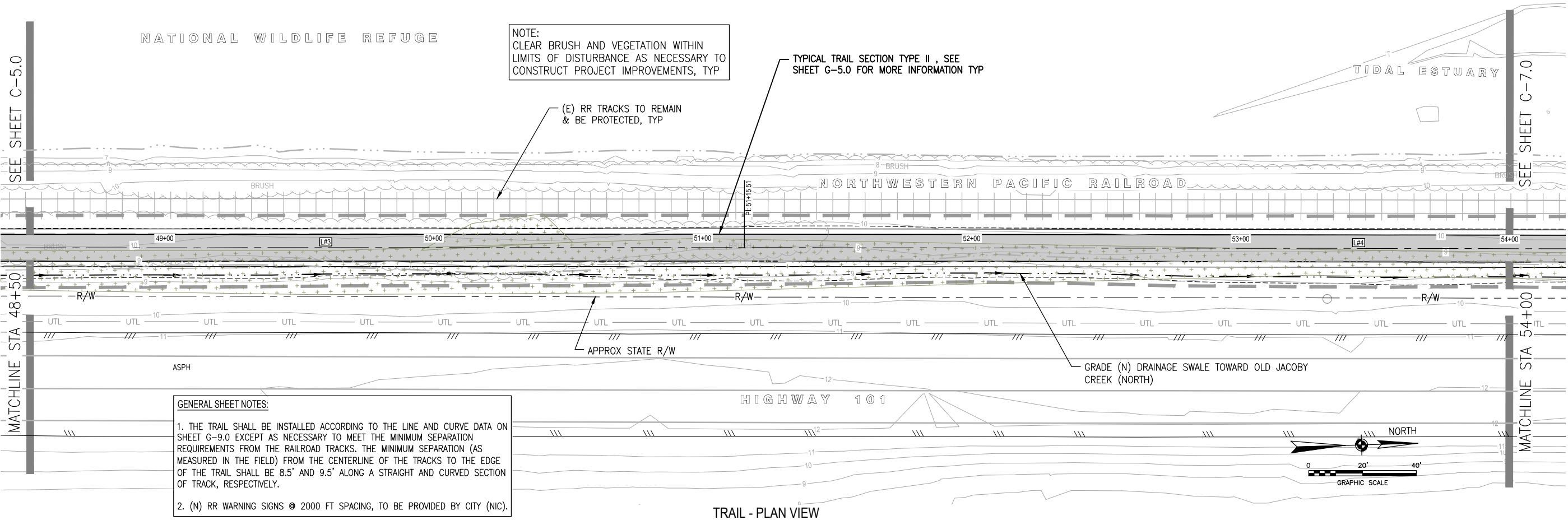


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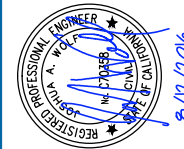
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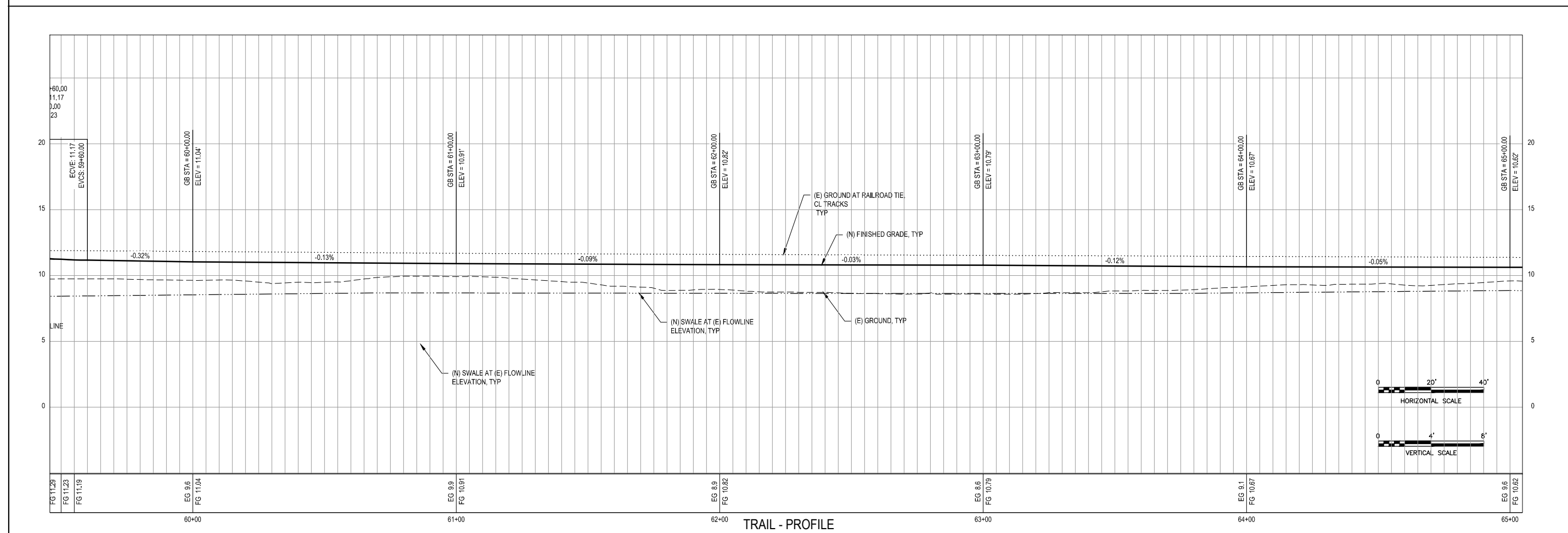
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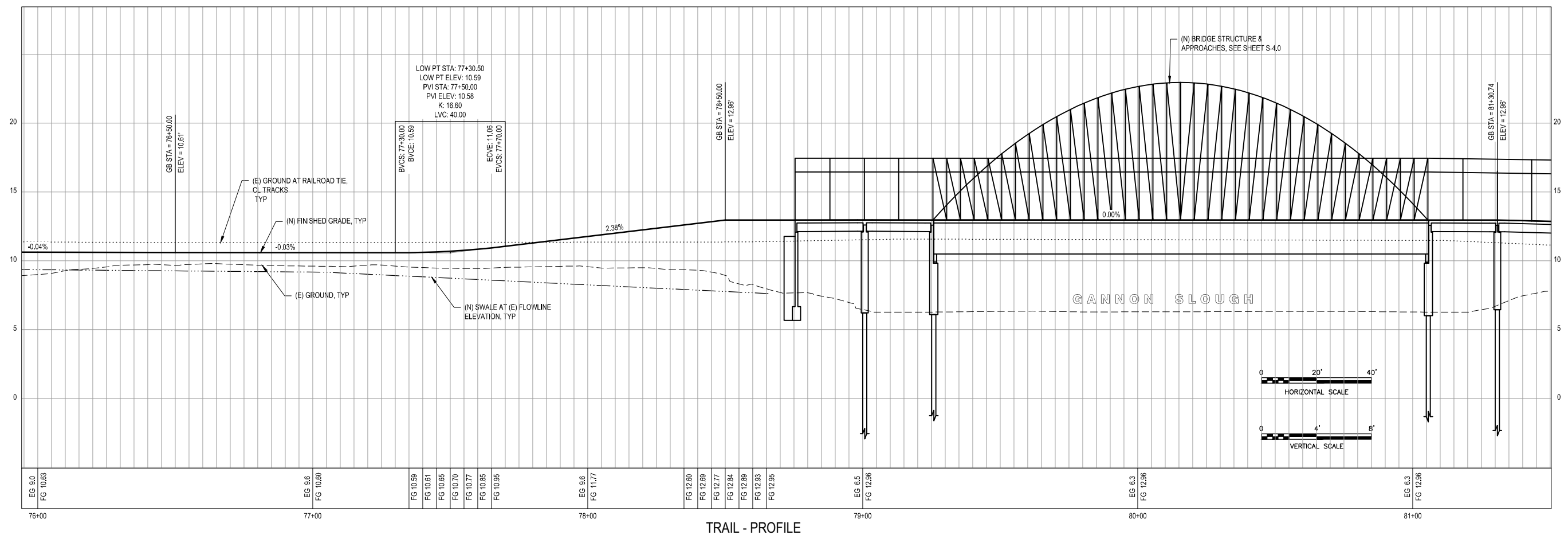
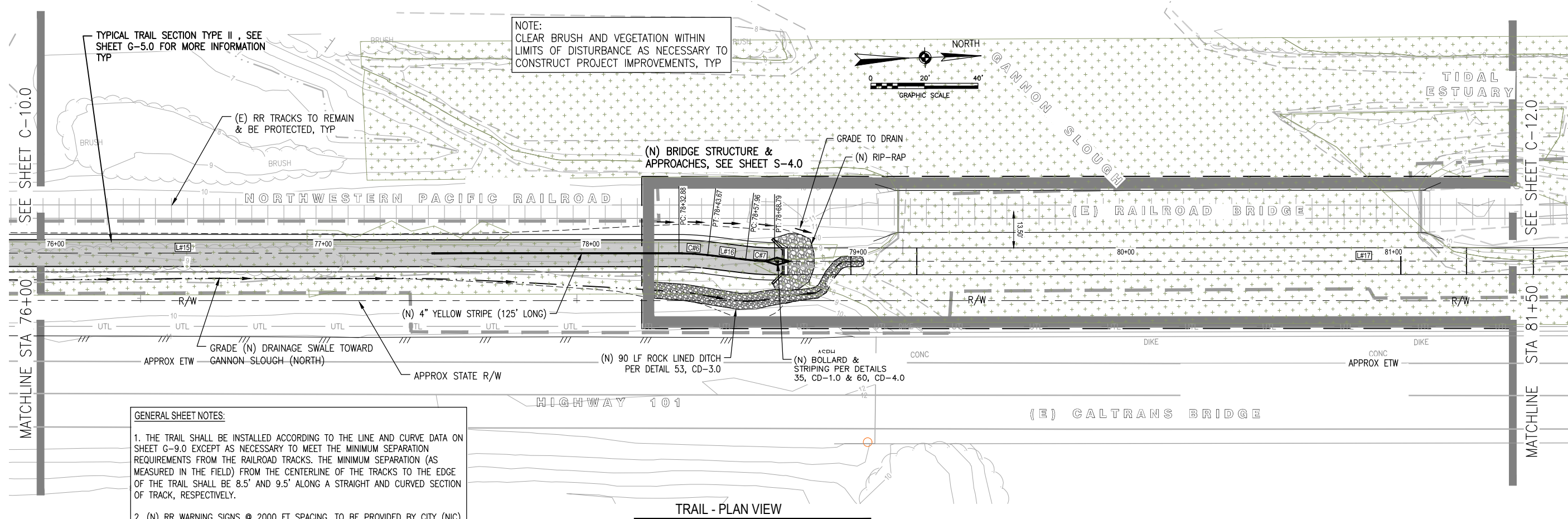
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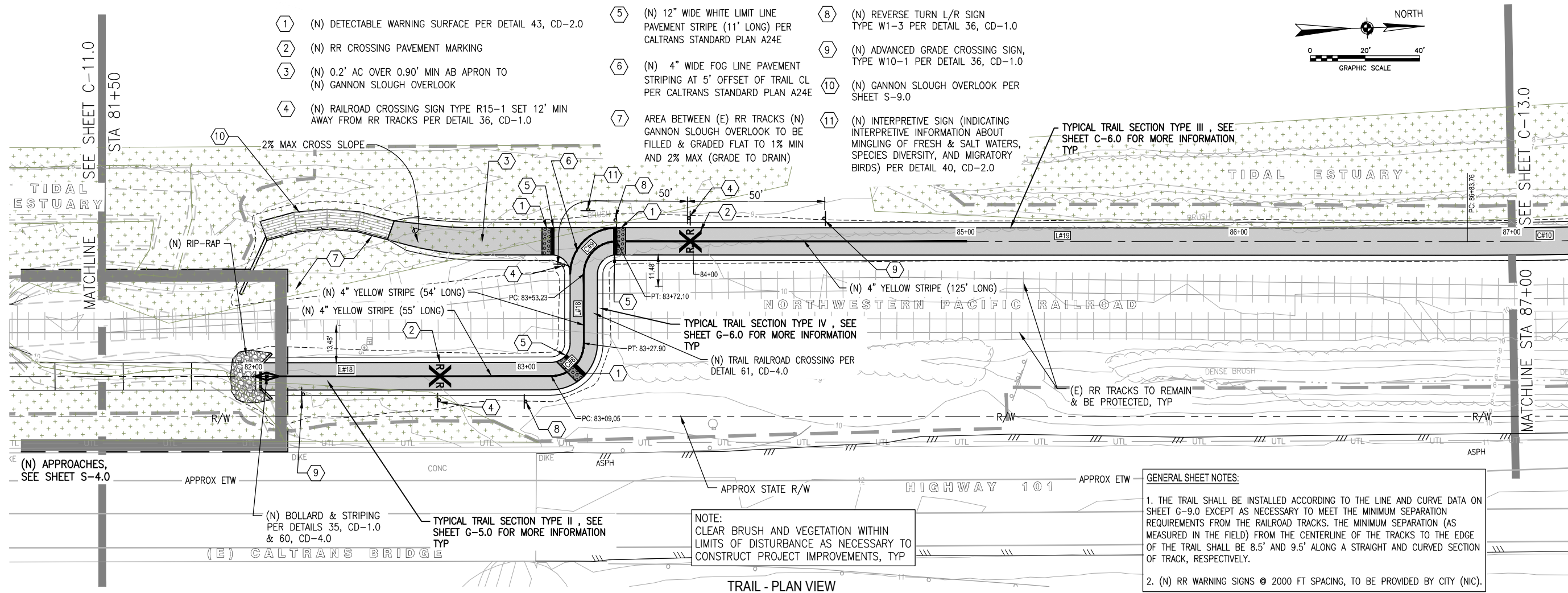
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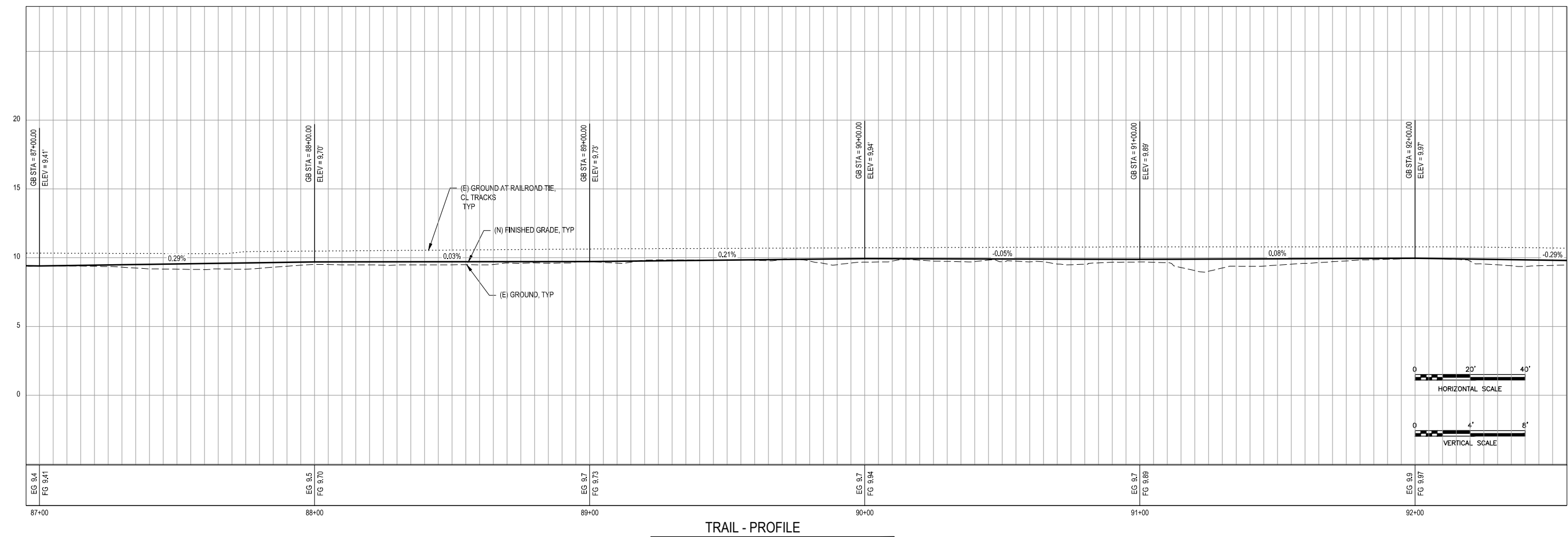
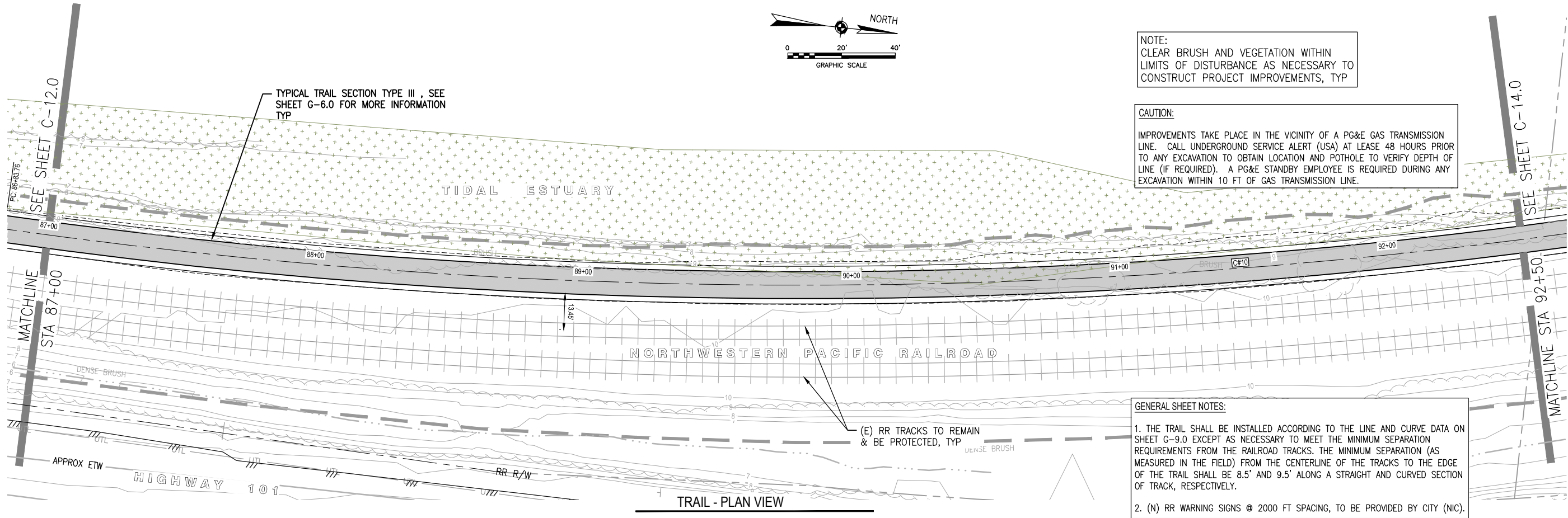
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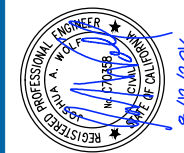
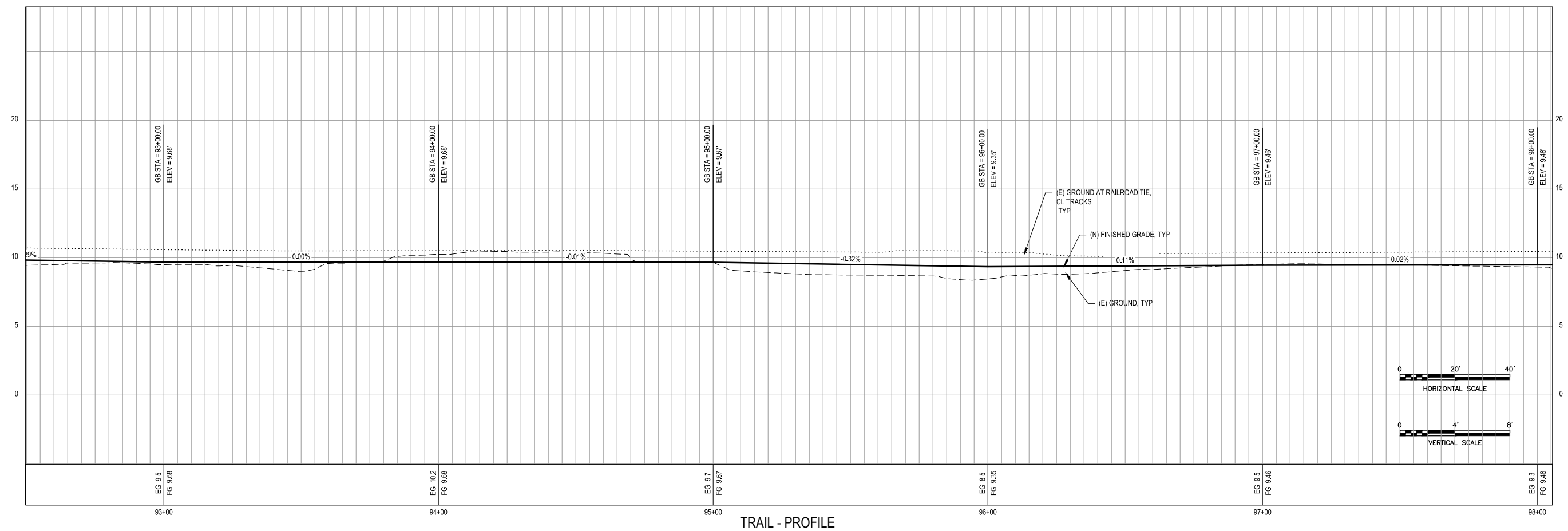
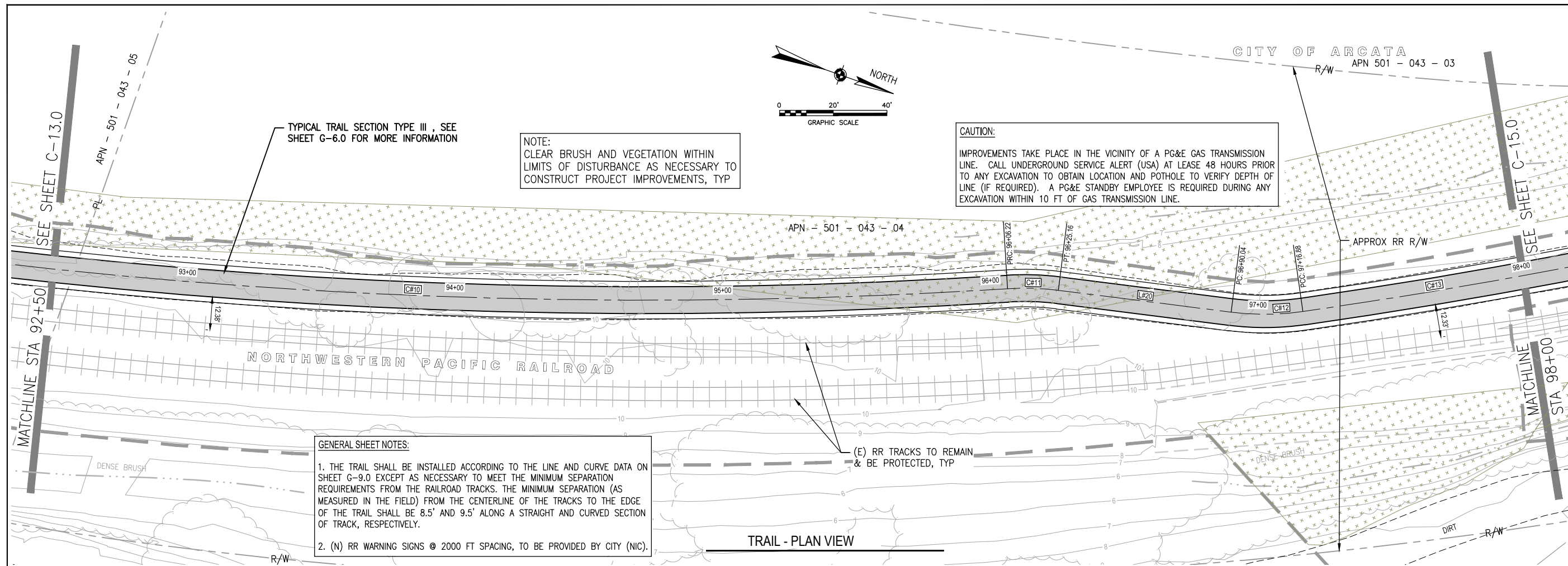
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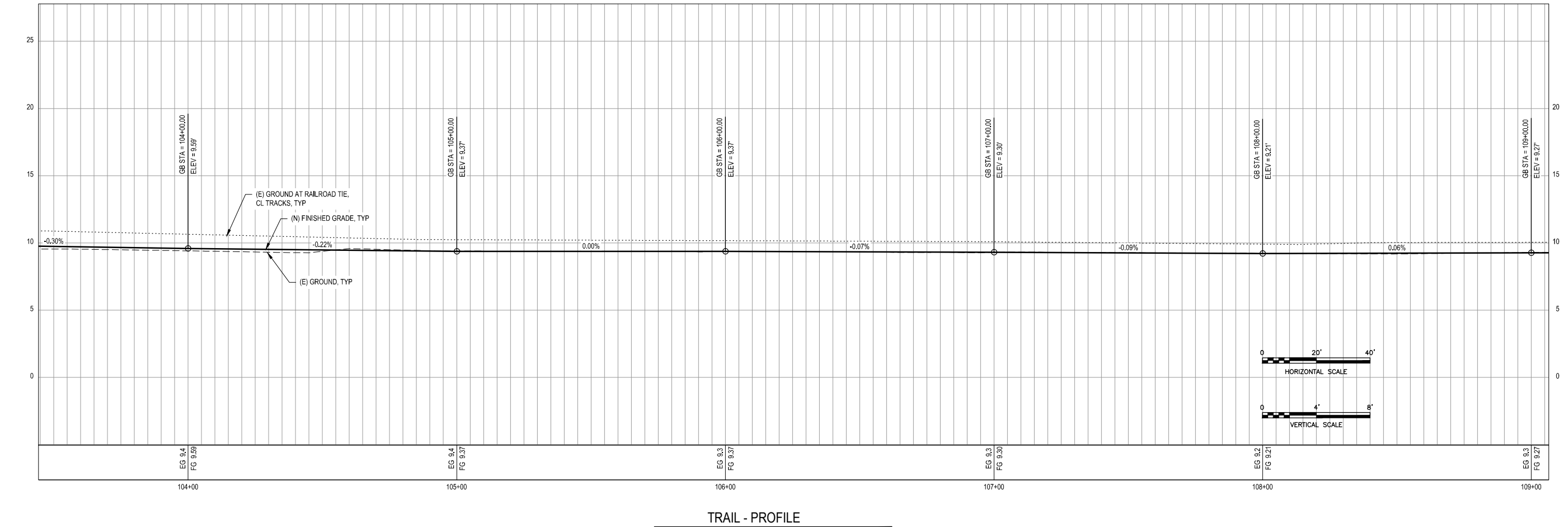
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STA 92+50 TO STA 98+00**

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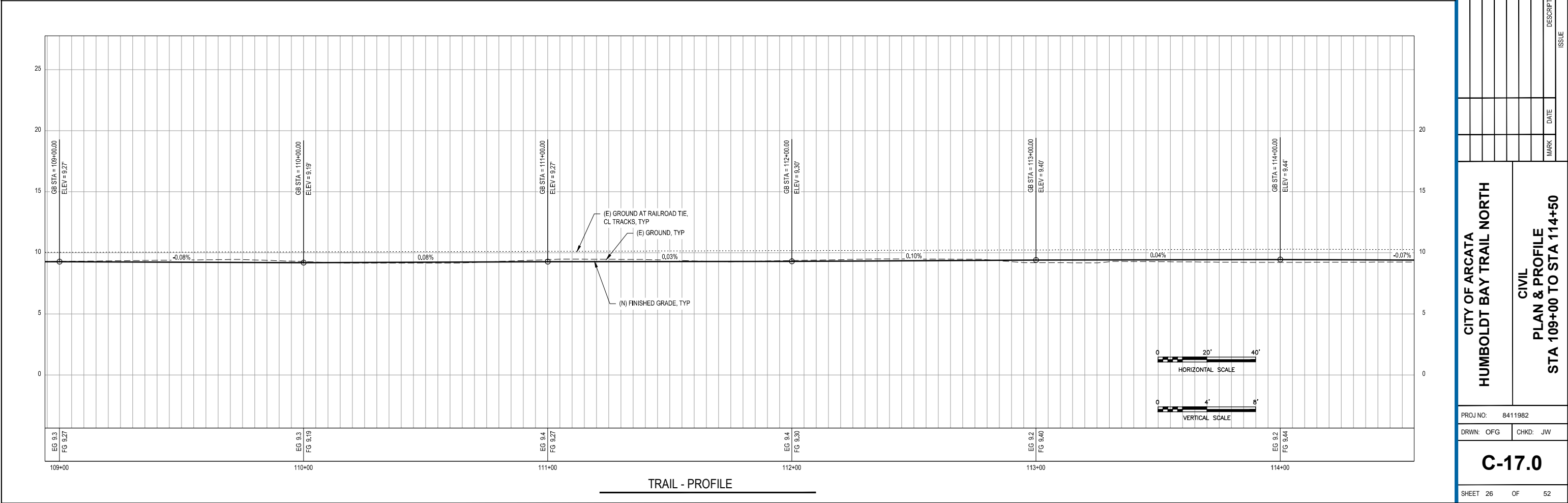
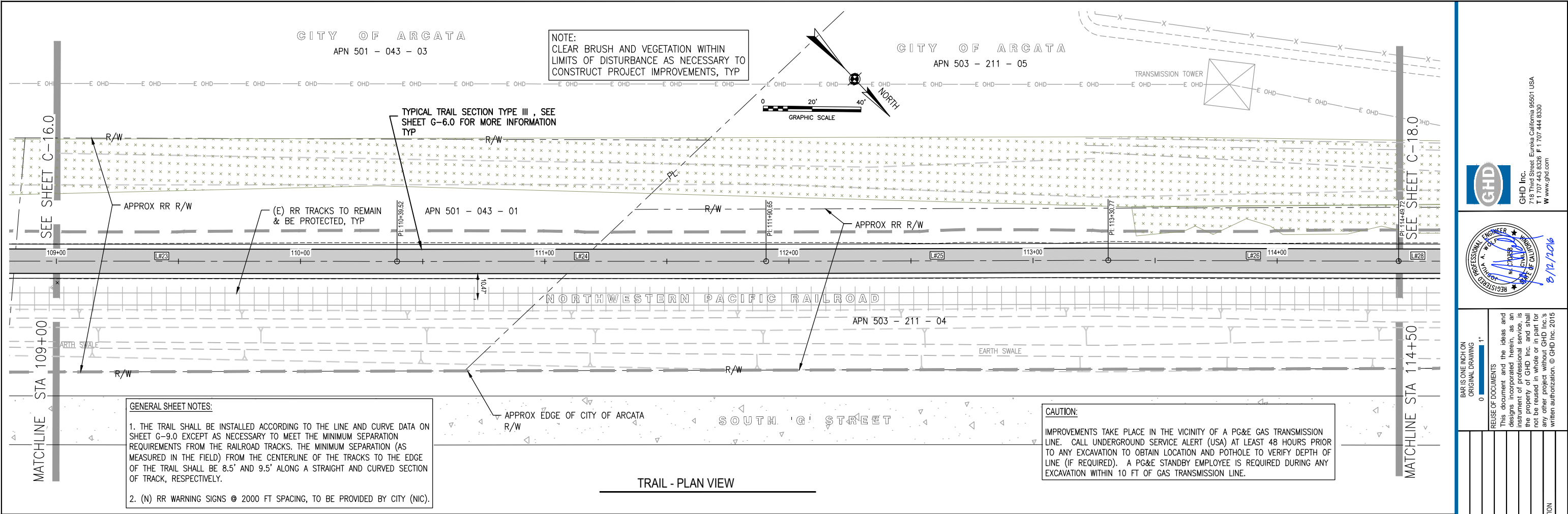
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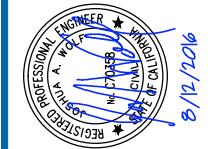
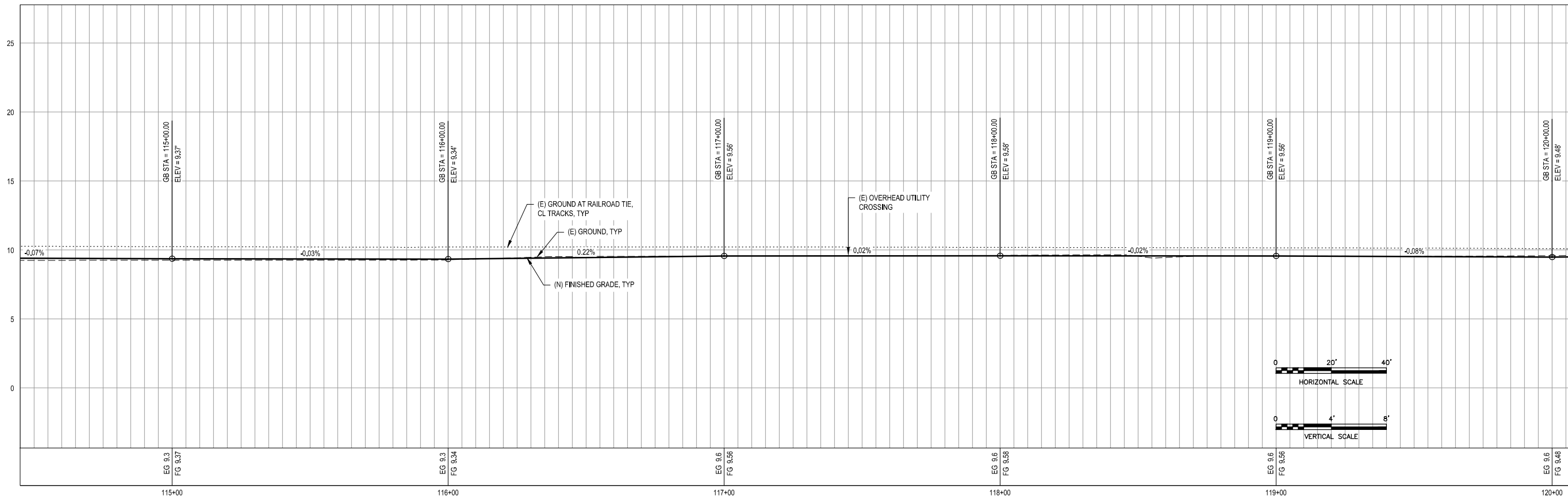
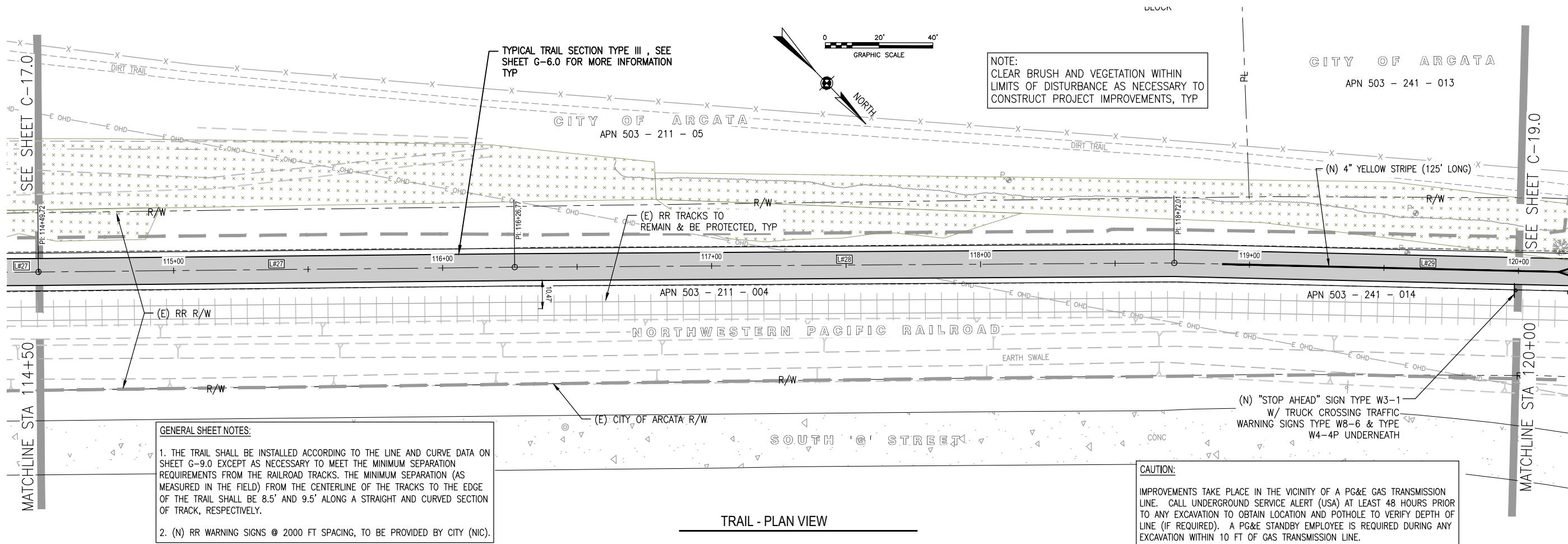
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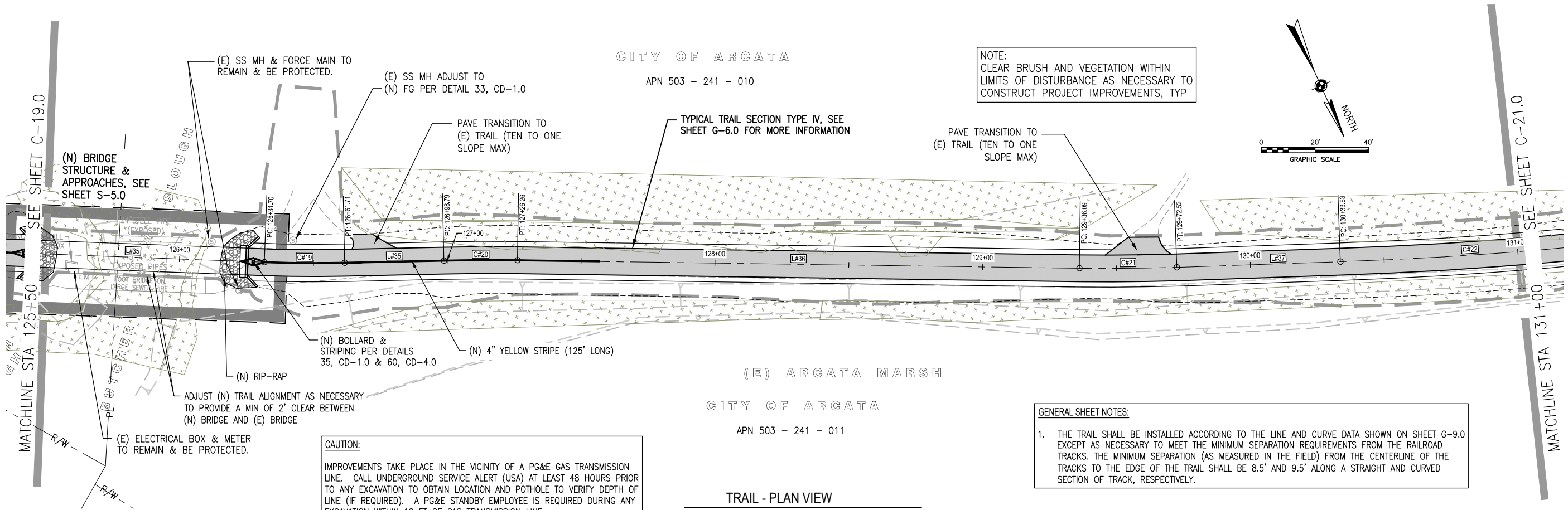
CIVIL
PLAN & PROFILE
STA 114+50 TO STA 120+00

PROJ NO: 8411982

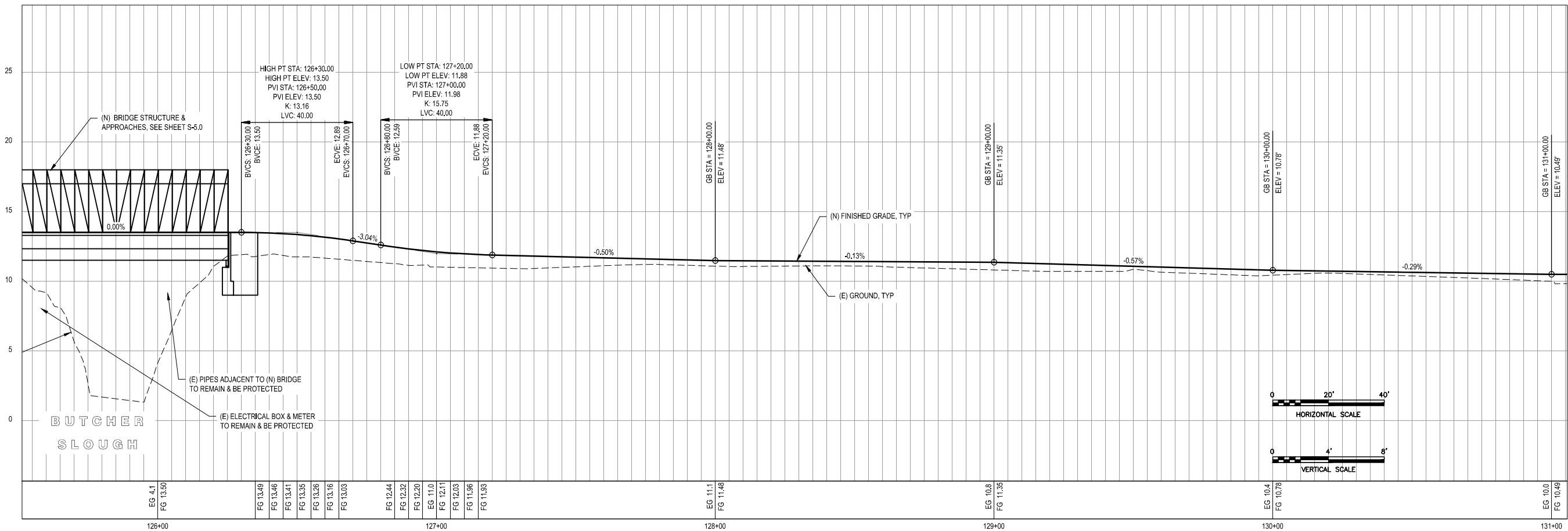
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SHEET 27 OF 52



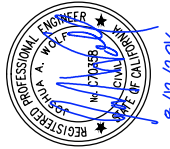
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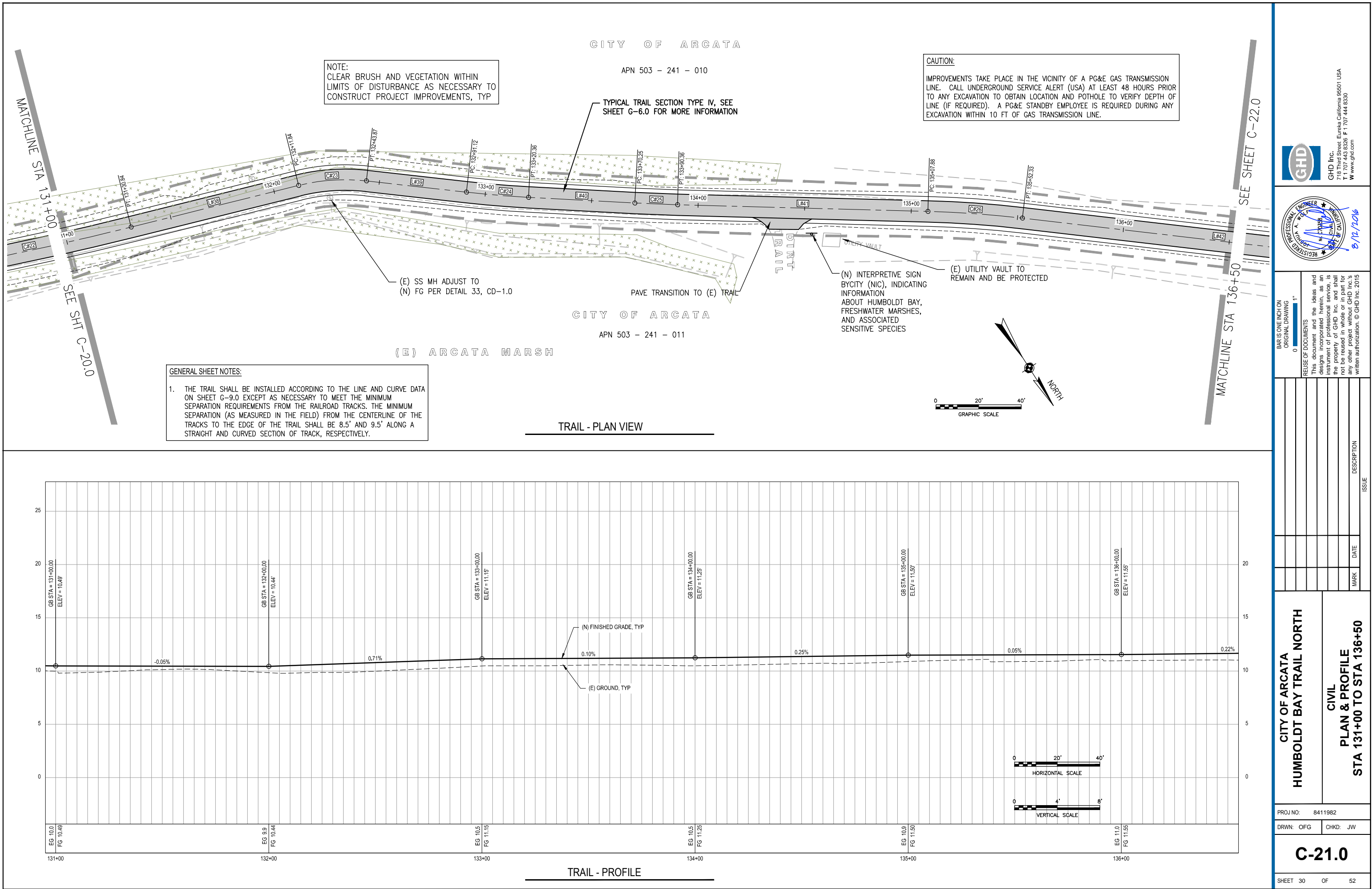
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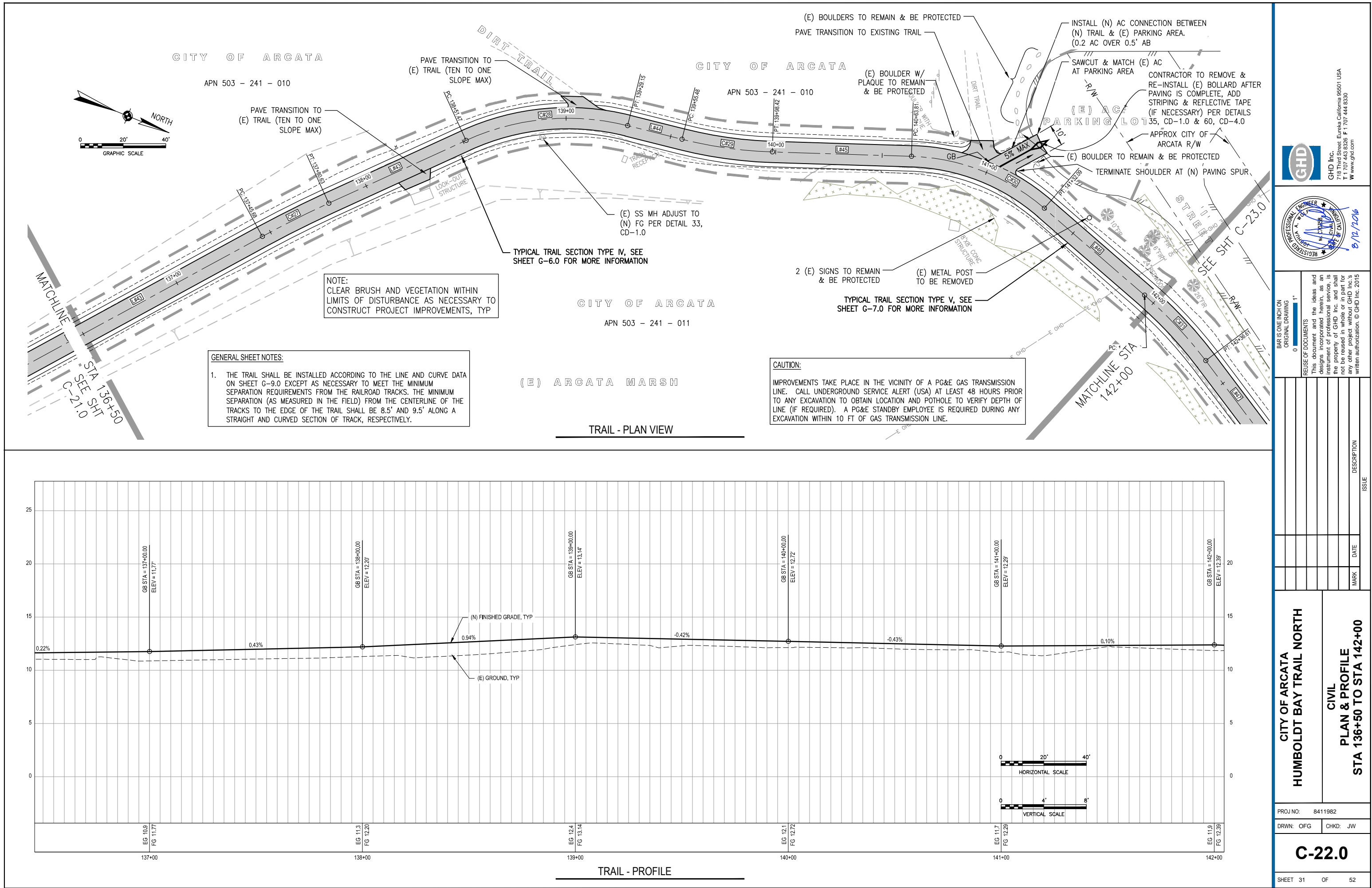
CITY OF ARCATA
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CIVIL
PLAN & PROFILE
STA 125+50 TO STA 131+00

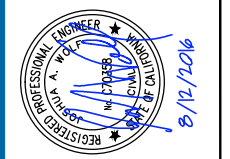
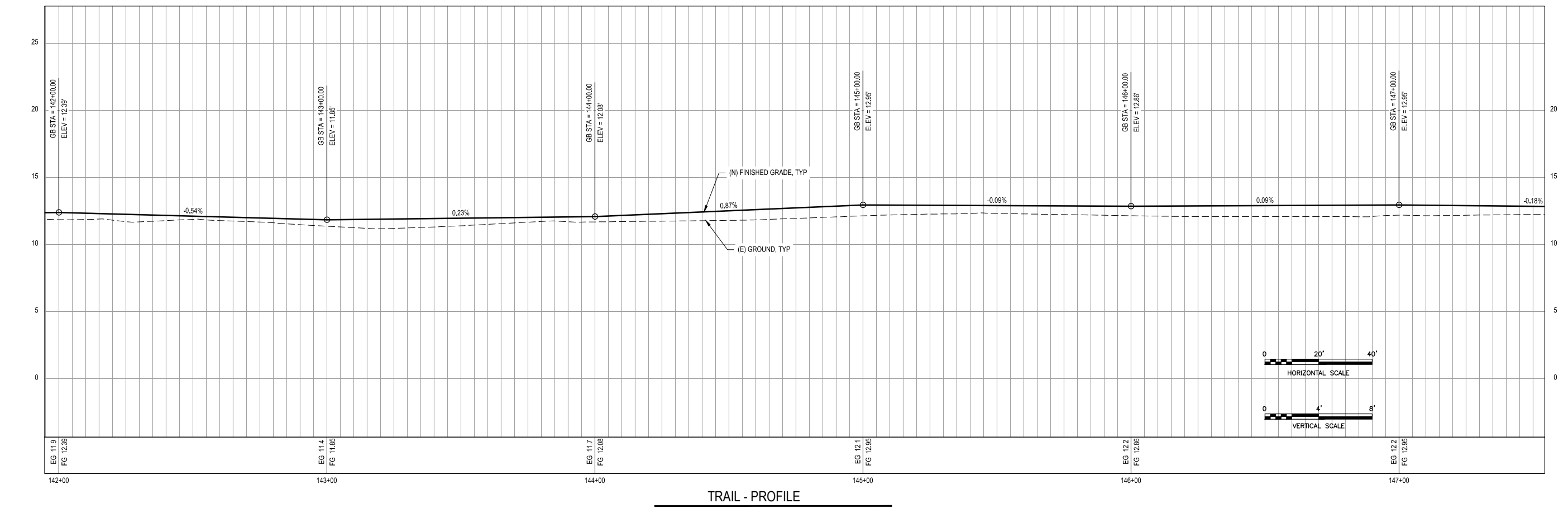
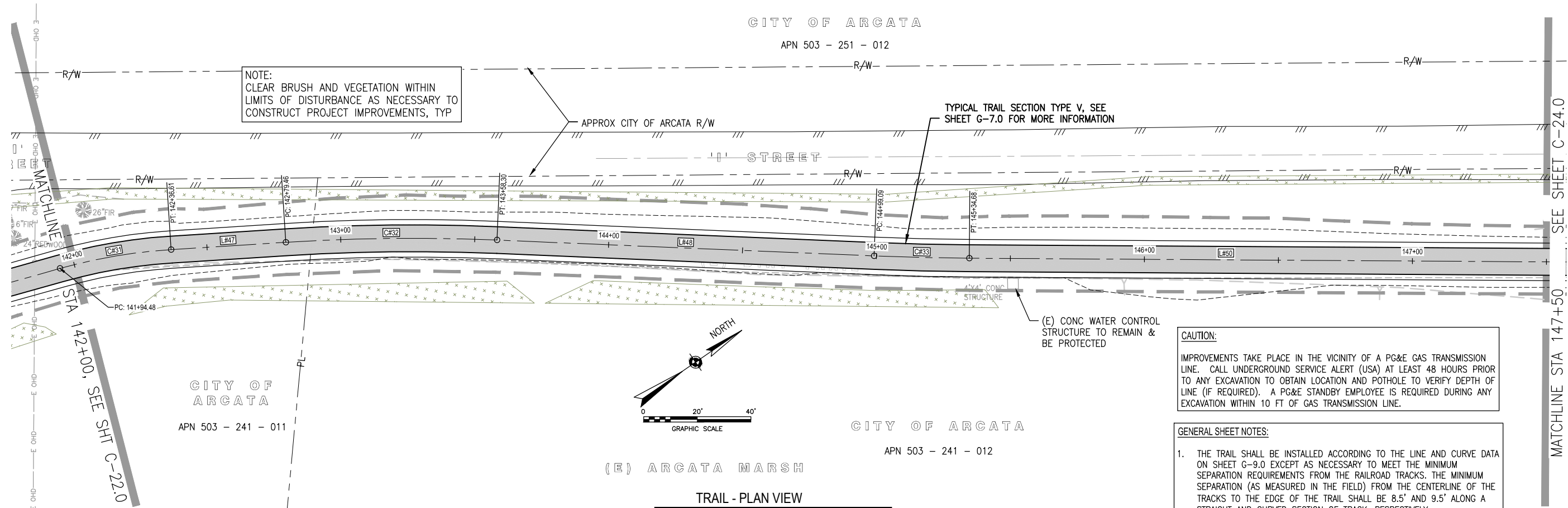
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CITY OF ARCATA
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STA 142+00 TO STA 147+50

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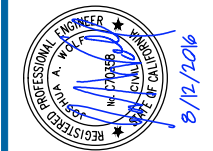
C-23.0

SHEET 32 OF 52

IMPROVEMENTS TAKE PLACE IN THE VICINITY OF A PG&E GAS TRANSMISSION LINE. CALL UNDERGROUND SERVICE ALERT (USA) AT LEAST 48 HOURS PRIOR TO ANY EXCAVATION TO OBTAIN LOCATION AND POTHOLE TO VERIFY DEPTH OF LINE (IF REQUIRED). A PG&E STANDBY EMPLOYEE IS REQUIRED DURING ANY EXCAVATION WITHIN 10 FT OF GAS TRANSMISSION LINE.

1. THE TRAIL SHALL BE INSTALLED ACCORDING TO THE LINE AND CURVE DATA ON SHEET G-9.0 EXCEPT AS NECESSARY TO MEET THE MINIMUM SEPARATION REQUIREMENTS FROM THE RAILROAD TRACKS. THE MINIMUM SEPARATION (AS MEASURED IN THE FIELD) FROM THE CENTERLINE OF THE TRACKS TO THE EDGE OF THE TRAIL SHALL BE 8.5' AND 9.5' ALONG A STRAIGHT AND CURVED SECTION OF TRACK, RESPECTIVELY.

NOTE:
CLEAR BRUSH AND VEGETATION WITHIN
LIMITS OF DISTURBANCE AS NECESSARY TO
CONSTRUCT PROJECT IMPROVEMENTS, TYP

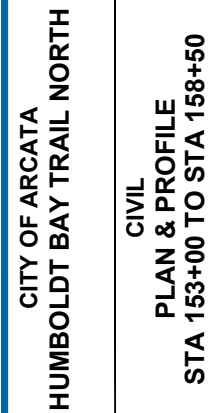


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0 1"

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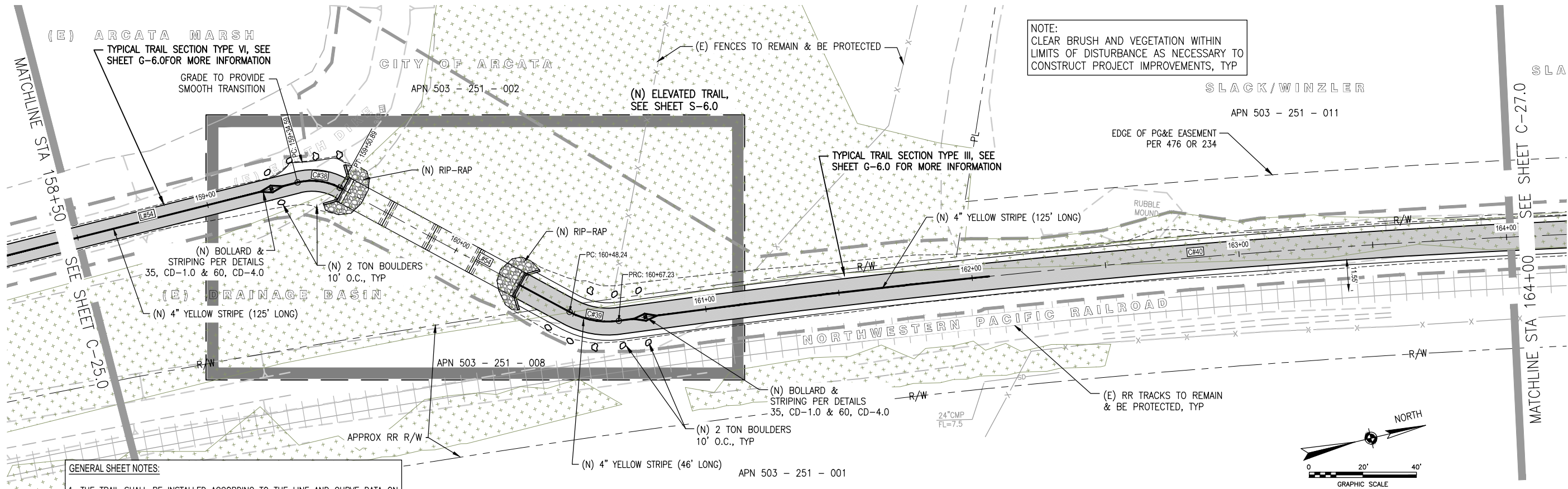
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SHEET 34 OF 52



GENERAL SHEET NOTES:

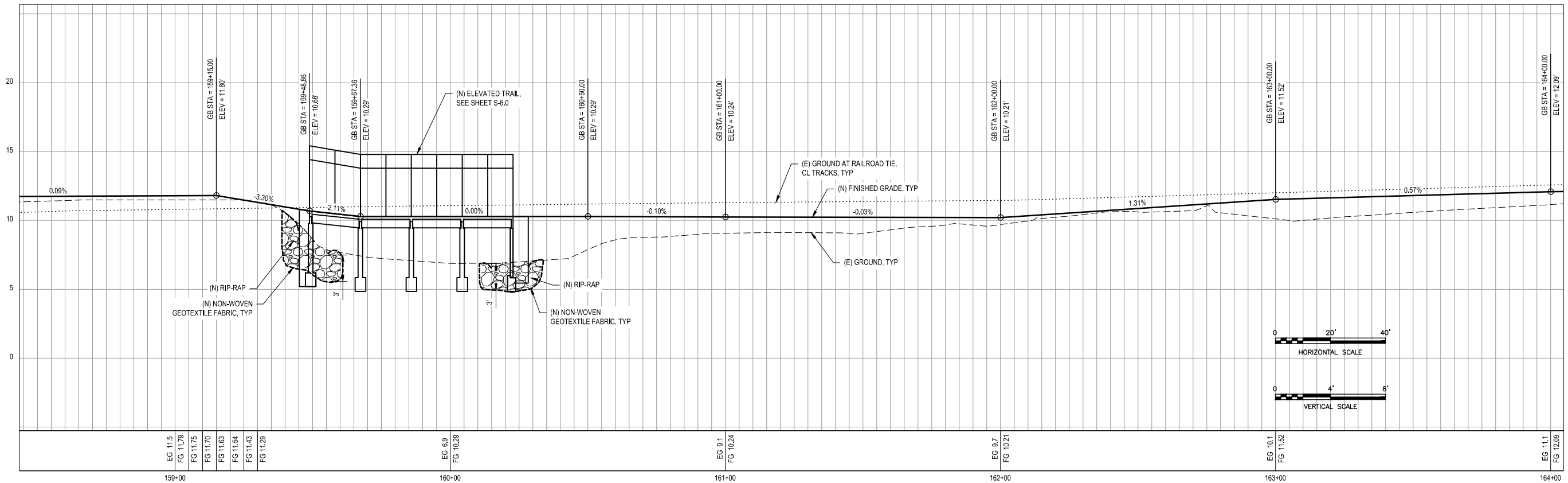
1. THE TRAIL SHALL BE INSTALLED ACCORDING TO THE LINE AND CURVE DATA ON SHEET G-9.0 EXCEPT AS NECESSARY TO MEET THE MINIMUM SEPARATION REQUIREMENTS FROM THE RAILROAD TRACKS. THE MINIMUM SEPARATION (AS MEASURED IN THE FIELD) FROM THE CENTERLINE OF THE TRACKS TO THE EDGE OF THE TRAIL SHALL BE 8.5' AND 9.5' ALONG A STRAIGHT AND CURVED SECTION OF TRACK, RESPECTIVELY.

2. (N) RR WARNING SIGNS @ 2000 FT SPACING, TO BE PROVIDED BY CITY (NIC).

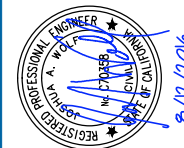
CAUTION:

IMPROVEMENTS TAKE PLACE IN THE VICINITY OF A PG&E GAS TRANSMISSION LINE. CALL UNDERGROUND SERVICE ALERT (USA) AT LEAST 48 HOURS PRIOR TO ANY EXCAVATION TO OBTAIN LOCATION AND POTHOLE TO VERIFY DEPTH OF LINE (IF REQUIRED). A PG&E STANDBY EMPLOYEE IS REQUIRED DURING ANY EXCAVATION WITHIN 10 FT OF GAS TRANSMISSION LINE.

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TRAIL - PROFILE



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0 1" 0

MARK	DATE	DESCRIPTION	ISSUE

CITY OF ARCATA

HUMBOLDT BAY TRAIL NORTH

CIVIL

PLAN & PROFILE

STA 158+50 TO STA 164+00

PROJ NO: 8411982

DRWN: OFG

CHKD: JW

C-26.0

SHEET 35 OF 52

APN 503 - 241 - 013

- 1 (N) 3 BOLLARD CLUSTER, CENTER BOLLARD IS REMOVABLE, SET 40' BACK FROM CROSSING PER DETAIL 35, CD-1.0
- 2 (N) 4" WIDE SOLID YELLOW (OBSTRUCTION) PAVEMENT STRIPING PER DETAIL 60, CD-4.0
- 3 (N) 4" WIDE SOLID YELLOW (PASSING NOT PERMITTED) PAVEMENT STRIPING BETWEEN STOP BAR & BOLLARD

- | | |
|---|--|
| 4 | (N) 12" WIDE WHITE LIMIT LINE PAVEMENT STRIPE PER CALTRANS STANDARD PLAN A24E |
| 5 | (N) BICYCLE CROSSING SIGN TYPE W11-1 W/ TYPE W16-7P UNDERNEATH PER DETAIL 36, CD-1.0 |
| 6 | REPLACE (E) AC SURFACING, SAWCUT AT JOINTS, (0.25' MIN AC) |
| 7 | (N) "YIELD" SIGN TYPE W3-2 PER DETAIL 36, CD-1.0 |

- 8 (N) 12" WIDE 10' WHITE LONGITUDINAL CROSSWALK LINE PER CALTRANS STANDARD PLAN A24E (STRIPE AT 3' OC)
- 9 (N) 10' (FULL TRAIL WIDTH) x 5' MIN DEEP CONC PAD W/ 10' (FULL TRAIL WIDTH) x 3' DEEP DETECTABLE WARNING SURFACE PER DETAIL 43, CD-2.0
- 10 (N) SIGN - "NO MOTOR VEHICLES", TYPE R5-3 PER DETAIL 36, CD-1.0
- 11 (N) DETECTABLE WARNING SURFACE (6'x3') PER DETAIL 43, CD-2.0

- (13) (N) 6' WIDE AC PATH CONNECTION TO (N) TRAIL
(0.2' AC OVER 0.5' AB)
- (14) (E) FENCE & GATE TO REMAIN & BE PROTECTED
- (15) (N) "NO PARKING" SIGN TYPE R8-3 PER DETAIL, 36, CD-1.0
- (16) (N) 4" DIAGONAL RED PAVEMENT STRIPING (STRIPES AT 3' OC
W/ 4" OUTLINE STRIPE)
- (17) (N) VAN ACCESSIBLE ADA PARKING SPACE, CONC WHEEL STOP
AND SIGNAGE PER CALTRANS STD PLAN A90A
- (18) (N) 4" WHITE PAVEMENT STRIPING
- (19) (N) 12" WIDE x 43' LONG WHITE CROSSWALK LINE PER
CALTRANS STD PLAN A24E

- 20 REMOVE (E) AC WITHIN LIMITS SHOWN, GRADE SUBGRADE TO DRAIN (2% MAX) AND RECOMPACT TO 95% RELATIVE COMPACTION. PLACE 0.25' AC AND GRADE TO DRAIN (2% MAX)
- 21 (N) WAY-FINDING SIGN (INDICATING DISTANCE AND DIRECTION TO ARCATA DOWNTOWN, ARCATA MARSH, AND PACIFIC COAST PER DETAIL 36, CD-1.0
- 22 (N) INTERPRETIVE SIGN BY CITY (INDICATING INFORMATION ABOUT WASTEWATER RECLAMATION) PER DETAIL 40, CD-4.0 (NIC)
- 23 GRADE TO DRAIN AND / OR FILL LOW AREAS TO PREVENT PONDING OF WATER



CAUTION:

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GENERAL SHEET NOTES

1. THE TRAIL SHALL BE INSTALLED ACCORDING TO THE LINE AND CURVE DATA ON SHEET G-9.0 EXCEPT AS NECESSARY TO MEET THE MINIMUM SEPARATION REQUIREMENTS FROM THE RAILROAD TRACKS. THE MINIMUM SEPARATION (AS MEASURED IN THE FIELD) FROM THE CENTERLINE OF THE TRACKS TO THE EDGE OF THE TRAIL SHALL BE 8.5' AND 9.5' ALONG A STRAIGHT AND CURVED SECTION OF TRACK, RESPECTIVELY.

2. (N) RR WARNING SIGNS @ 2000 FT SPACING, TO BE PROVIDED BY CITY (NIC).

LOCATIONS:			
NUMBER	DESCRIPTION	NORTHING	EASTING
①	EDGE STRIP	2201824.94	5983779.56
②	EDGE STRIP	2201803.80	5983798.22
③	EDGE STRIP PATH	2201813.47	5983819.74
④	SIDEWALK TRUNCATED DOME CORNER	2201856.40	5983794.02
⑤	SIDEWALK TRUNCATED DOME CORNER	2201879.60	5983772.99
⑥	SIDEWALK TRUNCATED DOME CORNER	2201863.86	5983800.75
⑦	SIDEWALK TRUNCATED DOME CORNER	2201886.72	5983780.03



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0 1"

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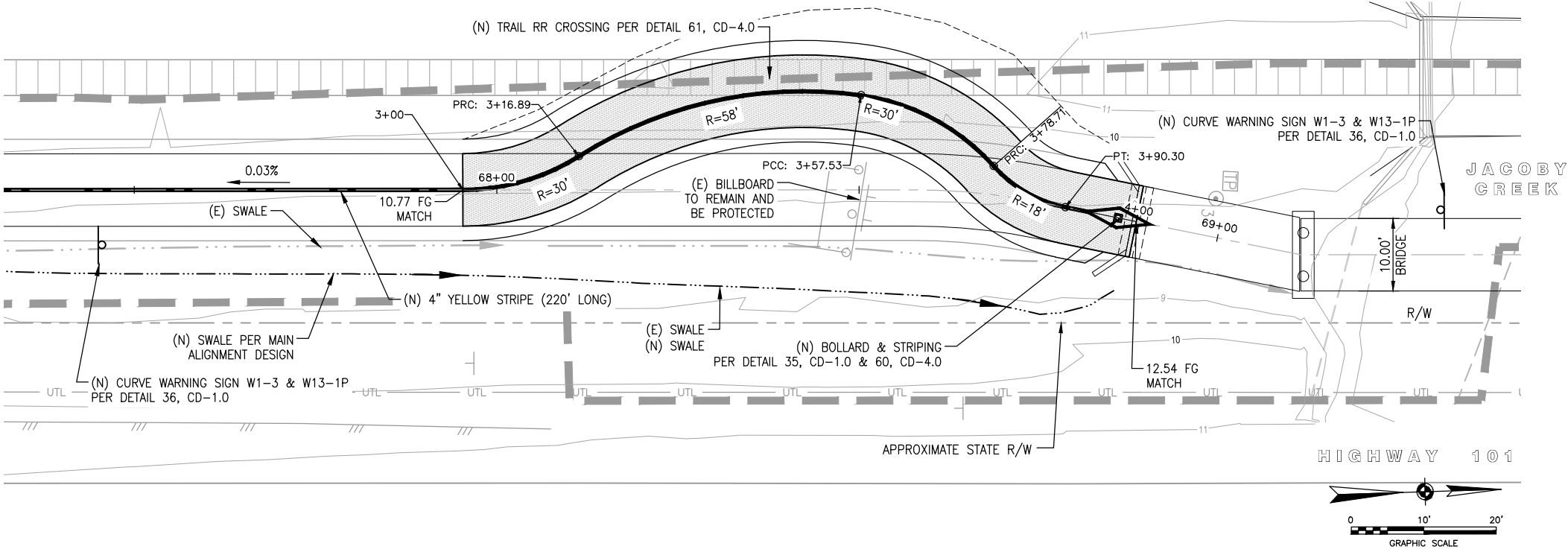
CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH

**CIVIL
INTERSECTION LAYOUT
WWTP ENTRANCE**

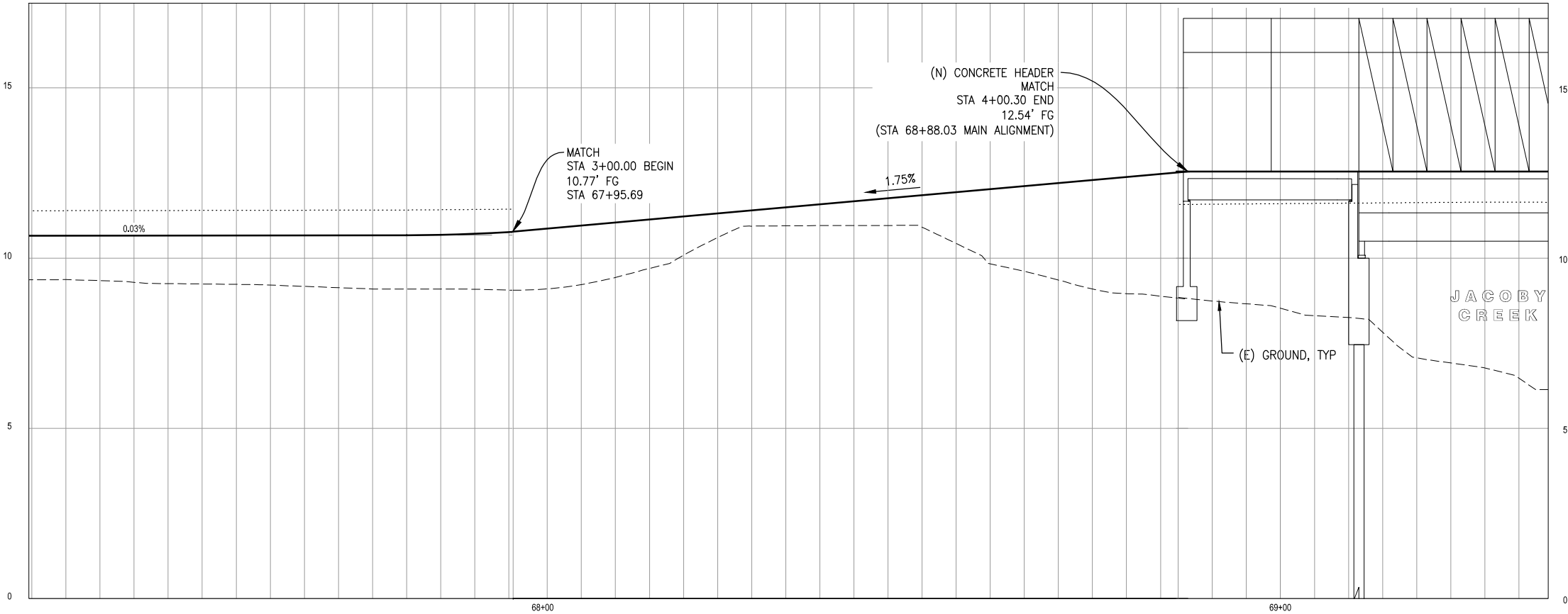
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DRWN: OFG	CHKD: JW

C-28.0

SHEET 37 OF 52



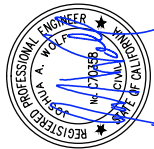
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TRAIL - PROFILE



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0 1" 20'

GRAPHIC SCALE

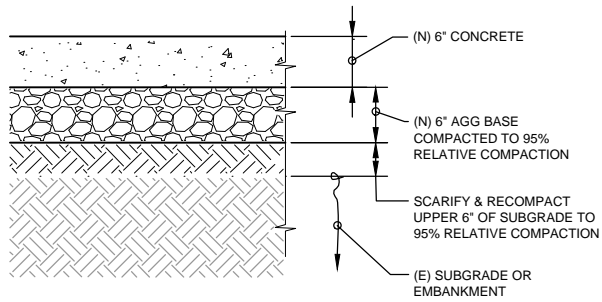
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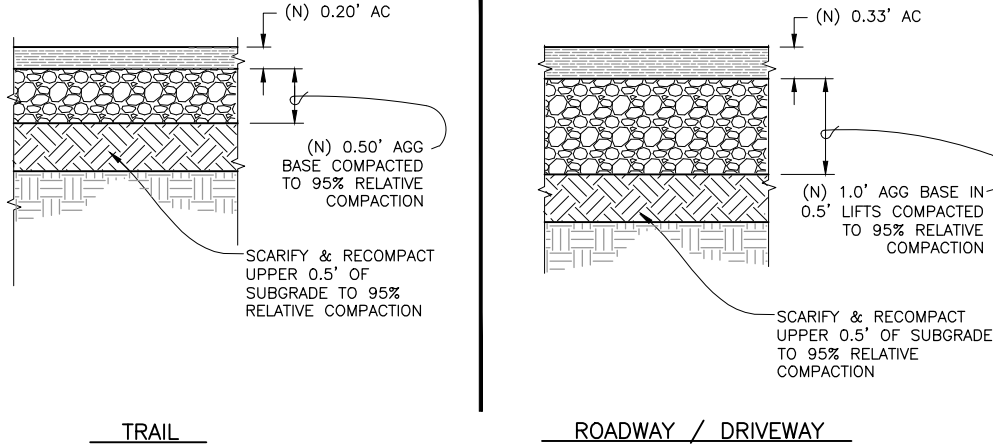
MARK	DATE	DESCRIPTION	ISSUE

CITY OF ARCATA	CIVIL
HUMBOLDT BAY TRAIL NORTH	JACOBY CREEK
	INTERIM BRIDGE APPROACH

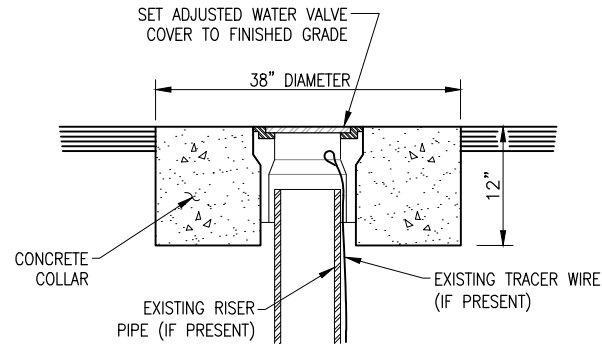
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DRWN:	CHKD:
C-30.0	
SHEET 39	OF 52



30 TYPICAL CONCRETE PAVEMENT DETAILS
TYP. CD-1.0 SCALE: N.T.S.

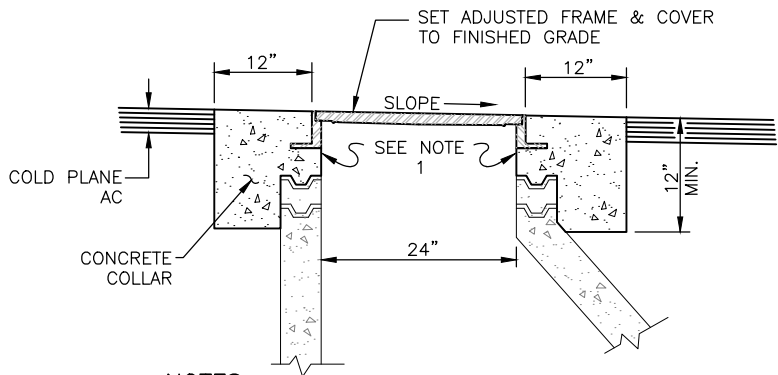


31 TYPICAL PAVEMENT STRUCTURAL SECTIONS
TYP. CD-1.0 SCALE: NTS



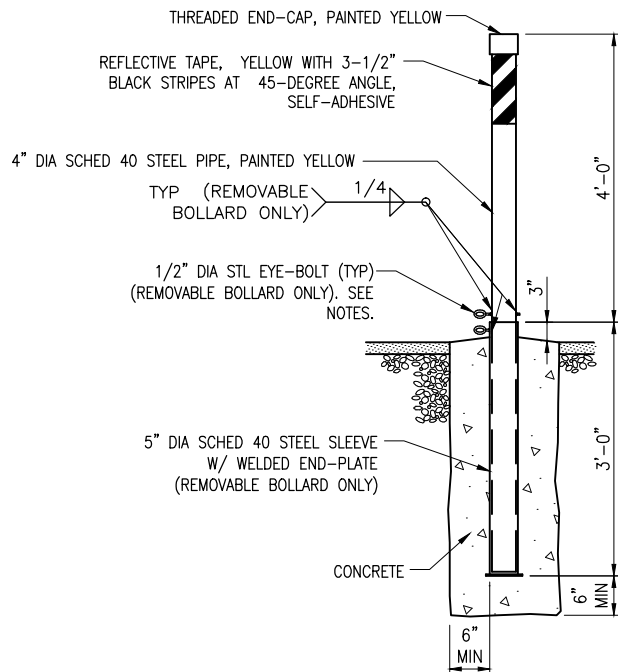
NOTES:
1. THE CONTRACTOR SHALL COMPLETELY DEMOLISH EXISTING CONCRETE COLLAR TO A DEPTH OF 12 INCHES.

34 TYPICAL ADJUST WATER VALVE COVER TO GRADE DETAIL
TYP. CD-1.0 SCALE: NTS



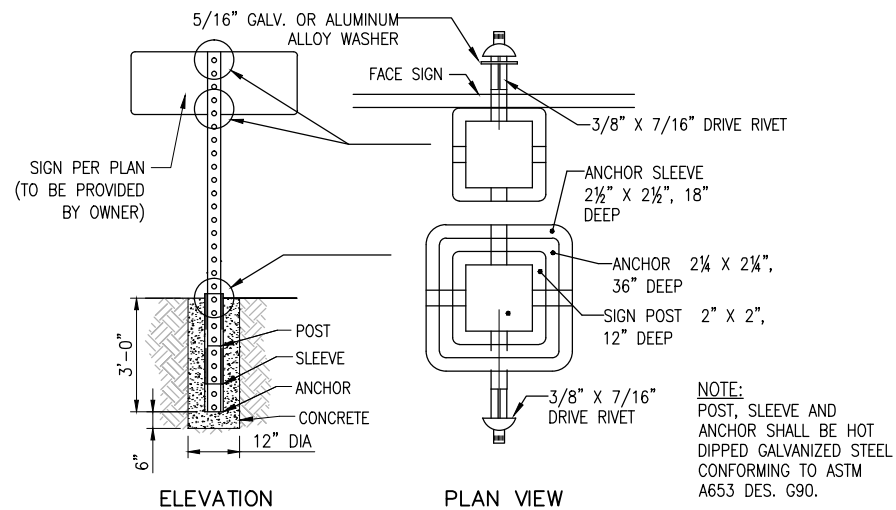
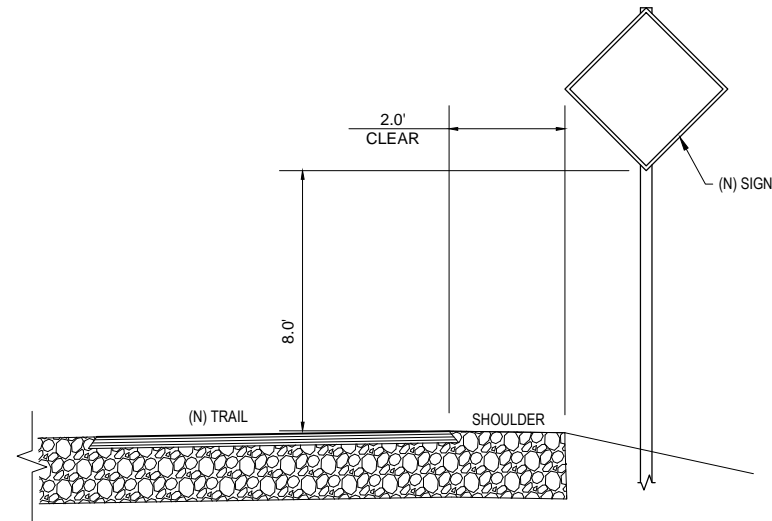
- NOTES:
- SHIMS SHALL BE PLACED UNDER THE MANHOLE FRAME & LID ON TOP OF THE HIGHEST GRADE RING AS NEEDED SUCH THAT THE FRAME & LID ARE FLUSH WITH THE FINISHED GRADE OF THE STREET. THE MINIMUM SHIM HEIGHT IS ONE INCH. CONCRETE SHALL BE PLACED IN BETWEEN THE HIGHEST GRADE RING AND THE SHIMMED FRAME & LID WHEN THE CONCRETE COLLAR IS CONSTRUCTED, IN EFFECT CREATING A 'CAST-IN-PLACE' GRADE RING OF VARYING THICKNESS.
 - ALL GRADE RINGS SHALL BE SEALED WITH NON-SHRINK GROUT ON BOTH INNER AND OUTER FACES OF EACH JOINT.

33 TYPICAL ADJUST MANHOLE TO GRADE DETAIL
TYP. CD-1.0 SCALE: NTS



- NOTES:
- DRILL 5/8 inch DIA HOLE THROUGH 4 inch DIA PIPE AND INSERT EYE-BOLT. WELD EYE-BOLT IN PLACE ON BOTH SIDES OF PIPE (REMOVABLE BOLLARDS ONLY).
 - CUT SECOND EYE-BOLT AND WELD EYE TO 5 inch DIA SLEEVE (REMOVABLE BOLLARDS ONLY).
 - EYE-BOLTS SHALL BE NO MORE THAN 1/2 inch APART TO PERMIT PAD LOCKING (REMOVABLE BOLLARDS ONLY).
 - HOT DIP GALVANIZE ALL STEEL AFTER FABRICATION.
 - ALL BOLLARDS SHALL BE PERMANENT (NON-REMOVABLE) UNLESS OTHERWISE NOTED ON PLANS.

35 TYPICAL BOLLARD DETAIL
TYP. CD-1.0 SCALE: N.T.S.

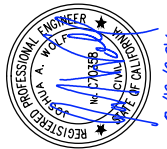


36 TYPICAL SIGN POST INSTALLATION, 1- POST DETAIL
TYP. CD-1.0 SCALE: NTS

32 NOT USED
TYP. CD-1.0 SCALE: N.T.S.



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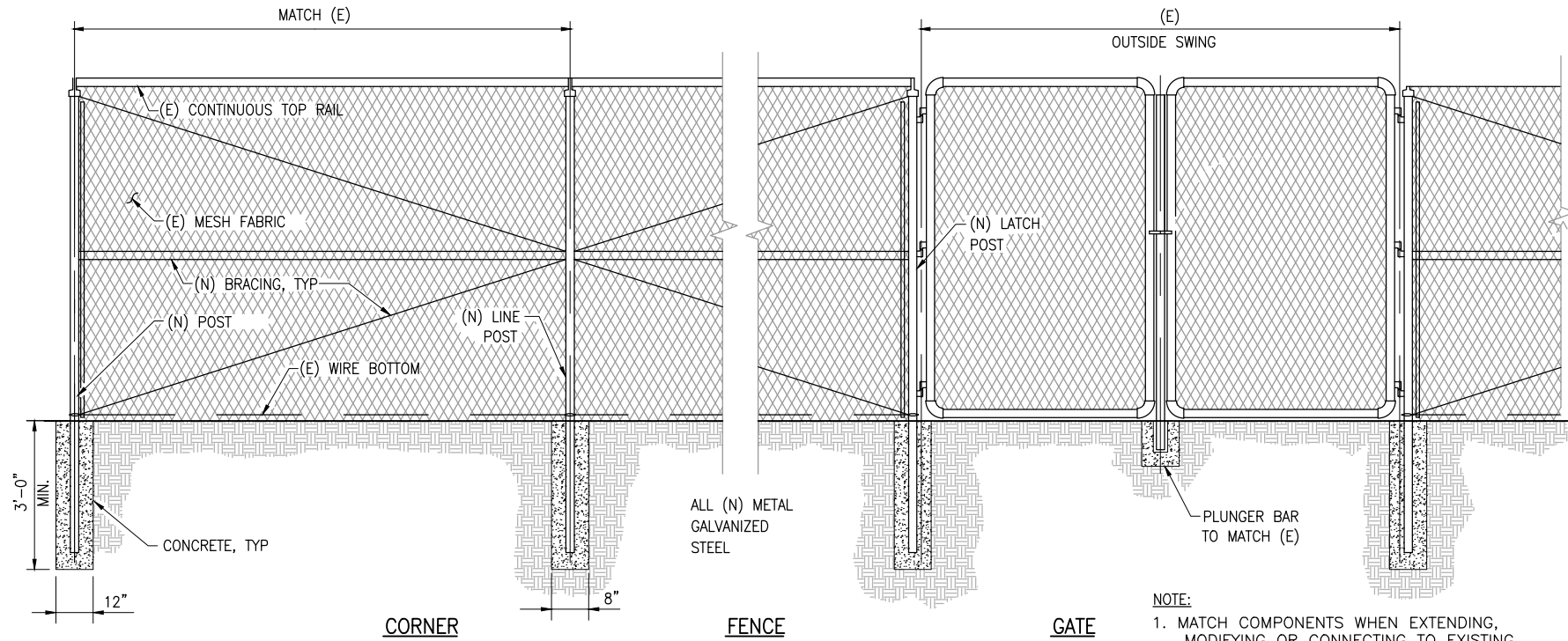
CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH

CIVIL DETAILS
STANDARD DETAILS

PROJ NO: 8411982
DRWN: OFG CHKD: JW

CD-1.0

SHEET 40 OF 52



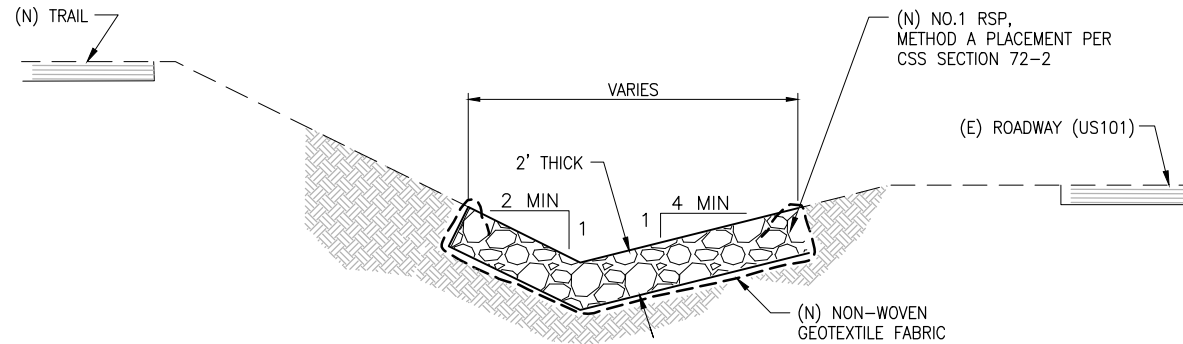
CORNER

FENCE

GATE

NOTE:
1. MATCH COMPONENTS WHEN EXTENDING,
MODIFYING OR CONNECTING TO EXISTING
(UNLESS OTHERWISE NOTED).

51 TYPICAL RELOCATE CHAIN LINK FENCE AND GATE DETAIL
TYP. CD-3.0 SCALE: N.T.S.



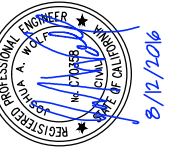
53 TYPICAL ROCK LINED DITCH DETAIL
TYP. CD-3.0 SCALE: NTS

52 NOT USED
TYP. CD-3.0 SCALE: NTS

54 NOT USED
TYP. CD-3.0 SCALE: NTS



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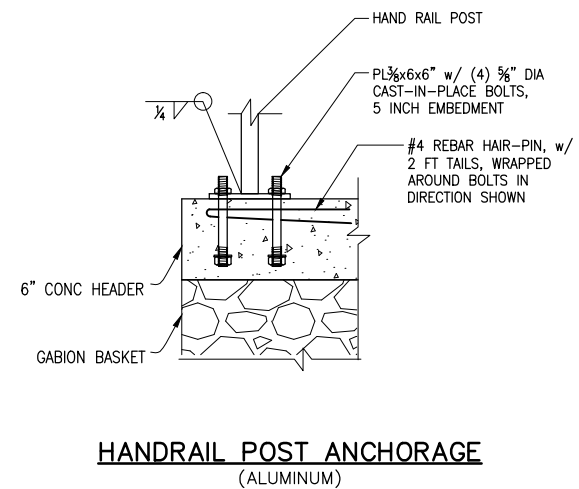
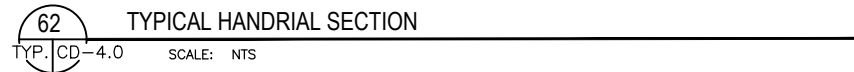
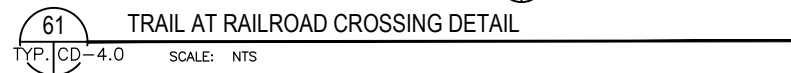
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CIVIL DETAILS
STANDARD DETAILS

PROJ NO: 8411982
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CD-3.0

SHEET 42 OF 52



- ## 1. DESIGN BASIS

- 1.1. THIS STRUCTURE HAS BEEN DESIGNED IN ACCORDANCE WITH THE CALTRANS BRIDGE DESIGN SPECIFICATIONS AND GEOTECHNICAL REPORT "ARCATA RAIL WITH TRAIL CONNECTIVITY PROJECT, ARCATA, CALIFORNIA" BY BLACKBURN CONSULTING. REFER TO THIS REPORT FOR ALL GEOTECHNICAL INFORMATION AND RECOMMENDATIONS.
- 1.2. STRUCTURE DESIGNED FOR SEISMIC LOADS BASED ON THE FOLLOWING PARAMETERS:
ARS CURVE PER GEOTECHNICAL REPORT
- 1.3. DESIGN LIVE LOADS: 90 PSF PEDESTRIAN LIVE LOAD

2. GENERAL NOTES

- 2.1. ALL WORK TO CONFORM TO REQUIREMENTS OF ALL PUBLICATIONS AND NOTES LISTED UNDER "DESIGN BASIS".
- 2.2. CIVIL DRAWINGS AND ALL OTHER DRAWINGS AS REQUIRED SHALL BE USED IN CONJUNCTION WITH STRUCTURAL DRAWINGS TO DEVELOP DETAILS AND DIMENSIONS FOR SHOP DRAWINGS, FABRICATION, ERECTION AND CONSTRUCTION.
- 2.3. THE CONTRACTOR IS TO VERIFY ALL EXISTING CONDITIONS AND ALL DIMENSIONS IN FIELD PRIOR TO START OF CONSTRUCTION AND PROTECT AND MAINTAIN ALL EXISTING CONSTRUCTION AND ITS CONTENTS IN FULL.
- 2.4. THE CONTRACTOR SHALL MAINTAIN A SET OF LATEST REVIEWED SHOP DRAWINGS ON JOB SITE.
- 2.5. THE STRUCTURE HAS BEEN DESIGNED TO BE STABLE AND SELF SUPPORTING AFTER THE CONSTRUCTION IS COMPLETE. IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY FOR THE STRUCTURES STABILITY DURING CONSTRUCTION. THIS RESPONSIBILITY ALSO INCLUDES BUT IS NOT LIMITED TO METHOD AND SEQUENCE OF ERECTION, TEMPORARY SHORING AND TEMPORARY BRACING.
- 2.6. IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO FOLLOW ALL APPLICABLE SAFETY CODES AND REGULATIONS DURING ALL PHASES OF CONSTRUCTION.
- 2.7. SHOULD ANY INFORMATION ON THE STRUCTURAL DRAWINGS CONFLICT WITH THE SPECIFICATIONS OR ANY OTHER PART OF THE DRAWINGS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND AN INTERPRETATION WILL BE GIVEN.
- 2.8. ALL SECTIONS, DETAILS, NOTES, DIMENSIONS AND CONDITIONS ARE APPLICABLE AT ANY OTHER LOCATION WHERE CONDITIONS AND DETAILS ARE SIMILAR BUT ARE NOT SPECIFICALLY NOTED AS SUCH OR ARE NOT SHOWN.
- 2.9. FOR QUALITY CONTROL INSPECTION AND TESTING REQUIREMENTS DURING CONSTRUCTION, SEE SPECIFICATIONS AND "SCHEDULE OF SPECIAL INSPECTIONS".

3. FOUNDATION NOTES

- 3.1. SOIL BEARING CAPACITY USED IN THE DESIGN OF FOUNDATIONS:
2000 PSF AS RECOMMENDED BY THE GEOTECHNICAL REPORT BY BCI INC. JUNE 2010.
- 3.2. ALL FOOTINGS TO BEAR ON NATURAL UNDISTURBED SOIL OR COMPACTED STRUCTURAL FILL HAVING MINIMUM BEARING CAPACITY AS INDICATED.
- 3.3. ALL COMPACTED STRUCTURAL FILL SHALL CONFORM TO ALL RECOMMENDATIONS CONTAINED IN THE PROJECT GEOTECHNICAL REPORT.
- 3.4. PLACEMENT OF ALL COMPACTED FILL AND COMPACTION OF SUBGRADE SHALL BE UNDER FULL TIME DIRECTION OF A QUALIFIED GEOTECHNICAL ENGINEER. CONCRETE SLABS AND FOOTINGS SHALL NOT BE PLACED UNTIL FILL AND SUBGRADE HAVE BEEN CHECKED IN PLACE AND APPROVED BY THE GEOTECHNICAL ENGINEER.
- 3.5. IF VERY SOFT "BAY MUDS" OR ORGANICS ARE PRESENT BELOW BRIDGE FOUNDATIONS OR WHERE COMPACTION IS UNACHIEVABLE DUE TO PUMPING SOILS, A LAYER OF GEOGRID, (TENSAR BX1200 OR EQUIVALENT) SHALL BE PLACED BEFORE PLACING FILL.

4. CONCRETE NOTES

- 4.1. ALL WORK TO CONFORM TO THE REQUIREMENTS OF THE FOLLOWING PUBLICATIONS:
 - 4.1.1. ACI "BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE" (ACI 318—LATEST EDITION) AND "DETAILS AND DETAILING OF CONCRETE REINFORCEMENT" (ACI 315—LATEST EDITION).
 - 4.1.2. "SPECIFICATION FOR WELDED STEEL WIRE FABRIC FOR CONCRETE REINFORCEMENT" (LATEST EDITION) BY THE WIRE REINFORCEMENT INSTITUTE, INC.
 - 4.2. MINIMUM COMPRESSIVE STRENGTH OF CONCRETE AT 28 DAYS:
 - 5000 PSI (NORMAL WEIGHT) AT PILES
 - 4000 PSI (NORMAL WEIGHT) AT ALL OTHER ELEMENTS
- PRIOR TO PLACING CONCRETE, MIX DESIGNS SHALL BE SUBMITTED FOR REVIEW.

- 4.3. ALL BAR REINFORCING FOR CONCRETE SHALL CONFORM TO ASTM A 615 GRADE 60 (DEFORMED). REINFORCING SHOWN ON DRAWING TO BE WELDED SHALL CONFORM TO ASTM A 706 GRADE 60 (DEFORMED) BARS SPECIALLY FORMULATED TO BE WELDABLE.
- 4.4. WELDED WIRE FABRIC SHALL CONFORM TO ASTM A 1064.
- 4.5. UNLESS OTHERWISE SHOWN, LOCATE REINFORCING BARS WITH FOLLOWING CLEAR DIMENSION TO FACE OF CONCRETE:

CONCRETE CAST ON GROUND: 3" CLEAR

CONCRETE TOPPING SLAB AT PRE-CAST BRIDGES: 1½" CLEAR

ALL OTHER LOCATIONS: 2" CLEAR, UNO
- 4.6. CONCRETE ACCESSORIES SHALL BE ADEQUATE TO MAINTAIN REINFORCING ACCURATELY IN PLACE AND BE NON-CORROSIVE, NON-STAINING TYPE.
- 4.7. LAP ALL BAR REINFORCING IN CONCRETE ELEMENTS AS SHOWN IN "REINFORCING CONCRETE LAP SCHEDULE". IN BEAMS, GRADE BEAMS AND GIRDERS, SPLICE TOP BARS AT MIDDLE OF SPAN BETWEEN SUPPORTS AND SPLICE BOTTOM BARS AT SUPPORTS (UNLESS OTHERWISE SHOWN).
- 4.8. SAW-CUT CONTROL JOINTS IMMEDIATELY AFTER CONCRETE HAS SET SUFFICIENTLY SO THAT CUTTING DOES NOT PRODUCE SHREDDING OF THE CONCRETE, BUT BEFORE CONCRETE HAS HAD A CHANCE TO CRACK DUE TO INITIAL SHRINKAGE. THE CUTTING PERIOD WILL VARY ACCORDING TO THE RATE OF SETTING OF THE CONCRETE. IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO CUT THESE JOINTS AT THE PROPER TIME AND USING THE PROPER PROCEDURE TO MINIMIZE SHRINKAGE CRACKING AND TO PRODUCE CLEAN, STRAIGHT JOINTS.
- 4.9. REFERENCE SPECIFICATIONS FOR CONCRETE CURING AND PROTECTION. BEGIN CONCRETE CURING AS SOON AS FINISHING OPERATIONS ARE COMPLETE (WITHIN TWO HOURS).
- 4.10. SHORE CONCRETE BEAMS, CANTILEVERS AND SUPPORTED SLABS DIRECTLY TO FIRM BEARING FOR AS LONG AS PRACTICAL (MINIMUM 28 DAYS) TO MINIMIZE CREEP AND DEFLECTION.
- 4.11. LIQUID MEMBRANE FORMING CURING COMPOUNDS SHALL COMPLY WITH ASTM C 309, TYPE I, CLASS A.

5. STEEL NOTES

- 5.1. ALL WORK TO CONFORM TO THE REQUIREMENTS OF THE FOLLOWING PUBLICATIONS:
 - 5.1.1. AISC 360-05 "DESIGN SPECIFICATION FOR STRUCTURAL STEEL BUILDINGS" (MANUAL OF STEEL CONSTRUCTION - 13TH EDITION)
- 5.2. ALL STEEL TUBES, ROUND AND SHAPED, SHALL CONFORM TO ASTM A500, GRADE B.
- 5.3. ALL STEEL PILINGS SHALL CONFORM TO ASTM A252.
- 5.4. ALL PLATES AND MISCELLANEOUS IRON SHALL CONFORM TO ASTM A36 (EXCEPT AS NOTED).
- 5.5. ALL BOLTED CONNECTIONS TO USE 3/4" DIAMETER A307 BOLTS UNLESS NOTED OTHERWISE.
- 5.6. ALL WELDS TO STRUCTURAL STEEL TO CONFORM TO "STRUCTURAL WELDING CODE - STEEL" (AWS D1.1 - LATEST EDITION). ALL WELDS TO COLD-FORMED SHEET STEEL TO CONFORM TO "STRUCTURAL WELDING CODE - SHEET STEEL" (AWS D1.3- LATEST EDITION).
- 5.7. ALL STEEL EXCEPT PILINGS SHALL BE GALVANIZED AND THEN PAINTED. STEEL PILINGS FILLED WITH CONCRETE SHALL BE PAINTED ABOVE THE MUD LINE.
- 5.8. GALVANIZING SHALL CONFORM TO ASTM A 123 OR ASTM A 153, AS APPLICABLE.
- 5.5. FIELD WELDING TO GALVANIZED STEEL: PRIOR TO FIELD WELDING CONNECTION, ZINC COATING AT ALL WELD CONNECTION AREAS SHALL BE REMOVED BY BURNING WITH OXYGEN FUEL GAS TORCH, SHOT BLASTING OR GRINDING TO BARE STEEL. APPLY A MINIMUM OF 2 COATS OF ZINC-RICH PAINT AFTER CLEANING COMPLETED WELD.

6. PRE-MANUFACTURED BRIDGE

- 6.1. PROVIDE AND INSTALL FULLY FUNCTIONAL PRE-MANUFACTURED BRIDGE PER THE STRUCTURAL DRAWINGS AND SPECIFICATIONS.
- 6.2. BRIDGES TO BE DESIGNED BY A CALIFORNIA PROFESSIONAL ENGINEER AND CONSTRUCTED IN ACCORDANCE WITH SPECIFICATIONS AND TO SUPPORT OWN WEIGHT, IMPOSED LIVE, WIND, AND SEISMIC LOADS. SEE "DESIGN BASIS" IN STRUCTURAL GENERAL NOTES FOR STRUCTURAL CRITERIA. SUBMIT DESIGN FOR REVIEW PRIOR TO FABRICATION. CONTRACTOR TO INCLUDE MULTIPLE STRUCTURAL REVIEWS IN PROJECT SCHEDULE. ALLOW 2 WEEKS FOR EACH REVIEW.

CONCRETE REINFORCEMENT								
CLASS B LAP SPLICE SCHEDULE (inches)								
F _c (psi)	#3	#4	#5	#6	#7	#8	#9	#10
2800	23	31	39	47	68	78	88	99
3000	21	28	36	43	62	71	80	90
4000	18	25	31	37	54	62	70	78
5000	17	22	28	33	48	55	62	70
6000	15	20	25	30	44	50	57	64


BASE FLOOD ELEVATION NOTE:
100 YR WATER SURFACE ELEVATION OF HUMBOLDT BAY ESTIMATED TO BE 10.2 FT
NAVD 88 BASED ON PRELIMINARY RESULTS OF FEMA FLOOD MODEL PROVIDED TO GHD BY
THE CITY OF ARCATA (1/21/2016). FOR PURPOSES OF DESIGN THE SOFFIT ELEVATION
WAS ROUNDED UP TO 10.5 FT.



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**CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH**

STRUCTURAL

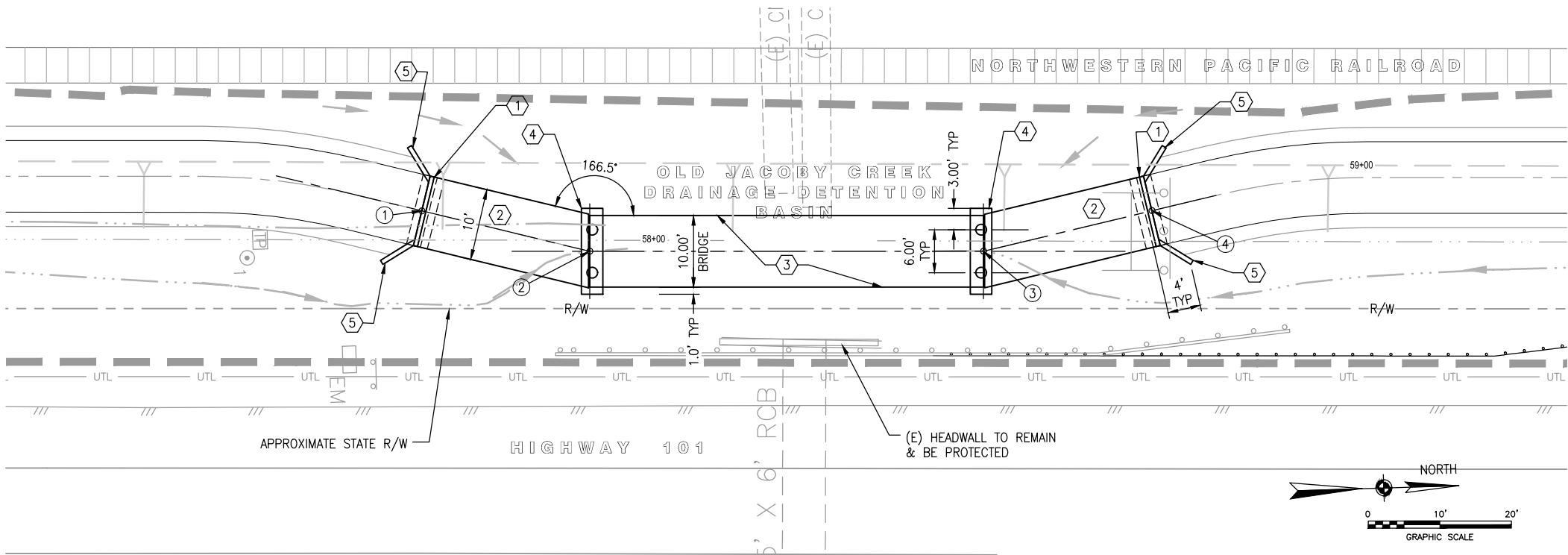
GENERAL NOTES

PROJ NO: 8411982

DRWN:	CHKD:
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BRIDGE SITE PLAN

NOTES:

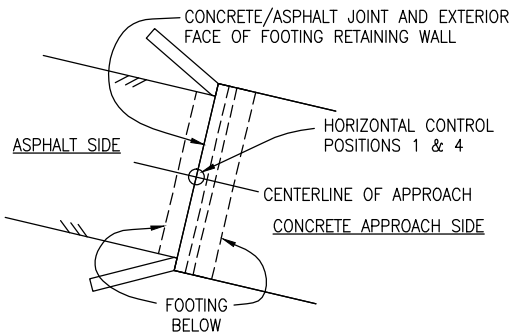
- DIMENSIONS SHOWN ARE TYPICAL. APPROACH SPANS AND FOOTINGS ARE SYMMETRICAL ABOUT CENTER LINE OF TRAIL.
- THE BRIDGE SOFFIT SHALL BE SET AT OR ABOVE ELEVATION 10.5 FT (NAVD 88).

LOCATIONS:

NUMBER	DESCRIPTION	NORTHING	EASTING
①	CL OF APPROACH (SEE FOOTING DIAGRAM)	2196227.07	5985611.48
②	CL OF BRIDGE (SEE PILE CAP DIAGRAM)	2196250.77	5985617.61
③	CL OF BRIDGE (SEE PILE CAP DIAGRAM)	2196330.73	5985620.14
④	CL OF APPROACH (SEE FOOTING DIAGRAM)	2196354.77	5985615.51

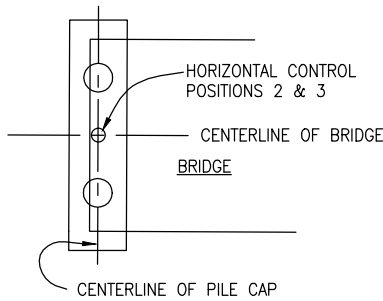
KEYNOTES:

- (N) CAST-IN-PLACE CONC RETAINING FTG PER DETAIL TYPIS-7.0
- (N) PRECAST CONC BRIDGE DECKS W/ PEDESTRIAN GUARDRAILS AND CONCRETE OVERLAY, TYP TYPIS-8.0
- (N) PRE-MANUFACTURED 55 FT LONG BY 10 FT WIDE ALUMINUM BRIDGE W/ PEDESTRIAN GUARDRAILS AND ALUMINUM DECKING TYPIS-8.0
- (N) CAST-IN-PLACE PILE CAP ON DRIVEN PILES PER TYPIS-7.0
- (N) CAST-IN-PLACE WING WALL PER DETAIL TYPIS-8.0
- (N) 18" STEEL SHELLLED PILE PER TYPIS-7.0



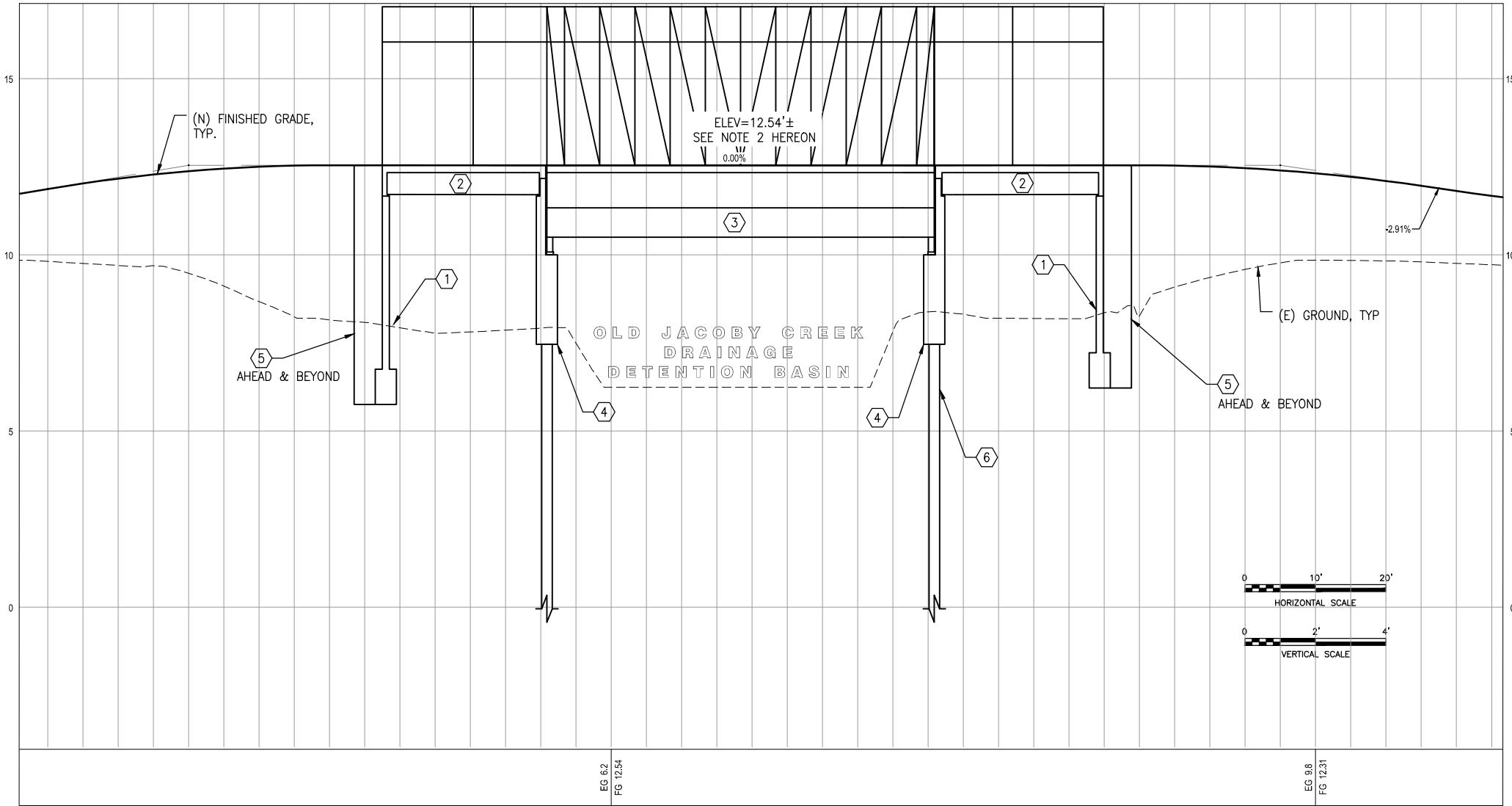
TYPICAL APPROACH FOOTING DIAGRAM

SCALE: NTS



TYPICAL PILE CAP DIAGRAM

SCALE: NTS



BRIDGE PROFILE



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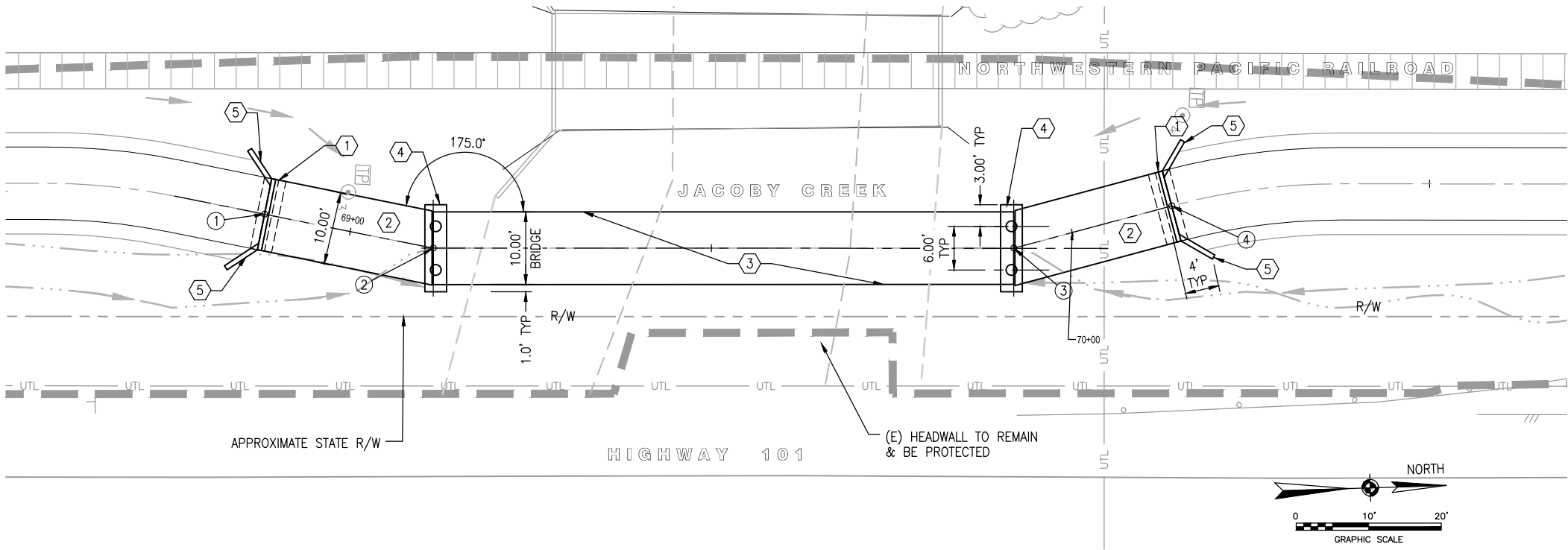
CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH
STRUCTURAL
OLD JACOBY CREEK CROSSING
SITE PLAN & PROFILE

PROJ NO: 8411982

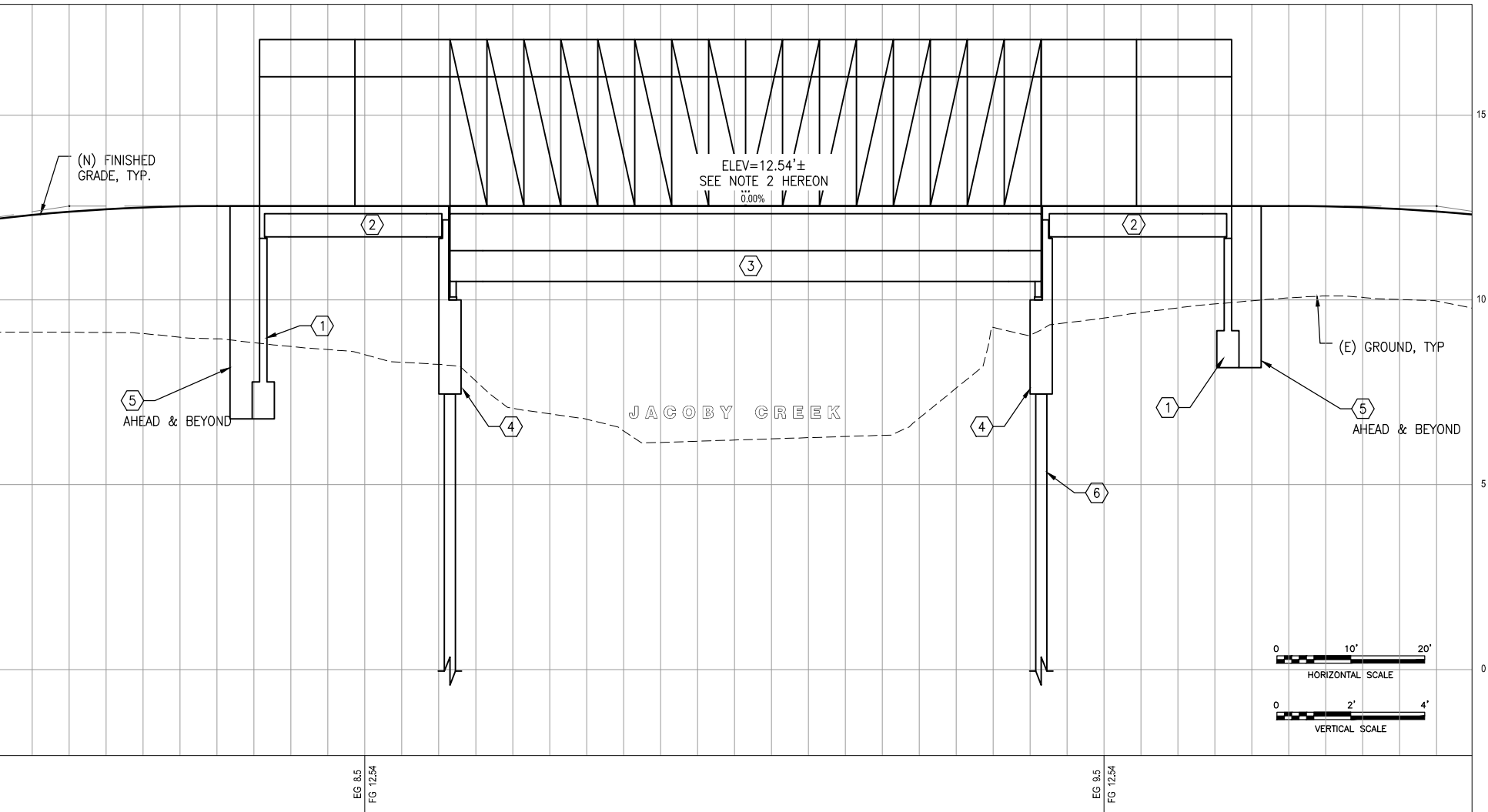
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BRIDGE SITE PLAN



BRIDGE PROFILE

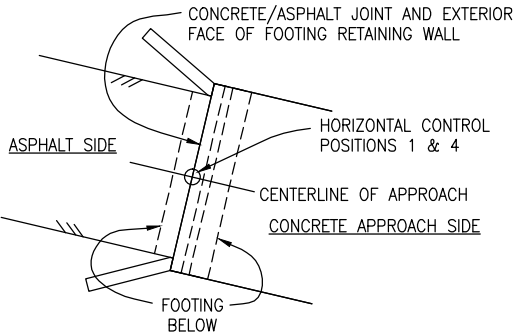
- NOTES:
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LOCATIONS:

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②	CL OF BRIDGE (SEE PILE CAP DIAGRAM)	2196250.77	5985617.61
③	CL OF BRIDGE (SEE PILE CAP DIAGRAM)	2196330.73	5985620.14
④	CL OF APPROACH (SEE FOOTING DIAGRAM)	2196354.77	5985615.51

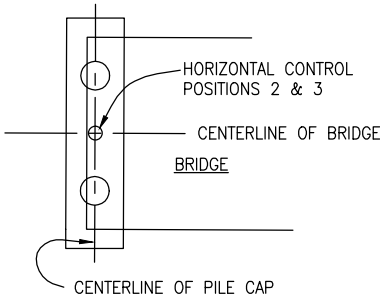
KEYNOTES:

- (N) CAST-IN-PLACE CONC RETAINING FTG PER DETAIL ① TYP S=7.0
- (N) PRECAST CONC BRIDGE DECKS W/ PEDESTRIAN GUARDRAILS AND CONCRETE OVERLAY, TYP ② TYP S=8.0
- (N) PRE-MANUFACTURED 80 FT LONG BY 10 FT WIDE ALUMINUM W/ PEDESTRIAN GUARDRAILS AND ALUMINUM DECKING ③ TYP S=7.0
- (N) CAST-IN-PLACE PILE CAP ON PRECAST DRIVEN PILES PER ④ TYP S=7.0
- (N) CAST-IN-PLACE WING WALL PER DETAIL ⑤ TYP S=8.0
- (N) 18" STEEL SHELLLED PILE PER ⑥ TYP S=7.0



TYPICAL APPROACH FOOTING DIAGRAM

SCALE: NTS



TYPICAL PILE CAP DIAGRAM

SCALE: NTS



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CITY OF ARCATA
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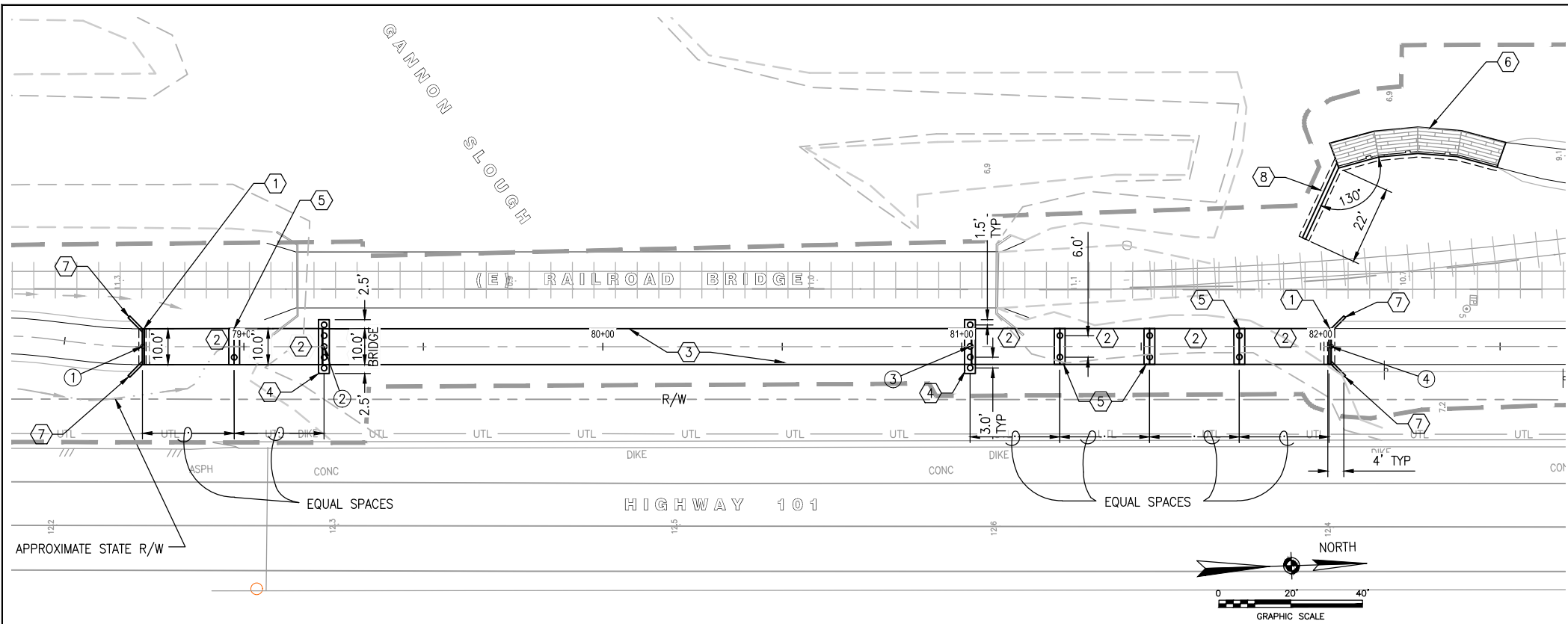
STRUCTURAL
JACOBY CREEK CROSSING
SITE PLAN & PROFILE

PROJ NO: 8411982

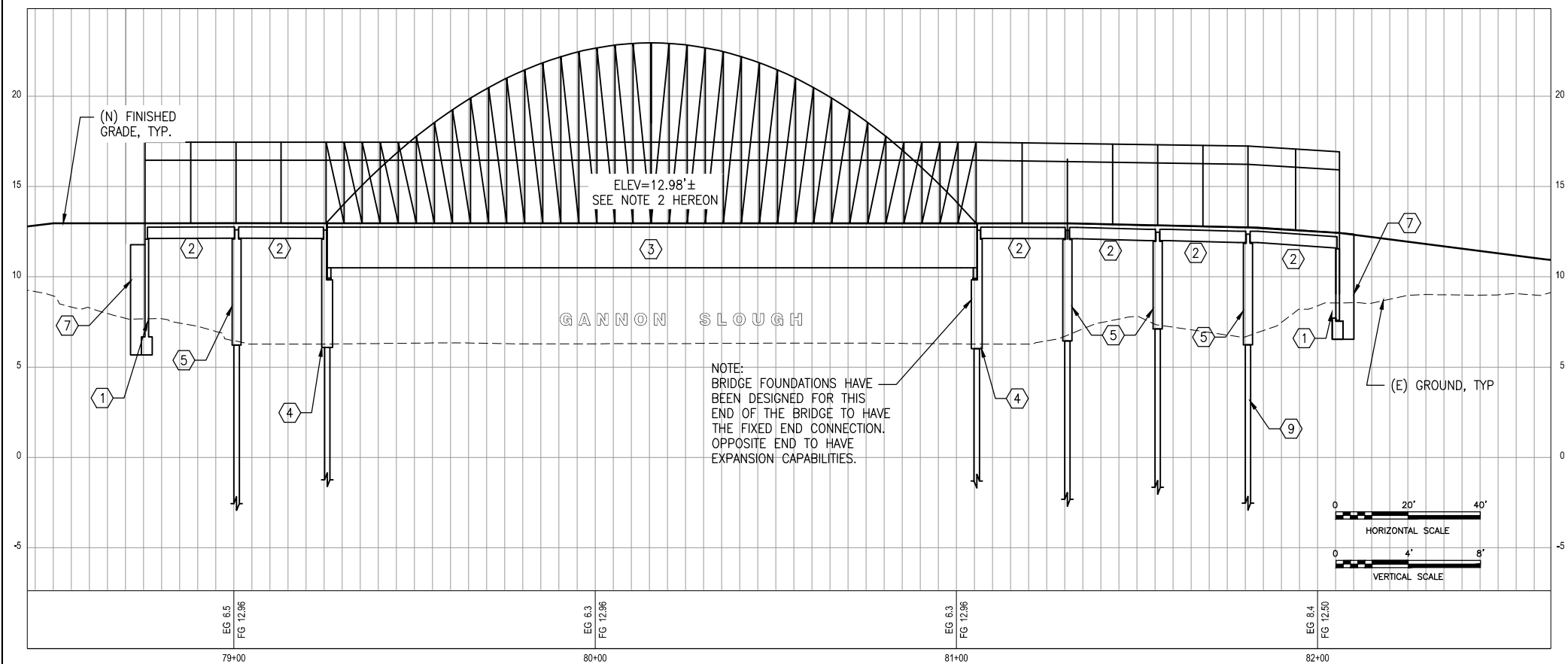
DRWN: CHKD:

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BRIDGE SITE PLAN



BRIDGE PROFILE

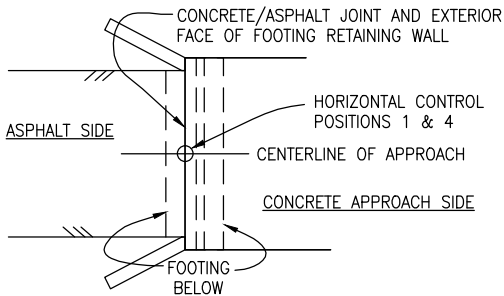
- NOTES:
- DIMENSIONS SHOWN ARE TYPICAL. APPROACH SPANS & FOOTINGS ARE SYMMETRICAL ABOUT CENTER LINE OF TRAIL.
 - THE BRIDGE SOFFIT SHALL BE SET AT OR ABOVE ELEVATION 10.5 FT (NAVD 88).

LOCATIONS:

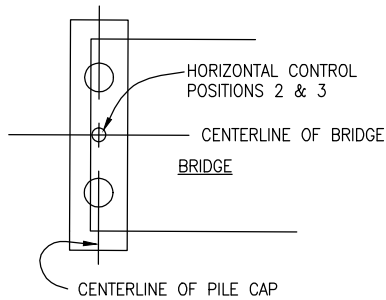
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③	CL OF BRIDGE (SEE PILE CAP DIAGRAM)	2198570.55	5985681.14
④	CL OF APPROACH (SEE FOOTING DIAGRAM)	2198671.09	5985684.28

KEYNOTES:

- ① (N) CAST-IN-PLACE CONC RETAINING FTG PER DETAIL ① TYP S-7.0
- ② (N) PRECAST CONC BRIDGE DECKS W/ PEDESTRIAN GUARDRAILS AND CONCRETE OVERLAY, TYP ② TYP S-8.0
- ③ (N) PRE-MANUFACTURED 180 FT LONG BY 10 FT WIDE STEEL BRIDGE W/ PEDESTRIAN GUARDRAILS AND AC SURFACE ③ TYP S-8.0
- ④ (N) CAST-IN-PLACE PILE CAP ON DRIVEN PILES PER ③ TYP S-7.0
- ⑤ (N) CAST-IN-PLACE PILE CAP ON DRIVEN PILES PER ② TYP S-7.0
- ⑥ (N) GANNON SLOUGH OVERLOOK PER S-4.0 S-9.0 ⑥ TYP S-9.0
- ⑦ (N) CAST-IN-PLACE WING WALL PER DETAIL ⑤ TYP S-8.0
- ⑧ (N) CAST-IN-PLACE FTG SIMILAR TO DETAIL EXCEPT DECK NOTCH ③ TYP S-9.0
- ⑨ (N) 18" STEEL SHELL PILE PER ⑥ TYP S-7.0



TYPICAL APPROACH FOOTING DIAGRAM
SCALE: NTS



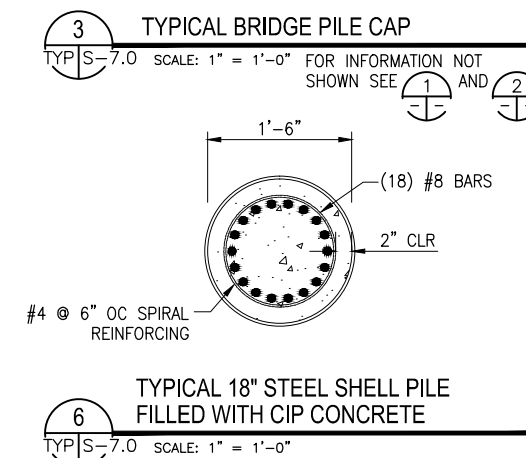
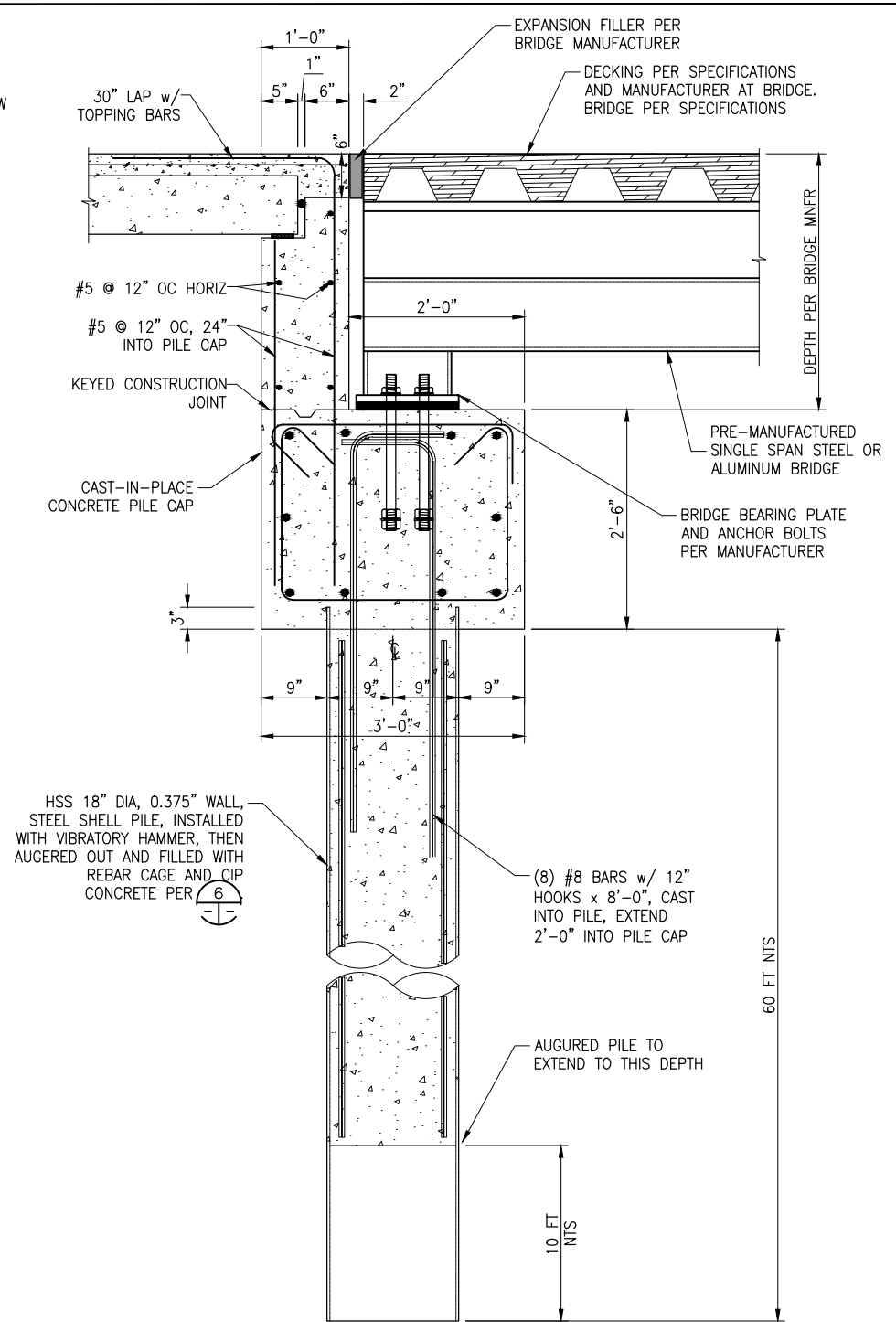
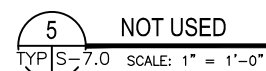
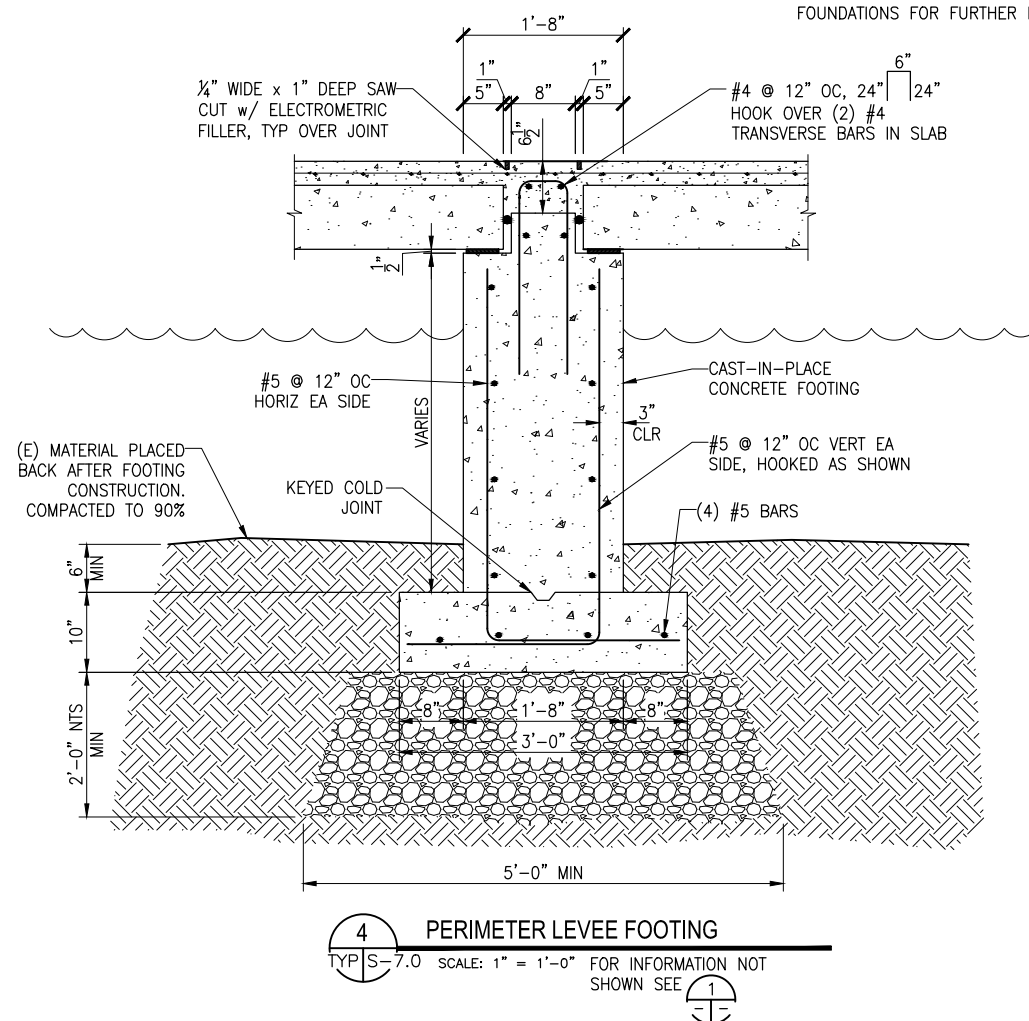
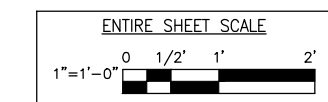
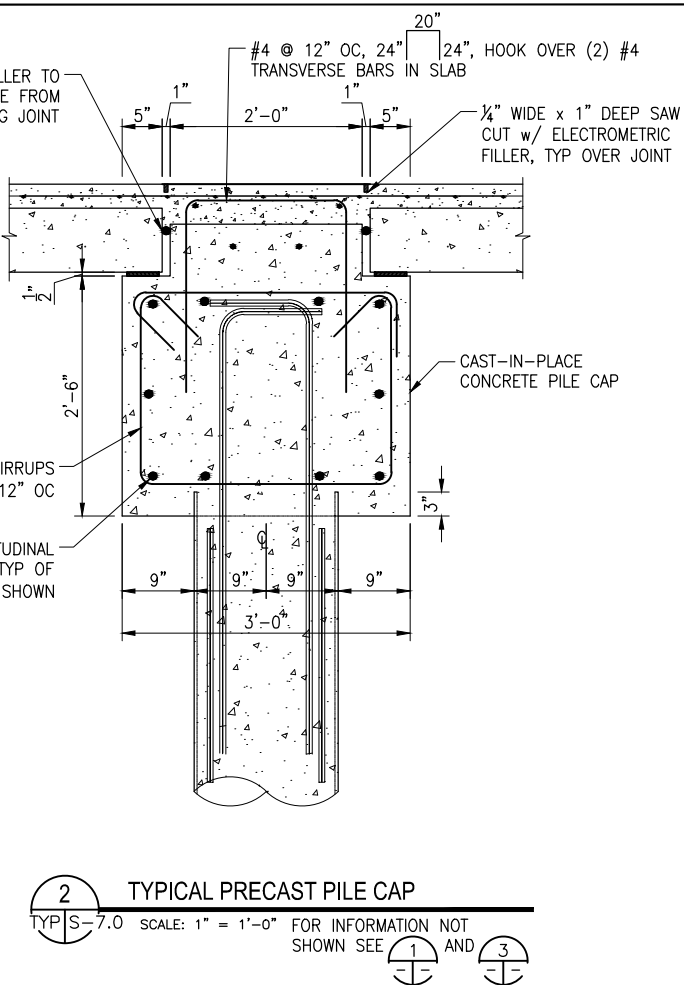
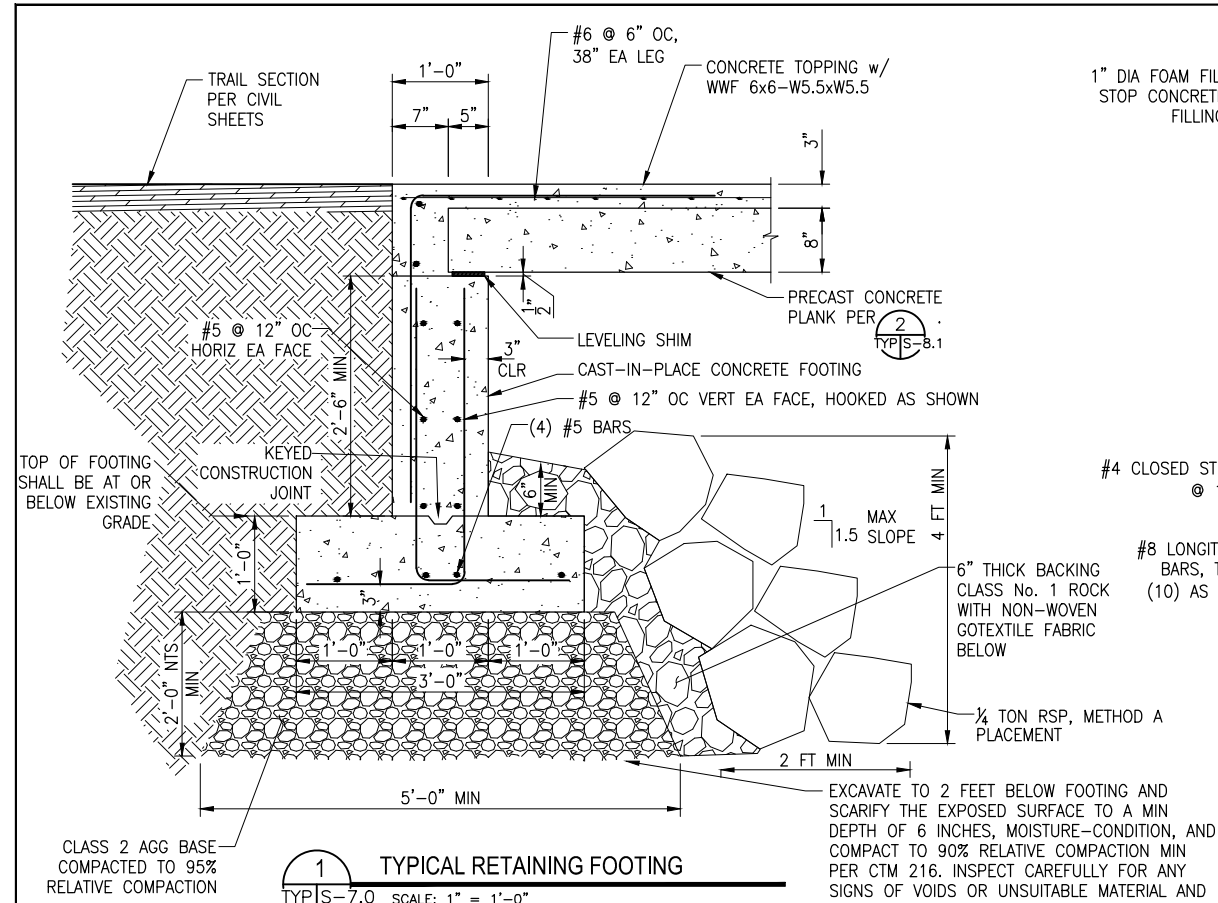
TYPICAL PILE CAP DIAGRAM
SCALE: NTS

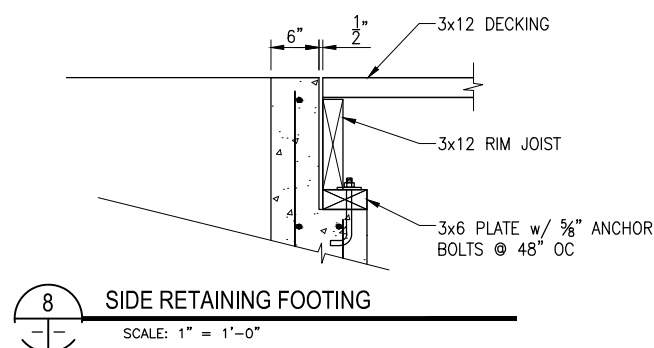
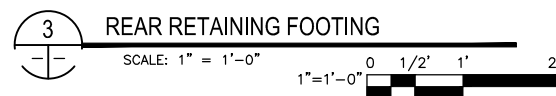
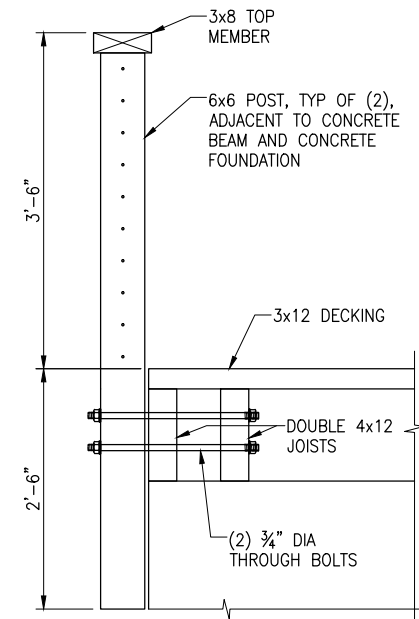
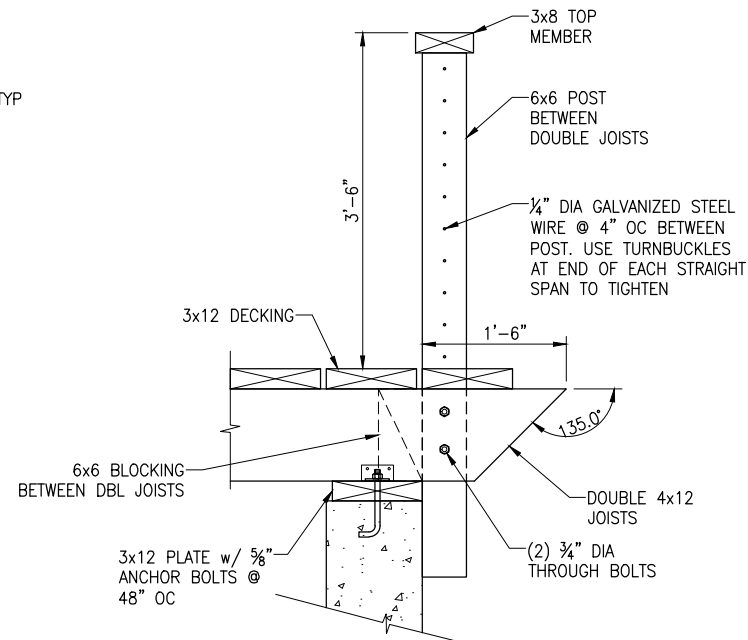
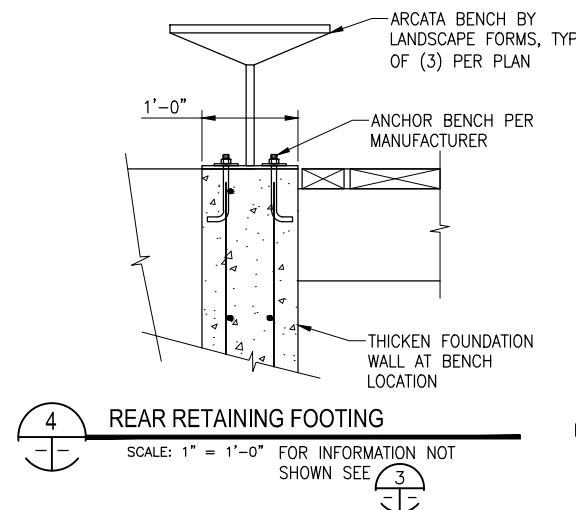
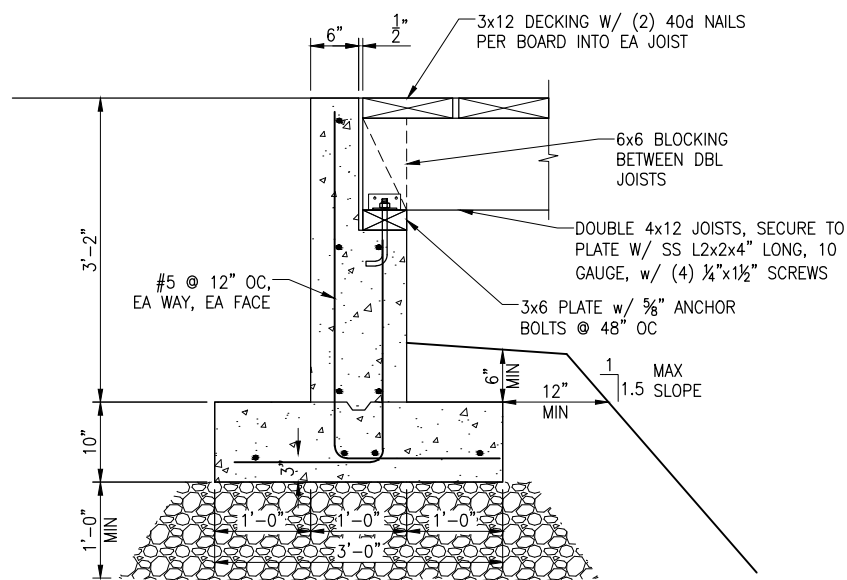
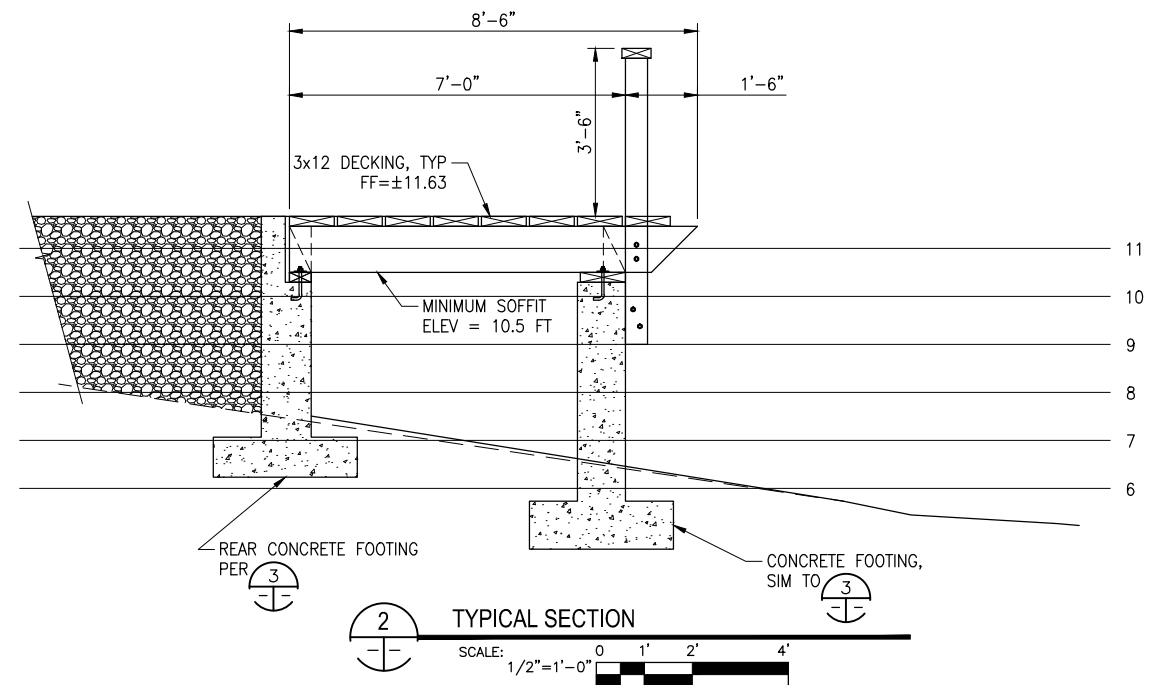
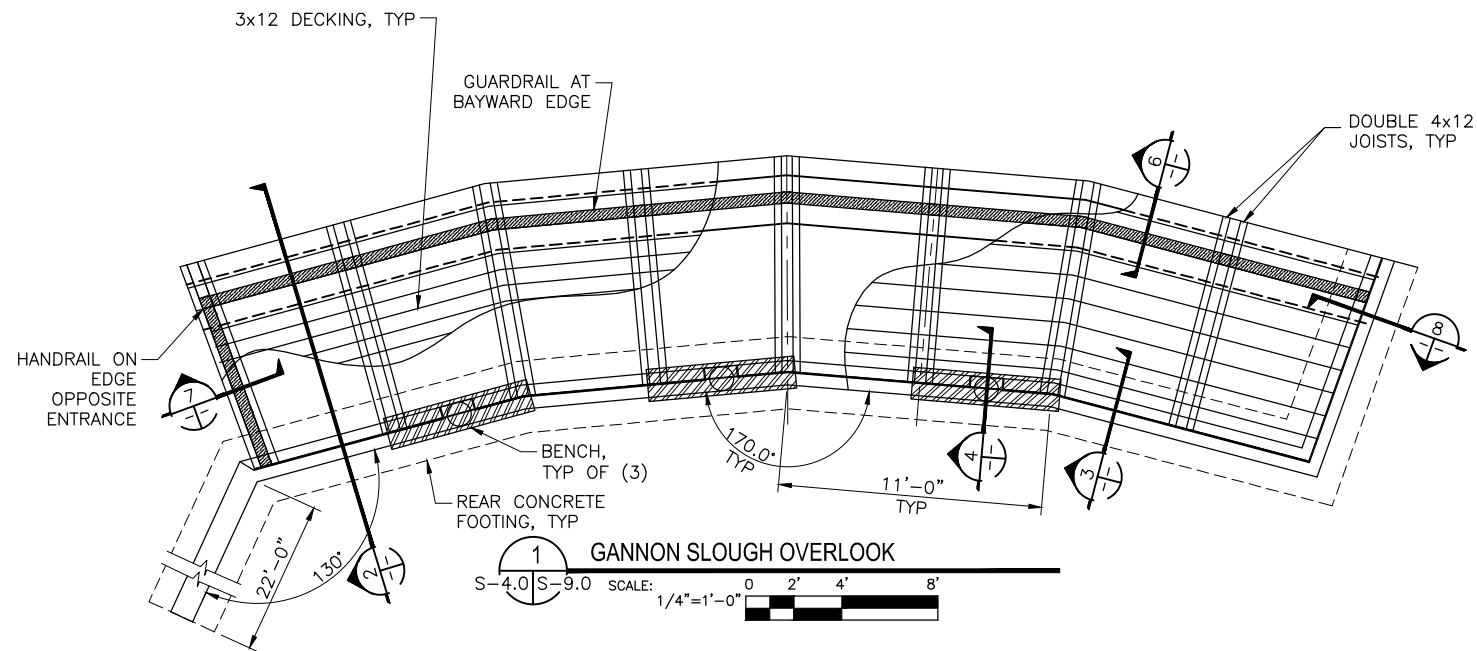


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SITE PLAN & PROFILE





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STRUCTURAL
GANNON SLOUGH OVERLOOK

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