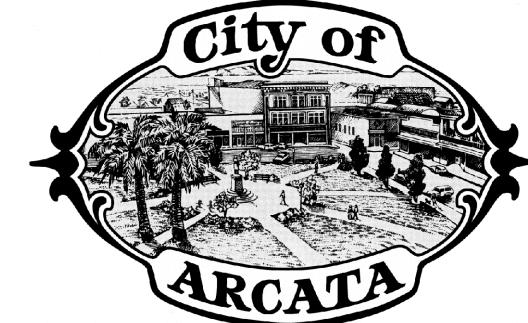


CITY OF ARCATA

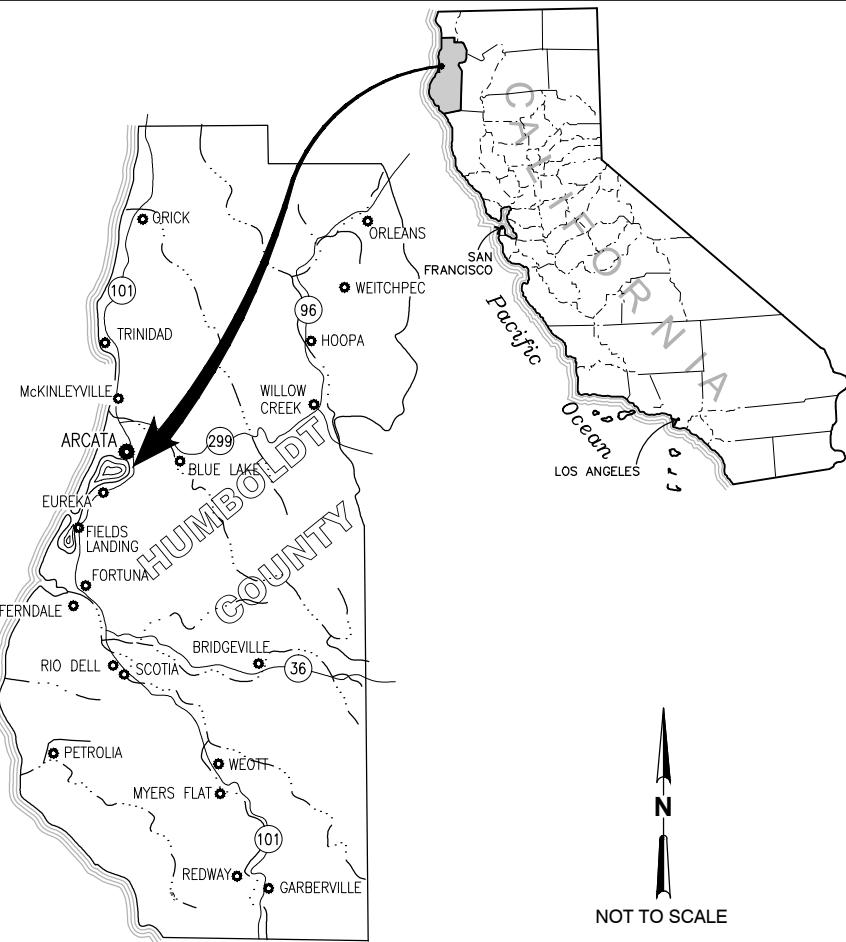
HUMBOLDT BAY TRAIL NORTH PROJECT

FEDERAL PROJECT NUMBER ATPL-5021(020)

AUGUST 2016



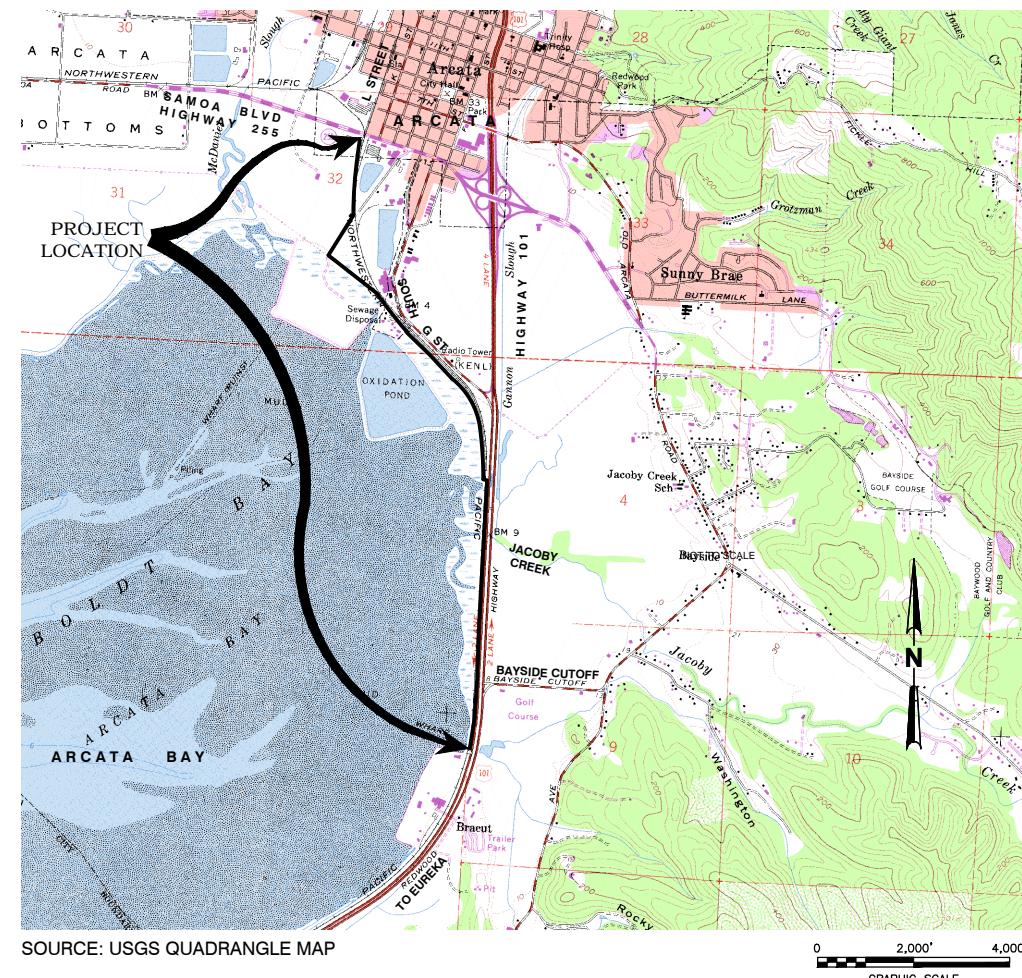
AREA MAP



N

NOT TO SCALE

LOCATION MAP



SOURCE: USGS QUADRANGLE MAP

APPROVALS

CITY OF ARCATA
ASSISTANT CITY ENGINEER:
NETRA KHATRI, PE

NSKhatri
- 8/30/16

CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH
GENERAL
COVER SHEET

PROJ NO: 8411982

DRWN: OFG CHKD: JW

G-1.0

SHEET 1 OF 52

GHD Inc.
718 Third Street, Eureka, California 95501 USA
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GENERAL SHEETS:

- 1 G-1.0 COVER SHEET
- 2 G-2.0 DRAWING INDEX
- 3 G-3.0 GENERAL SYMBOLS AND ABBREVIATIONS
- 4 G-4.0 CONSTRUCTION NOTES
- 5 G-5.0 TYPICAL TRAIL SECTIONS
- 6 G-6.0 TYPICAL TRAIL SECTIONS
- 7 G-7.0 TYPICAL TRAIL SECTIONS
- 8 G-8.0 PLAN SHEET INDEX, STAGING AREAS & SIGNAGE
- 9 G-9.0 LINE & CURVE TABLES

CIVIL SHEETS:

10	C-1.0	PLAN & PROFILE STA 25+00 TO STA 28+50
11	C-2.0	PLAN & PROFILE STA 28+50 TO STA 32+00
12	C-3.0	PLAN & PROFILE STA 32+00 TO STA 37+50
13	C-4.0	PLAN & PROFILE STA 37+50 TO STA 43+00
14	C-5.0	PLAN & PROFILE STA 43+00 TO STA 48+50
15	C-6.0	PLAN & PROFILE STA 48+50 TO STA 54+00
16	C-7.0	PLAN & PROFILE STA 54+00 TO STA 59+50
17	C-8.0	PLAN & PROFILE STA 59+50 TO STA 65+00
18	C-9.0	PLAN & PROFILE STA 65+00 TO STA 70+50
19	C-10.0	PLAN & PROFILE STA 70+50 TO STA 76+00
20	C-11.0	PLAN & PROFILE STA 76+00 TO STA 81+50
21	C-12.0	PLAN & PROFILE STA 81+50 TO STA 87+00

CIVIL SHEETS (CONTINUED):

22	C-13.0	PLAN & PROFILE STA 87+00 TO STA 92+50
23	C-14.0	PLAN & PROFILE STA 92+50 TO STA 98+00
24	C-15.0	PLAN & PROFILE STA 98+00 TO STA 103+50
25	C-16.0	PLAN & PROFILE STA 103+50 TO STA 109+00
26	C-17.0	PLAN & PROFILE STA 109+00 TO STA 114+50
27	C-18.0	PLAN & PROFILE STA 114+50 TO STA 120+00
28	C-19.0	PLAN & PROFILE STA 120+00 TO STA 125+50
29	C-20.0	PLAN & PROFILE STA 125+50 TO STA 131+00
30	C-21.0	PLAN & PROFILE STA 131+00 TO STA 136+50
31	C-22.0	PLAN & PROFILE STA 136+50 TO STA 142+00
32	C-23.0	PLAN & PROFILE STA 142+00 TO STA 147+50
33	C-24.0	PLAN & PROFILE STA 147+50 TO STA 153+00
34	C-25.0	PLAN & PROFILE STA 153+00 TO STA 158+50
35	C-26.0	PLAN & PROFILE STA 158+50 TO STA 164+00
36	C-27.0	PLAN & PROFILE STA 164+00 TO STA 169+50
37	C-28.0	INTERSECTION LAYOUT - WWTP ENTRANCE
38	C-29.0	OLD JACOBY CREEK - INTERIM BRIDGE APPROACH
39	C-30.0	JACOBY CREEK - INTERIM BRIDGE APPROACH

STRUCTURAL SHEETS:

44	S-1.0	GENERAL NOTES
45	S-2.0	OLD JACOBY CREEK CROSSING SITE PLAN & PROFILE
46	S-3.0	JACOBY CREEK CROSSING SITE PLAN & PROFILE
47	S-4.0	GANNON SLOUGH BRIDGE SITE PLAN & PROFILE
48	S-5.0	BUTCHER SLOUGH BRIDGE SITE PLAN & PROFILE
49	S-6.0	PERIMETER LEVEE ELEVATED TRAIL SITE PLAN & PROFILE
50	S-7.0	STANDARD DETAILS
51	S-8.0	STANDARD DETAILS
52	S-9.0	GANNON SLOUGH OVERLOOK



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MARK	DATE	DESCRIPTION

**CITY OF ARCAT
HUMBOLDT BAY TRAIL NORTH
GENERAL
DRAWING INDEX**

PROJ NO: 8411982

G-20

SYMBOLS:

GENERAL:

	NEW	EXISTING
¢	CENTERLINE	UTL UNKNOWN UTILITY
Ø	DIAMETER	W W WATER MAIN
±	APPROXIMATE	SD SD STORM DRAIN/CULVERT
@	AT	SS SANITARY SEWER MAIN

SECTION OR DETAIL DESIGNATION (LETTER INDICATES SECTION, NO. INDICATES DETAIL)

SHEET WHERE SECTION OR DETAIL OCCURS

TOPOGRAPHIC:

	NEW	EXISTING
		LIMITS OF DISTURBANCE
		PROPERTY LINE (APPROXIMATE)
		R/W RIGHT OF WAY LINE (APPROXIMATE)
		X X FENCE
		ROAD/TRAIL CENTERLINE
	/// AC PAVEMENT	/// AC PAVEMENT
	ROADSIDE GUARDRAIL	ROADSIDE GUARDRAIL
	RAILROAD TRACKS	RAILROAD TRACKS
	10-12 CONTOUR LINE	10-12 CONTOUR LINE
	LIMITS OF EARTHWORK CUT/FILL	LIMITS OF EARTHWORK CUT/FILL
	TOP OF BANK	TOP OF BANK
	TOE OF BANK	TOE OF BANK
	DRIVEWAY	DRIVEWAY
	CONCRETE SLAB	CONCRETE SLAB
	DRAINAGE/CREEK /DITCH/SWALE	DRAINAGE/CREEK /DITCH/SWALE
	BRUSH /VEGETATION LINE	BRUSH /VEGETATION LINE
	TREE	TREE
	WET AREA / WETLAND	WET AREA / WETLAND
▲ TBM	TEMPORARY BENCH MARK	TEMPORARY BENCH MARK
◎	SURVEY MONUMENT	SURVEY MONUMENT
○	ROAD SIGN	ROAD SIGN
●	MONITORING WELL	MONITORING WELL

UTILITY:

	NEW	EXISTING
		W W WATER MAIN
		SD SD STORM DRAIN/CULVERT
		SS SANITARY SEWER MAIN
		FM FM SANITARY SEWER FORCE MAIN
		— E — E — E — E — E UNDERGROUND ELECT CONDUIT
		— E OHD — OVERHEAD ELECTRICAL LINE
		— OHE&T — OVERHEAD ELECTRICAL & TELEPHONE
		— TV — TV — CABLE TELEVISION (COMM) LINE
		— G — G — GAS MAIN

ABBREVIATIONS:

AB	ANCHOR BOLT	FP	FIRE PROTECTION	PVC	POLYVINYL CHLORIDE PLASTIC PIPE
AB	AGGREGATE BASE	FS	FINISHED SURFACE	PVI	POINT OF VERTICAL INTERSECT
AC	ASPHALT CONCRETE	FT	FOOT OR FEET		
AGG	AGGREGATE	FTG	FOOTING	R, RAD	RADIUS
APE	AREA OF POTENTIAL EFFECT	G	GAS LINE	RC	RELATIVE COMPACTION
ARV	AIR-VACUUM RELEASE VALVE	GAL	GALLON	RCP	REINFORCED CONCRETE PIPE
ASS'Y.	ASSEMBLY	GALV	GALVANIZED	RD	ROAD
AVE	AVENUE	GB	GRADE BREAK	RDWD	REDWOOD
AWWA	AMERICAN WATER WORKS ASSOCIATION	GR	GRADE	REQ'D	REQUIRED
@	AT	GRD	GROUND	REQ'T	REQUIREMENT
		GV	GATE VALVE	RPP	REDUCED PRESSURE PRINCIPAL
B-	BORING	HB	HOSE BIBB	RR	RAILROAD
BC	BEGIN CURVE	HORZ	HORIZONTAL	RSP	ROCK SLOPE PROTECTION
BFP	BACK FLOW PREVENTER	HPS	HIGH PRESSURE SODIUM	RT	RIGHT
BM	BENCH MARK	HWY	HIGHWAY	R/W	RIGHT-OF-WAY
BLDG	BUILDING	HYD	HYDRANT		
BLVD	BOULEVARD				
BO	BLOW OFF				
BOT	BOTTOM	IP	IRON PIPE		
BVC	BEGIN VERTICAL CURVE	INV	INVERT		
BVCS	BEGIN VERTICAL CURVE ELEVATION				
	BEGIN VERTICAL CURVE STATION	JCT	JUNCTION		
C	CONDUIT	JP	JOINT UTILITY POLE		
CITY	CITY OF ARCATA	K	RATE OF CURVATURE		
CL, ¢	CENTERLINE	L	LENGTH		
CLR	CLEAR, CLEARANCE	LAT	LATERAL		
CO	CLEANOUT	LS	LIFT STATION		
CMP	CORRUGATED METAL PIPE	LT	LEFT		
CMU	CONCRETE MASONRY UNIT	LVC	LENGTH VERTICAL CURVE		
CONC	CONCRETE	MAX	MAXIMUM	T	TANGENT
CONT	CONTINUOUS	M	METER	T, TEL	TELEPHONE
CONT'D	CONTINUED	MFR	MANUFACTURER	TC	TOP OF CURB
COORD	COORDINATE	MH	MAN HOLE	THK	THICK
COR	CORNER	MIN	MINIMUM	TG	TOP GRATE
CU	CUBIC	MISC	MISCELLANEOUS	TP	TEST PIT
CV	CHECK VALVE	MG	MILLION GALLON	TS	TOP OF SLAB
d	PENNY (NAIL SIZE)	N	NORTH	TW	TOP OF WALK
DIA, Ø	DIA, Ø	(N)	NEW	TYP	TYPICAL
DTL	DETAIL	NCRA	NORTH COAST RAILROAD AUTHORITY	UBC	UNIFORM BUILDING CODE
DI	DROP (DRAINAGE) INLET	NIC	NOT IN CONTRACT	UNO	UNLESS OTHERWISE NOTED
DR	DRIVE	NO	NUMBER		
DW	DOMESTIC WATER LINE	NTS	NOT TO SCALE	V	VOLT(S)
DWG	DRAWING	(E)	EXISTING	VERT	VERTICAL
		E	EAST		
		EA	EACH		
		EC	END CURVE		
		EF	EACH FACE		
		EP	EDGE PAVING		
		EQ	EQUAL		
		ER	EDGE ROAD		
		EL/ELEV	ELEVATION		
		ELEC	ELECTRIC		
		ENGR	ENGINEER		
		ETW	EDGE OF TRAVELED WAY		
		EVC	END VERTICAL CURVE		
		EVCE	END VERTICAL CURVE ELEVATION		
		EVCS	END VERTICAL CURVE STATION		
		EW	EACH WAY		
		FIN	FINISH		
		FF	FINISH FLOOR		
		FH	FIRE HYDRANT		
		FL, FL	FLOW LINE		
		FLR	FLOOR		
		OC	ON CENTERS	W/	WITH
		OPNG	OPENING	W	WATER
		PC	POINT OF CURVE	WD	WIDE
		PCC	PORTLAND CONCRETE CEMENT	XING	CROSSING
		PE	POLYETHYLENE	YD	YARD
		PI	POINT OF INTERSECTION		
		PL	PLATE		
		P/L, P	PROPERTY LINE		
		PLCS	PLACES		
		PLWD	PLYWOOD		
		POC	POINT OF CONNECTION		
		PP	POWER POLE		
		PRC	POINT OF REVERSE CURVE		
		PRV	PRESSURE REDUCING VALVE		
		PSI	POUNDS PER SQUARE INCH		
		PT	POINT		
		PT	POINT OF TANGENT BEGINNING		
		PT	PRESSURE TREATED		

NOTE: CONTACT ENGINEER FOR ABBREVIATIONS NOT LISTED.

CITY OF ARCATA HUMBOLDT BAY TRAIL NORTH

GENERAL SYMBOLS AND ABBREVIATIONS

G-3.0

SHEET 3 OF 52

PROJ NO: 8411982
DRWN: OFG CHKD: JW

Plot Date: 12 August 2016 - 9:10 AM
Plotted By: David Caisse

Cad File No: \ghdnet\ghd\US\Eureka\Projects\8411982\06-CAD\Sheets\8411982_03-G-3.0.dwg

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PROFESSIONAL ENGINEER IN THE STATE OF CALIFORNIA
SARAH COOPER, PE
3/12/2016

GENERAL NOTES

- ALL WORKMANSHIP AND MATERIALS SHALL CONFORM TO STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION (CALTRANS) STANDARD PLANS & SPECIFICATIONS, AND THE CONTRACT SPECIAL PROVISIONS FOR THIS PROJECT, UNLESS SHOWN OTHERWISE.
- PROJECT REQUIRES A CLASS A GENERAL ENGINEERING CONTRACTOR'S LICENSE IN THE STATE OF CALIFORNIA.
- QUANTITIES OF ITEMS, LENGTH OF PROJECT, AND SITE CONDITIONS SHOWN IN THE PLANS ARE APPROXIMATE. ALL MATERIALS SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR UNLESS OTHERWISE NOTED.
- CONTRACTOR AGREES TO ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR THE JOB SITE DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY; THAT THIS REQUIREMENT SHALL APPLY CONTINUOUSLY AND NOT BE LIMITED TO NORMAL WORKING HOURS; AND THAT THE CONTRACTOR SHALL DEFEND, INDEMNIFY, AND HOLD THE CITY, GHD, NCRA, US FISH & WILDLIFE AND THEIR REPRESENTATIVES HARMLESS FROM ANY AND ALL LIABILITY, REAL AND/OR ALLEGED, IN CONJUNCTION WITH THE PERFORMANCE OF THIS PROJECT.
- CONTRACTOR SHALL BE HELD RESPONSIBLE FOR ANY AND ALL DAMAGES TO EXISTING STRUCTURES, ROADS, RAILROAD TRACK AND TRACK BED, AND UTILITIES DURING CONSTRUCTION. ALL DAMAGE SHALL BE RESTORED TO EQUAL OR BETTER CONDITION AT THE CONTRACTOR'S EXPENSE.
- CONTRACTOR SHALL VERIFY LOCATIONS, ELEVATIONS, DISTANCES, AND FEATURES THAT MAY AFFECT THE WORK. SHOULD EXISTING CONDITIONS DIFFER FROM THOSE SHOWN OR INDICATED, OR IF IT APPEARS THAT THESE PLANS, STANDARD SPECIFICATIONS, AND SPECIAL PROVISIONS DO NOT ADEQUATELY DETAIL THE WORK TO BE DONE, CONTRACTOR SHALL NOTIFY THE CITY PRIOR TO CONTINUING WITH ANY RELATED WORK. NO ALLOWANCE WILL BE MADE ON HIS BEHALF FOR ANY EXTRA EXPENSE RESULTING FROM FAILURE OR NEGLECT IN DETERMINING THE CONDITIONS UNDER WHICH WORK IS TO BE PERFORMED. NOTED DIMENSIONS TAKE PRECEDENCE OVER SCALE.
- A SET OF SIGNED WORKING DRAWINGS AND A SET OF SPECIFICATIONS WILL BE KEPT AT ALL TIMES AT THE JOB SITE ON WHICH ALL CHANGES OR VARIATIONS IN THE WORK, INCLUDING ALL EXISTING UTILITIES, ARE TO BE RECORDED AND/OR CORRECTED DAILY AND SUBMITTED TO THE CITY WHEN THE WORK TO BE DONE IS COMPLETED.
- CONTRACTOR SHALL PROVIDE AND MAINTAIN SUFFICIENT TEMPORARY BARRIERS TO PROVIDE FOR THE SAFETY OF THE STAFF AND PUBLIC TO THE SATISFACTION OF THE CITY, CALTRANS AND NCRA.
- SHOULD GRADING OPERATIONS ENCOUNTER HAZARDOUS MATERIALS, OR WHAT APPEAR TO BE HAZARDOUS MATERIALS, STOP WORK IN THE AFFECTED AREA IMMEDIATELY AND CONTACT 911 OR THE APPROPRIATE AGENCY FOR FURTHER INSTRUCTION. IF SAID MATERIALS ARE DISCOVERED ON RAILROAD RIGHT-OF-WAY, NCRA SHALL BE NOTIFIED.
- CONTRACTOR SHALL NOTIFY THE CITY AT LEAST 72 HOURS IN ADVANCE OF COMMENCEMENT OF ANY PART OF THE WORK AND SHALL COORDINATE CONSTRUCTION SCHEDULE WITH THE CITY TO MINIMIZE IMPACT TO RESIDENTS. NCRA SHALL BE NOTIFIED AT LEAST 5 BUSINESS DAYS BEFORE THE START OF CONSTRUCTION.
- UNUSUITABLE EXCAVATED MATERIAL SHALL BE REMOVED FROM SITE AND DISPOSED OF IN A MANNER CONSISTENT WITH APPLICABLE REGULATIONS SUCH AS CITY OR COUNTY GRADING ORDINANCES. THE CONTRACTOR SHALL ASSUME FULL RESPONSIBILITY AND EXPENSE FOR PROPER DISPOSAL OF UNSUITABLE MATERIALS TAKEN FROM SITE AND PROVIDE SUITABLE DOCUMENTATION OF PERMISSION AND ENVIRONMENTAL DOCUMENTATION FOR USE OF ANY DISPOSAL SITE AS REQUESTED BY THE CITY.
- THE DESIGN FEATURES INCLUDING HORIZONTAL AND VERTICAL ALIGNMENTS, TYPICAL SECTIONS, APPROACHES, AND OTHER DESIGN DETAILS SHOWN ON THESE DESIGN PLANS SHALL NOT BE ALTERED OR MODIFIED IN ANY WAY DURING CONSTRUCTION WITHOUT THE EXPRESSED, WRITTEN DIRECTION AND APPROVAL OF THE CITY AND/OR CONTRACTING OFFICER. DRAINAGE STRUCTURES SHALL BE INSTALLED AS SHOWN IN THE PLANS WITH ONLY MINOR CORRECTIONS IN LOCATION, SKEW AND/OR ELEVATIONS AS NEEDED TO FIT FIELD CONDITIONS AS DETERMINED BY THE CITY'S REPRESENTATIVE.
- THE CONTRACTOR SHALL READ AND MAKE CAREFUL EXAMINATION OF THE PLANS, SPECIFICATIONS, QUANTITIES AND MATERIAL ESTIMATES AND VISIT THE SITE OF THE PROPOSED CONSTRUCTION TO BECOME FAMILIAR WITH THE SITE CONDITIONS AND LIMITATIONS BEFORE MAKING A BID. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY AND ALL ERRORS RESULTING FROM THE FAILURE TO MAKE SUCH AN EXAMINATION. ANY INFORMATION DERIVED FROM THE MAPS, PLANS, SPECIFICATIONS, PROFILES, DRAWINGS OR FROM THE CITY WILL NOT RELIEVE THE CONTRACTOR FROM ANY RISK OR FROM FULFILLING THE TERMS OF THE CONTRACT.
- NO WORK SHALL BE PERFORMED OUTSIDE OF THE DESIGNATED CONSTRUCTION LIMITS WITHOUT THE APPROVAL OF THE CITY'S REPRESENTATIVE.
- THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING STAGING AREAS WITH THE CITY OF ARCATA. ALL STAGING AREAS WITHIN THE RIGHT-OF-WAY SHALL BE PRE-APPROVED BY THE CITY. ANY PROPOSED STAGING AREA WITHIN RAILROAD RIGHT-OF-WAY SHALL BE APPROVED BY NCRA.
- UPON COMPLETION OF THE CONSTRUCTION PROJECT, THE CONTRACTOR SHALL LEAVE THE PROJECT AREA FREE OF DEBRIS AND UNUSED MATERIAL. ALL DAMAGE CAUSED BY THE CONTRACTOR SHALL BE RESTORED TO AN "AS GOOD OR BETTER" CONDITION.
- EXISTING FENCING SHALL REMAIN IN PLACE AND UNDISTURBED BY CONSTRUCTION OPERATIONS UNLESS OTHERWISE SHOWN ON THE PLANS. CONTRACTOR SHALL BE HELD RESPONSIBLE FOR ANY AND ALL DAMAGES TO FENCES DURING CONSTRUCTION. ALL DAMAGE SHALL BE RESTORED TO EQUAL OR BETTER CONDITION AT THE CONTRACTOR'S EXPENSE.
- ALL WORK WITHIN NCRA RIGHT-OF-WAY SHALL FOLLOW NCRA'S BEST MANAGEMENT PRACTICES (BMP).
- THE CONTRACTOR SHALL NOTIFY THE CITY A MINIMUM OF 72 HOURS IN ADVANCE OF ANY EXCAVATION THAT WILL EXTEND 2 FEET OR DEEPER, AND WILL NOT PROCEED WITH ANY EXCAVATION WORK UNTIL CLEARED TO DO SO BY THE CITY. A PROJECT ARCHAEOLOGIST MAY BE ON SITE DURING CONSTRUCTION ACTIVITIES. THE CONTRACTOR IS ADVISED THAT IF ANY ARCHAEOLOGICAL FINDINGS ARE DISCOVERED DURING CONSTRUCTION THAT THE MONITOR OR ARCHAEOLOGIST HAS THE AUTHORITY TO SLOW OR STOP CONSTRUCTION ACTIVITIES AS THEY DEEM NECESSARY.
- EXISTING SIGN SYSTEMS SHALL BE PROTECTED THROUGHOUT CONSTRUCTION ACTIVITY UNLESS NOTED OTHERWISE ON PLANS. CONTRACTOR SHALL BE HELD RESPONSIBLE FOR ANY AND ALL DAMAGES TO SIGNS DURING CONSTRUCTION. ALL DAMAGE SHALL BE RESTORED TO EQUAL OR BETTER CONDITION AT THE CONTRACTOR'S EXPENSE.
- STRUCTURAL EXCAVATION AND BACKFILL OF CULVERTS AND OTHER DRAINAGE STRUCTURES SHALL BE CONSIDERED INCIDENTAL TO INSTALLATION OF THE STRUCTURE. EXCESS MATERIAL REMOVED MAY BE USED TO REBUILD APPROACHES OR MAY BE PLACED ALONG ROADWAY/TRAIL SHOULDERS IN AREAS AS DIRECTED BY THE CITY'S REPRESENTATIVE.
- CONTRACTOR SHALL OBTAIN ENCROACHMENT PERMITS FROM CALTRANS AND NCRA PRIOR TO BEGINNING WORK.

TRAFFIC CONTROL NOTES

- ALL PERMANENT AND TEMPORARY ROADSIDE SIGNS SHALL BE PLACED IN ACCORDANCE WITH THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND IN ACCORDANCE WITH THE DETAILS IN THESE PLANS. CONTRACTOR SHALL USE MUTCD FOR TRAFFIC CONTROL. SEE SPECIFICATIONS FOR OTHER TRAFFIC CONTROL REQUIREMENTS.
- THE CONTRACTOR IS RESPONSIBLE FOR PREPARING AND SUBMITTING A TRAFFIC CONTROL PLAN IN ACCORDANCE WITH THESE DETAILS, THE CONTRACTOR'S SEQUENCING PLAN, MUTCD, AND THE PROJECT SPECIFICATIONS.
- IF CONTRACTOR SUBMITS A TRAFFIC CONTROL PLAN OTHER THAN A STANDARD FROM MUTCD OR CALTRANS STANDARD PLAN, THEN THE TRAFFIC CONTROL PLAN MUST BE SIGNED BY A LICENSED ENGINEER.

SURVEY NOTES

- THE CONTRACTOR IS RESPONSIBLE FOR FIELD VERIFICATION OF ALL SURVEY DATA. CONTRACTOR IS RESPONSIBLE FOR IDENTIFYING THE LIMITS OF DISTURBANCE, LIMITS OF WETLAND IMPACTS (AS SHOWN ON THE PLANS), TRAIL ALIGNMENT AND GRADE, RIGHT-OF-WAY LINES, SLOPE EASEMENTS, AND ALL HORIZONTAL AND VERTICAL CONTROL PRIOR TO CONSTRUCTION.
- CONTRACTOR IS RESPONSIBLE FOR CONSTRUCTION STAKING AND SHALL ARRANGE FOR STAKING WITH A CALIFORNIA LICENSED SURVEYOR. TWO-DIMENSIONAL COMPUTER AIDED DESIGN (CAD) FILES WILL BE AVAILABLE AND PROVIDED TO THE CONTRACTOR PRIOR TO CONSTRUCTION. THESE PRINTED PLANS ARE THE CONTRACT DOCUMENTS AND THE CONTRACTOR ASSUMES ALL LIABILITY FOR THE USE OF ANY ELECTRONIC FILES. STAKING WILL BE REVIEWED BY OWNER FOR CONFORMANCE TO DESIGN PRIOR TO CONSTRUCTION.

UTILITY NOTES

- THE LOCATION OF UTILITIES SHOWN IN THE PLANS IS APPROXIMATE AND IS ONLY SHOWN TO ASSIST THE CONTRACTOR IN COMPLETING THE WORK. THE CONTRACTOR SHALL VERIFY ALL UTILITIES WITH OWNERS PRIOR TO CONSTRUCTION. ANY DAMAGE TO EXISTING UTILITIES BY THE CONTRACTOR SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AT THE CONTRACTOR'S EXPENSE.
- IF ANY WORK IS IN CLOSE PROXIMITY TO EXISTING UTILITIES AND COORDINATION IS REQUESTED BY THE UTILITY COMPANY, THE CONTRACTOR SHALL COORDINATE UTILITY WORK WITH THE RESPONSIBLE UTILITY COMPANY (UTILITY OWNER).
- THE ACTUAL LOCATION OF EXISTING UTILITIES MAY VARY FROM THAT SHOWN ON THE PLANS. EXACT LOCATION AND COMPLETENESS ARE NOT GUARANTEED. CONTRACTOR SHALL POTHOLE AND LOCATE ALL EXISTING UTILITIES VARIATIONS IN LOCATION AND DEPTH SHALL BE BROUGHT TO THE ATTENTION OF THE CITY IMMEDIATELY SO THAT THE LOCATION OF UTILITIES MAY BE CHECKED WITH THE PROPOSED DESIGN. CONTRACTOR SHALL NOTIFY UNDERGROUND SERVICES ALERT (800) 227-2600 A MIN. OF 48 HOURS PRIOR TO ANY EXCAVATION AND POTHOLE FOR EXACT LOCATION.
- ALL LOCATIONS FOR WORK SHALL BE CHECKED AND COORDINATED WITH EXISTING CONDITIONS IN THE FIELD BEFORE BEGINNING CONSTRUCTION.
- ALL EXISTING UTILITIES AND TIE-IN POINTS SHOULD BE CONSIDERED ACTIVE UTILITIES UNLESS OTHERWISE INDICATED.
- CONSTRUCTION ACTIVITY WILL TAKE PLACE IN THE VICINITY OF OVERHEAD ELECTRIC TRANSMISSION LINES. IT IS THE CONTRACTOR'S RESPONSIBILITY TO BE AWARE OF, AND OBSERVE, THE MINIMUM CLEARANCES FOR WORKERS AND EQUIPMENT OPERATING NEAR HIGH VOLTAGE ELECTRIC LINES AS SET OUT IN THE HIGH VOLTAGE SAFETY ORDERS OF THE CALIFORNIA DIVISION OF INDUSTRIAL SAFETY AS WELL AS OTHER APPLICABLE SAFETY REGULATIONS.

GRADING NOTES

- CONTRACTOR SHALL BE RESPONSIBLE FOR THE SAFETY OF ALL CONSTRUCTION. ADEQUATE SHORING BRACING, TIES, AND SUPPORTS SHALL BE USED TO PROVIDE PROPER TEMPORARY INTEGRITY DURING ALL PHASES OF CONSTRUCTION.
- ALL EXISTING LANDSCAPED AND UNPAVED AREAS WHICH ARE DISTURBED BY CONSTRUCTION OR EARTHWORK OPERATIONS SHALL BE HAND RAKED SMOOTH AND RETURNED TO ORIGINAL EXISTING CONDITIONS.
- ALL DITCHES, SWALES, GUTTERS, ETC. SHOULD BE CONSIDERED ACTIVE STORM CONVEYANCES UNLESS OTHERWISE INDICATED. CONTRACTOR IS RESPONSIBLE FOR ADDRESSING STORM WATER DRAINAGE AND Dewatering OF WORK AREAS DURING CONSTRUCTION.

EROSION CONTROL NOTES

- CONTRACTOR IS RESPONSIBLE FOR PREPARING THE SWPPP AND FOR EROSION AND SEDIMENT CONTROL. CONTRACTOR SHALL USE CALTRANS STORMWATER QUALITY HANDBOOKS, CONSTRUCTION CONTRACTOR'S GUIDE AND SPECIFICATIONS. CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH THE PROVISIONS IN THE SWPPP AND EROSION CONTROL PLAN.
- DURING WET WEATHER PERIODS, CONTRACTOR IS RESPONSIBLE FOR SEQUENCING CONSTRUCTION IN A MANNER TO MINIMIZE IMPACT ON OPEN EARTHWORK AND COMPACTION OPERATIONS.
- ALL NEWLY GRADED AREAS WITHIN THE PROJECT LIMITS, INCLUDING AREAS COVERED WITH EROSION CONTROL BLANKETS ARE TO BE SEADED. THE PERMANENT PLANTINGS/SEED MIXTURE SHALL BE AS DIRECTED BY THE CITY AND IN ACCORDANCE TO THE PROJECT PERMITS.
- AFTER SEEDING, A MULCH CONSISTING OF GRASS, HAY OR STRAW PER CALTRANS SPECIAL PROVISIONS SHALL BE BLOWN ON AND PUNCHED INTO ALL NEWLY SEADED AREAS AT A RATE OF TWO TONS PER ACRE. IMMEDIATELY FOLLOWING APPLICATION, THE MULCH SHALL BE PUNCHED INTO THE SOIL BY A TILLER CONSISTING OF A SERIES OF DULL FLAT DISKS WITH NOTCHED OR CUTOUT EDGES.
- IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO FIX ANY DEFICIENCIES INDICATED BY THE CITY TO PREVENT EROSION AND CONTROL SEDIMENT.
- PRIOR TO FINAL ACCEPTANCE TEMPORARY SEDIMENT CONTROL MEASURES SHALL BE REMOVED AND ALL AREAS OF SOIL DISTURBANCE, INCLUDING BUT NOT LIMITED TO CUT AND FILL SLOPES, SWALES AND DITCHES SHALL BE STABILIZED.
- THE PROPOSED EROSION AND SEDIMENT CONTROL MEASURES ARE A MINIMUM BEST MANAGEMENT PRACTICE. THE CONTRACTOR SHALL INSPECT THE SITE DAILY, IDENTIFYING DEFICIENCIES AND PROVIDE ADDITIONAL EROSION CONTROL MEASURES AS REQUIRED TO ENSURE THAT NO SEDIMENT LADEN WATER EXITS THE SITE, ENTERS THE EXISTING STORMWATER SYSTEM OR ENTERS SENSITIVE AREAS. ADJUSTMENTS MAY BE MADE TO THE BMP'S AND SWPPP IN THE FIELD SUBJECT TO APPROVAL OF OR AT THE DIRECTION OF THE OWNERS REPRESENTATIVE. IF NECESSARY, SECTION 3, EROSION AND SEDIMENT CONTROL BMP'S, OF THE CALIFORNIA STORMWATER BMP HANDBOOK SHALL BE REFERENCED TO IDENTIFY THE VARIOUS FIELD CONDITIONS.
- THE EROSION AND SEDIMENT CONTROL MEASURES IDENTIFIED ON THESE PLANS OR THE SWPPP SITE MAPS ARE APPROPRIATE TO MINIMIZE EROSION AND PREVENT SEDIMENT DISCHARGE IN THE EVENT OF SUMMER RAINSTORMS (APRIL 15TH THROUGH OCTOBER 15TH). IN THE EVENT THERE IS A DELAY IN THE CONSTRUCTION SCHEDULE BMP'S/ OR FINAL STABILIZATION MEASURES (INCLUDING SEED MIX AND METHOD OF APPLICATION) MAY NEED TO BE ADJUSTED.
- ALL EROSION AND SEDIMENT CONTROL MEASURES SHALL REMAIN IN PLACE AND MAINTAINED IN ACCORDANCE WITH THEIR RESPECTIVE BMP FACT SHEETS UNTIL THE DISTURBED AREAS ARE PERMANENTLY STABILIZED. FINAL, PERMANENT STABILIZATION SHALL BE 70% ESTABLISHED VEGETATIVE GROWTH AS DETERMINED BY THE OWNERS REPRESENTATIVE.


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MARK	DATE	DESCRIPTION	ISSUE

CITY OF ARCATA HUMBOLDT BAY TRAIL NORTH		GENERAL CONSTRUCTION NOTES	
PROJ NO:	8411982	DRWN:	OFG CHKD: JW

EARTHWORK VOLUMES		
CUT (CY)	FILL (CY)	EXCESS (CY)
3,655	2,425	1,230

NOTE: THE EARTHWORK VOLUMES SHOWN ARE BASED ON THE DIFFERENCE BETWEEN EXISTING GROUND AND PAVEMENT SECTION SUBGRADE. THE VOLUMES ARE UNADJUSTED FOR COMPACTION, EXPANSION, DRAINAGE DITCH AND STRUCTURE EXCAVATION, TOPSOIL OR TRENCH SPOILS. THE COMPLETENESS AND ACCURACY OF THE DATA IS NOT GUARANTEED. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL EARTHWORK QUANTITIES.

G-4.0

SHEET 4 OF 52



CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH

TYPICAL TRAIL SECTIONS

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0 1"

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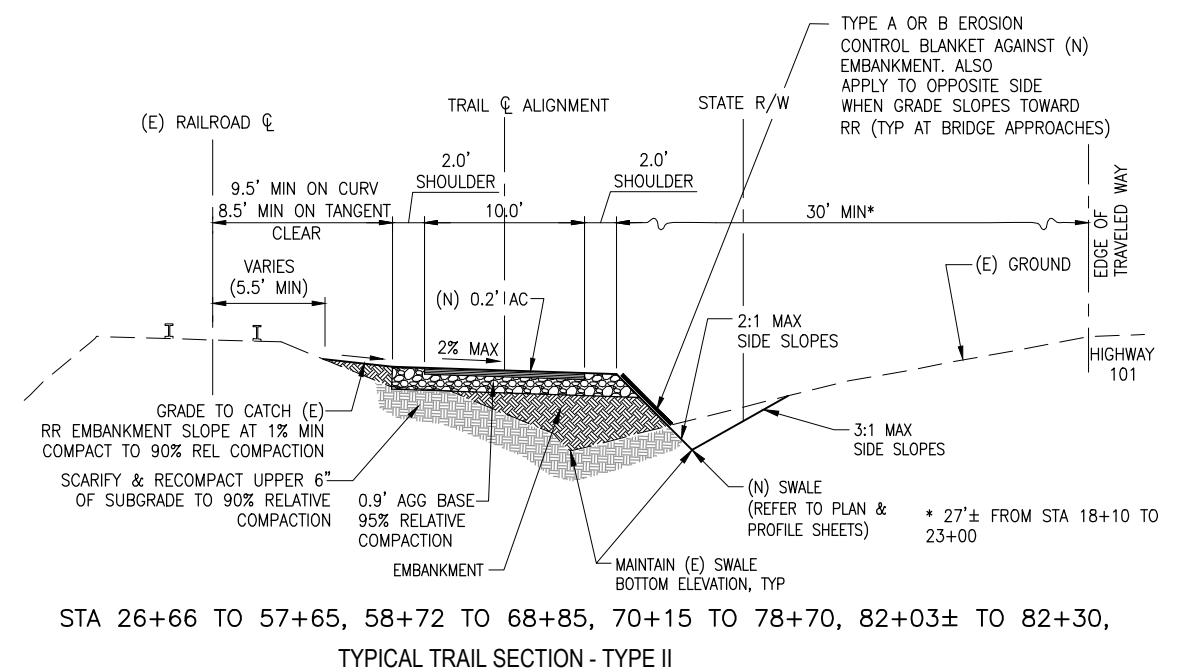
MARK	DATE	DESCRIPTION	ISSUE

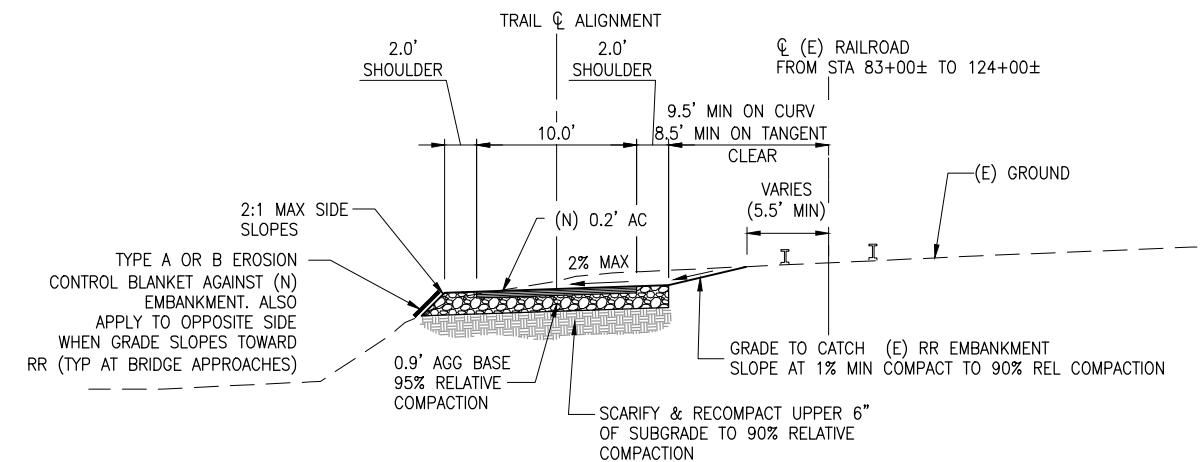
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DRWN: OFG CHKD: JW

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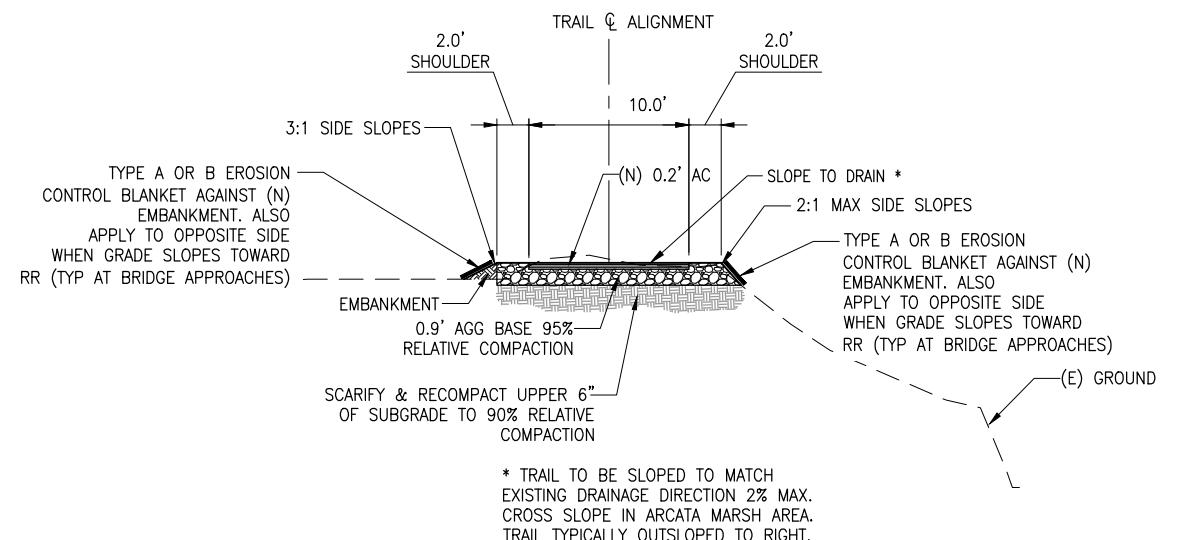
SHEET 5 OF 52

NOTES:
1. PROVIDE A SMOOTH AND GRADUAL
TRANSITION FROM ONE TYPICAL TRAIL
SECTION TYPE TO ANOTHER. STANDARD
TRANSITION RATE IS 10' PER 1% CHANGE IN
CROSS-SLOPE (UNLESS OTHERWISE NOTED).
2. NO EXCAVATION SHALL OCCUR W/IN 5.5' OF
RAILROAD CL, TYP.





NOTES:
 1. PROVIDE A SMOOTH AND GRADUAL TRANSITION FROM ONE TYPICAL TRAIL SECTION TYPE TO ANOTHER. STANDARD TRANSITION RATE IS 10' PER 1% CHANGE IN CROSS-SLOPE (UNLESS OTHERWISE NOTED).
 2. NO EXCAVATION SHALL OCCUR W/IN 5.5' OF RAILROAD CL, TYP.



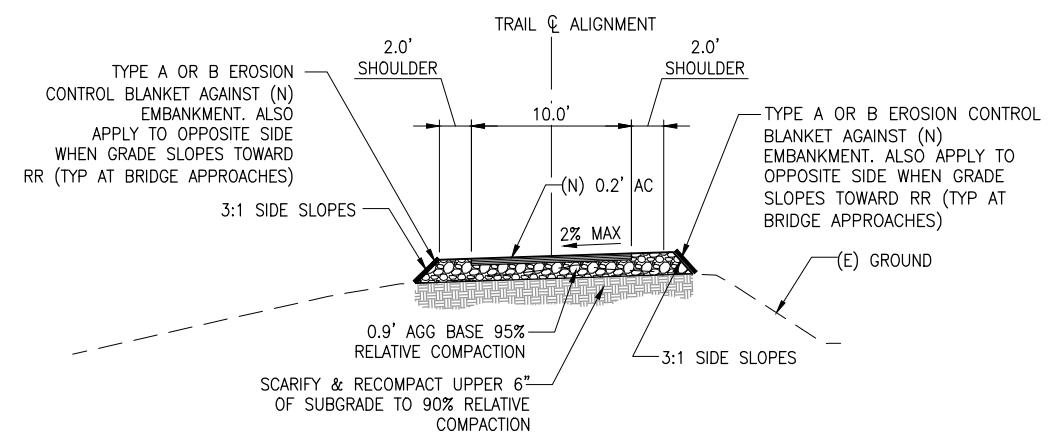
CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH
TYPICAL TRAIL SECTIONS

PROJ NO: 8411982
DRWN: OFG CHKD: JW

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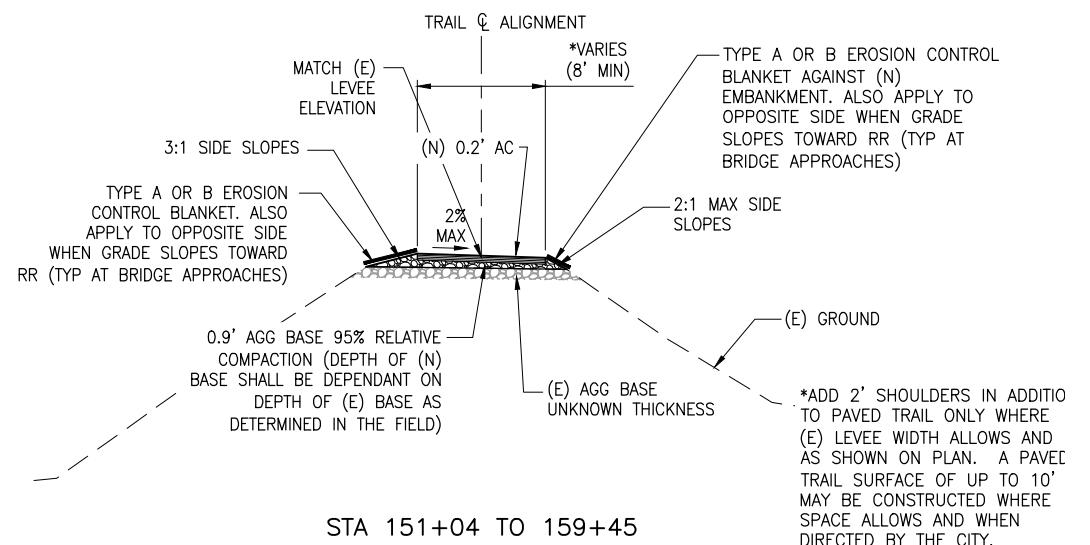
SHEET 6 OF 52





STA 140+50 TO 150+72
TYPICAL TRAIL SECTION - TYPE V

NOTE: PROVIDE A SMOOTH AND GRADUAL TRANSITION FROM ONE TYPICAL TRAIL SECTION TYPE TO ANOTHER. STANDARD TRANSITION RATE IS 10' PER 1% CHANGE IN CROSS-SLOPE (UNLESS OTHERWISE NOTED)



STA 151+04 TO 159+45
TYPICAL TRAIL SECTION - TYPE VI

NOTE:
REMOVE TOP 6" OF RED LAVA ROCK TRAIL SURFACING. STOCKPILE IN CITY CORP YARD AND UTILIZE FOR TRAIL SHOULDER SURFACING (4" THICK) AT FOLLOWING STATIONS: STA 126+18 TO 159+45.

CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH
TYPICAL TRAIL SECTIONS

PROJ NO: 8411982
DRWN: OFG CHKD: JW

G-7.0

SHEET 7 OF 52



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0 1"

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NOTE: CONSTRUCTION STAGING IS THE CONTRACTORS RESPONSIBILITY AND SHALL OCCUR WITHIN PAVED OR GRAVELED AREAS OR A DESIGNATED, PREVIOUSLY DISTURBED CORPORATION YARD. CONTRACTOR SHALL COORDINATE WITH THE CITY FOR APPROVAL.

NATIONAL WILDLIFE REFUGE

(N) GAN BRIDGE C

BRACUT.

CONSTRUCTION STAGING AREA, TYP

BRAINARD'S

BEGIN PROJECT NORTH OF BRAINARD'S SLOUGH AT STA 26+66

C-1.0

25+00 30+00 35+00 40+00 45+00 50+00 55+00 60+00 65+00 70+00

C-3.0

C-2.0

C-4.0

C-5.0

C-6.0

C-7.0

C-8.0

C-9.0

NORTHWESTERN PACIFIC RAILROAD

CALTRANS

CONSTRUCTION STAGING AREA, TYP

CONSTRUCTION STAGING AREA, TYP

(N) WAY-FINDING SIGN BY CITY (NIC), INDICATING DISTANCE & DIRECTION TO DOWNTOWN ARCAT, ARCAT MARSH, BRACUT, AND CITY OF EUREKA

BAYSIDE CUTOFF

OLD JACOBY CREEK CROSSING

JACOBY

(N) OLD JACOBY CREEK BRIDGE CROSSING, SEE SHEET S-2.0

ARCATA WASTEWATER TREATMENT PLANT

(N) TRAIL OVERLOOK, SEE SHEET S-9.0

(N) INTERPRETIVE SIGN BY CITY (NIC), INDICATING INTERPRETIVE INFORMATION ABOUT MINGLING OF FRESH & SALT WATERS, SPECIES DIVERSITY, AND MIGRATING BIRDS

CONSTRUCTION STAGING AREA, TYP

CONSTRUCTION STAGING AREA, TYP

W E R C O U G H

MATCHLINE STA 120+53 120+62

SEE PANEL BELOW

C-18.0

C-17.0

C-16.0

C-15.0

C-14.0

C-13.0

C-12.0

SOUTH NORTHWEST G STREET

HIGHWAY 101

80+00 85+00 90+00 95+00 100+00 105+00 110+00 115+00

11.0

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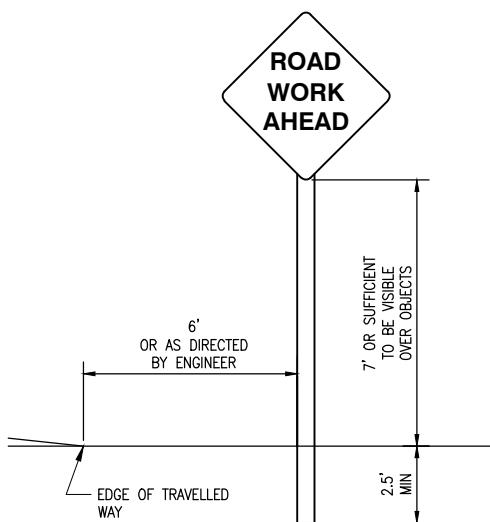
3855+00

3860+00

3865+00</

This map provides a detailed view of the Humboldt Bay area, focusing on the Butcher Slough and Arcata Marsh regions. Key features include:

- Geographical Labels:** (E) ARCATA MARSH, (E) JOLLY MARSH, (E) SOUTHERN MARSH, and (E) GIANT CREEK.
- Roads and Highways:** HIGHWAY 101, Samoa Blvd., and Samoa Street.
- Interpretive Signs:**
 - (N) INTERPRETIVE SIGN BY CITY (NIC), INDICATING INFORMATION ABOUT HUMBOLDT BAY, FRESHWATER MARSHES AND ASSOCIATED SENSITIVE SPECIES
 - (N) BUTCHER SLOUGH BRIDGE CROSSING, SEE SHEET S-5.0
 - (N) WAY-FINDING SIGN BY CITY (NIC), INDICATING DISTANCE & DIRECTION TO DOWNTOWN ARCATA, ARCATA MARSH, AND PACIFIC COAST
 - (N) INTERPRETIVE SIGN BY CITY (NIC), INDICATING INFORMATION ABOUT WASTEWATER RECLAMATION
 - (N) PERIMETER LEVEE ELEVATED TRAIL, SEE SHEET S-6.0
- Construction Staging Areas:** CONSTRUCTION STAGING AREA, TYP, located in various locations including the marshes and along the railroad.
- Distance Markers:** Elevation markers such as 120+00, 125+00, 130+00, 135+00, 140+00, 145+00, 150+00, 155+00, 160+00, 165+00, and 169+47.
- Other Labels:** MATCH LINE STA 120+00, SEE PANEL ABOVE, and END PROJECT AT SAMOA BLVD.



TYPICAL STATIONARY CONSTRUCTION AREA SIGNS

STATIONARY MOUNTED CONSTRUCTION AREA SIGNS

STATIONARY MOUNTED CONSTRUCTION AWARENESS					
Sign Label	Sign Code	Panel Size	Sign Message	Number & Post Size	No. of Signs
A	W20-1	36"x36"	ROAD WORK AHEAD	(1) 4"x4"	4
B	G20-2	36"x18"	END ROAD WORK	(1) 4"x4"	4

**CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH**

BRUNSWICK

DRWN: QEG CHKD: IW

G-8.0



GHD Inc.
718 Third Street Eureka California 95501 USA
T 1 707 443 8326 F 1 707 444 8330
W www.ghd.com



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**CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH**

GENERAL LINE & CURVE TABLES

PROJ NO: 8411982

DRWN: OFG | CHKD: JW

G-9.0

SHEET 9 OF 52

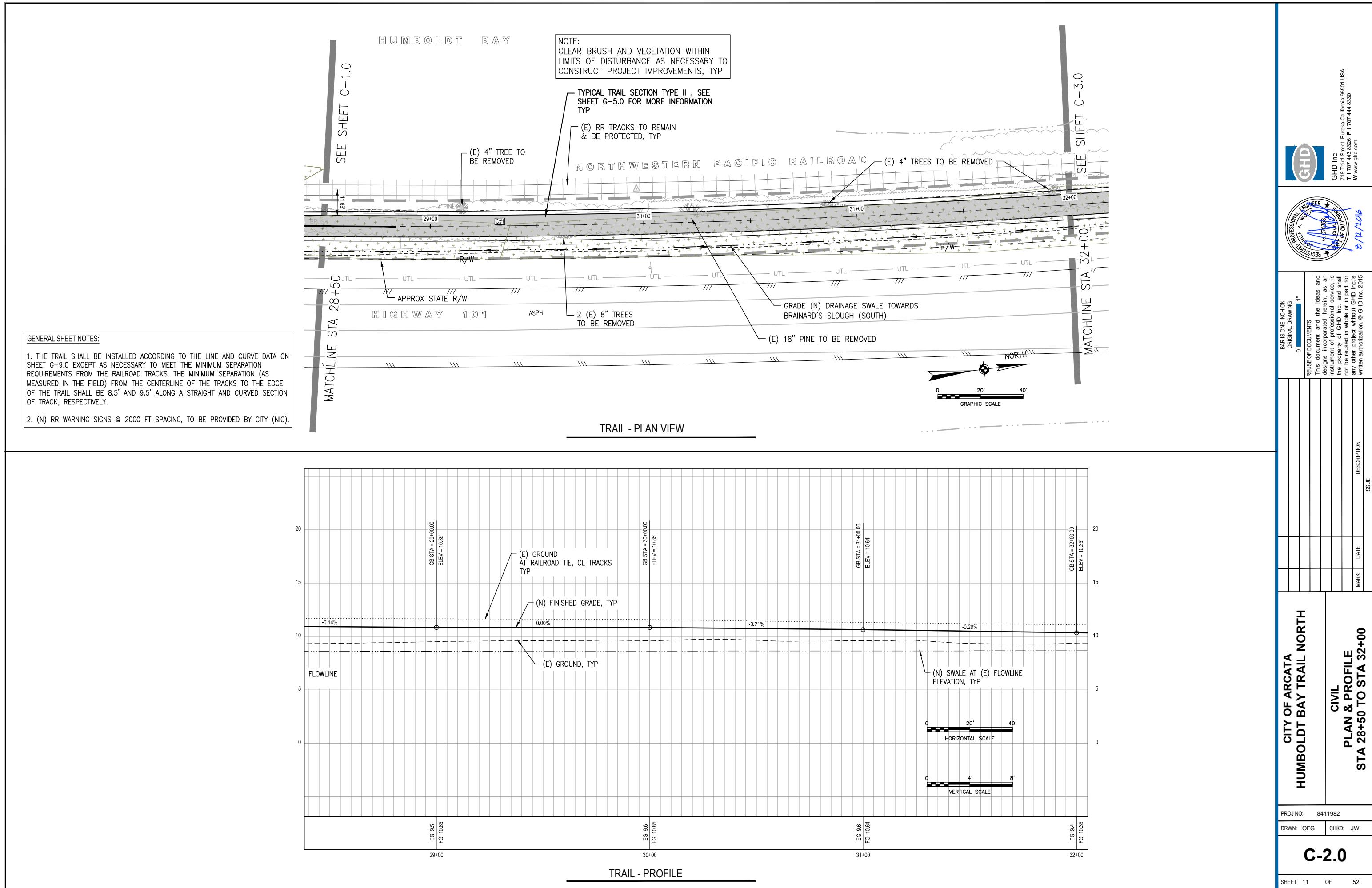
LINE TABLE (TRAIL CL)		
LINE #	LENGTH	DIRECTION
L#1	1088.93'	N01°47'42"E
L#2	391.36'	N01°46'32"E
L#3	311.29'	N01°42'30"E
L#4	456.66'	N01°48'20"E
L#5	163.69'	N01°48'16"E
L#6	33.93'	N15°20'36"E
L#7	55.00'	N01°48'32"E
L#8	33.43'	N11°43'31"W
L#9	550.25'	N01°49'25"E
L#10	406.38'	N01°47'01"E
L#11	36.33'	N13°12'03"E
L#12	80.00'	N01°47'57"E
L#13	22.53'	N13°12'03"W
L#14	424.31'	N01°53'46"E
L#15	370.67'	N01°48'48"E
L#16	14.29'	N08°40'01"E
L#17	440.27'	N01°47'17"E
L#18	25.33'	N88°12'50"W
L#19	311.67'	N01°51'10"E
L#20	64.88'	N12°17'13"W
L#21	228.36'	N46°39'47"W
L#22	172.07'	N46°54'20"W
L#23	192.77'	N46°43'00"W
L#24	151.13'	N46°48'03"W
L#25	140.12'	N46°55'22"W
L#26	118.95'	N46°36'37"W
L#27	177.05'	N46°44'46"W
L#28	245.24'	N46°32'31"W
L#29	188.58'	N44°50'35"W
L#30	97.46'	N42°11'38"W

LINE TABLE (TRAIL CL)		
LINE #	LENGTH	DIRECTION
L#31	29.27'	N54°54'48"W
L#32	97.06'	N39°46'53"W
L#33	28.72'	N68°12'37"W
L#34	153.33'	N63°30'24"W
L#35	37.08'	N66°56'45"W
L#36	209.83'	N64°50'51"W
L#37	61.11'	N67°37'48"W
L#38	81.00'	N71°20'36"W
L#39	47.25'	N50°57'20"W
L#40	49.90'	N54°18'22"W
L#41	117.52'	N55°50'30"W
L#42	193.35'	N50°44'53"W
L#43	70.84'	N46°44'35"W
L#44	26.33'	N08°02'29"W
L#45	65.39'	N20°20'38"W
L#46	62.39'	N18°46'43"E
L#47	42.85'	N30°50'47"E
L#48	140.79'	N36°52'09"E
L#49	453.69'	N34°49'48"E
L#50	144.47'	N06°12'58"W
L#51	196.78'	N03°28'31"W
L#52	190.09'	N01°02'18"W
L#53	233.12'	N00°46'50"W
L#54	97.35'	N41°39'50"E
L#55	353.22'	N12°12'31"E
L#56	11.13'	N42°58'43"W
L#57	6.48'	N17°20'19"W

CURVE TABLE (TRAIL CL)			
CURVE #	RADIUS	LENGTH	DELTA
C#1	4900.0'	658.33'	N5° 38' 39"E
C#2	90.00'	21.27'	N8° 34' 26"E
C#3	90.00'	21.28'	N4° 57' 03"W
C#4	90.00'	17.93'	N7° 29' 32"E
C#5	90.00'	23.71'	N5° 39' 08"W
C#6	90.00'	10.79'	N5° 14' 49"E
C#7	90.00'	10.83'	N5° 14' 04"E
C#8	12.00'	18.85'	N43° 12' 47"W
C#9	12.00'	18.86'	N43° 10' 50"W
C#10	2115.00'	922.46'	N10° 38' 32"W
C#11	100.00'	18.94'	N17° 42' 43"W
C#12	100.00'	26.84'	N19° 58' 35"W
C#13	2200.00'	729.43'	N37° 09' 52"W
C#14	1200.00'	55.48'	N43° 31' 06"W
C#15	90.00'	19.98'	N48° 33' 13"W
C#16	90.00'	23.77'	N47° 20' 50"W
C#17	100.00'	49.62'	N53° 59' 45"W
C#18	200.00'	16.42'	N65° 51' 30"W
C#19	500.00'	30.01'	N65° 13' 35"W
C#20	750.00'	27.47'	N65° 53' 48"W
C#21	750.00'	36.42'	N66° 14' 20"W
C#22	1500.00'	97.21'	N69° 29' 12"W
C#23	90.00'	32.02'	N61° 08' 58"W
C#24	500.00'	29.24'	N52° 37' 51"W
C#25	750.00'	20.10'	N55° 04' 26"W
C#26	500.00'	44.45'	N53° 17' 42"W
C#27	500.00'	34.95'	N48° 44' 44"W
C#28	115.00'	77.68'	N27° 23' 32"W
C#29	200.00'	42.94'	N14° 11' 33"W
C#30	100.00'	68.28'	N0° 46' 57"W

CURVE TABLE (TRAIL CL)			
CURVE #	RADIUS	LENGTH	DELTA
C#31	200.00'	42.12'	N24° 48' 45"E
C#32	750.00'	78.84'	N33° 51' 28"E
C#33	1000.00'	35.59'	N35° 50' 58"E
C#34	115.00'	82.38'	N14° 18' 25"E
C#35	1000.00'	47.84'	N4° 50' 45"W
C#36	1000.00'	42.53'	N2° 15' 25"W
C#37	2000.00'	9.01'	N0° 54' 34"W
C#38	22.00'	16.30'	N20° 26' 30"E
C#39	30.00'	18.99'	N23° 31' 40"E
C#40	3770.00'	448.56'	N8° 48' 00"E
C#41	25.00'	24.08'	N15° 23' 06"W
C#42	40.00'	20.34'	N57° 32' 54"W
C#43	10.00'	15.61'	N27° 23' 23"W

<p style="text-align: center;">CITY OF ARATA HUMBOLDT BAY TRAIL NORTH</p> <p style="text-align: center;">GENERAL LINE & CURVE TABLES</p>			
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NOTE:
CLEAR BRUSH AND VEGETATION WITHIN
LIMITS OF DISTURBANCE AS NECESSARY TO
CONSTRUCT PROJECT IMPROVEMENTS, TYP

NATIONAL WILDLIFE REFUGE

✓ TYPICAL TRAIL SECTION TYPE II, SEE SHEET G-5.0 FOR MORE INFORMATION

✓ (E) RR TRACKS TO REMAIN
& BE PROTECTED, TYP

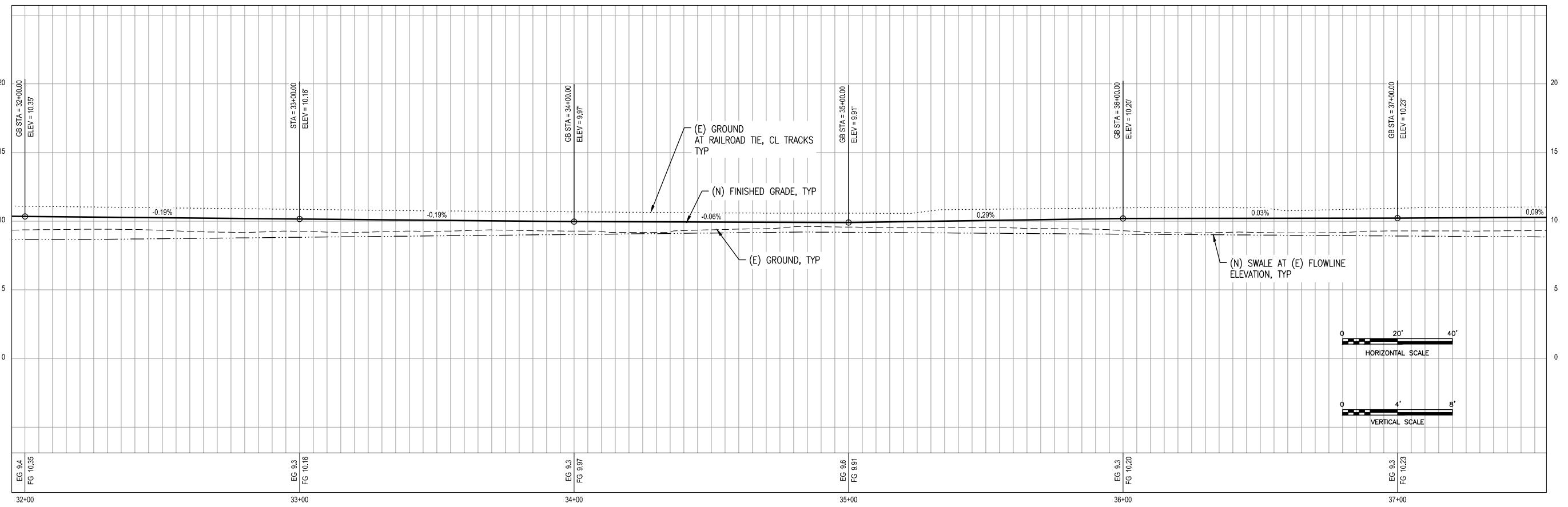
TIDAL ESTUARY

SEE SHEET C-4.0

GENERAL SHEET NOTE

1. THE TRAIL SHALL BE INSTALLED ACCORDING TO THE LINE AND CURVE DATA ON SHEET G-9.0 EXCEPT AS NECESSARY TO MEET THE MINIMUM SEPARATION REQUIREMENTS FROM THE RAILROAD TRACKS. THE MINIMUM SEPARATION (AS MEASURED IN THE FIELD) FROM THE CENTERLINE OF THE TRACKS TO THE EDGE OF THE TRAIL SHALL BE 8.5' AND 9.5' ALONG A STRAIGHT AND CURVED SECTION OF TRACK, RESPECTIVELY.
2. (N) RR WARNING SIGNS @ 2000 FT SPACING, TO BE PROVIDED BY CITY (NIC)

TRAIL - PLAN VIEW



**CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH**

CIVIL
PLAN & PROFILE
STA 32+00 TO STA 37+50

-3-0

OF 52

NATIONAL WILDLIFE REFUGE

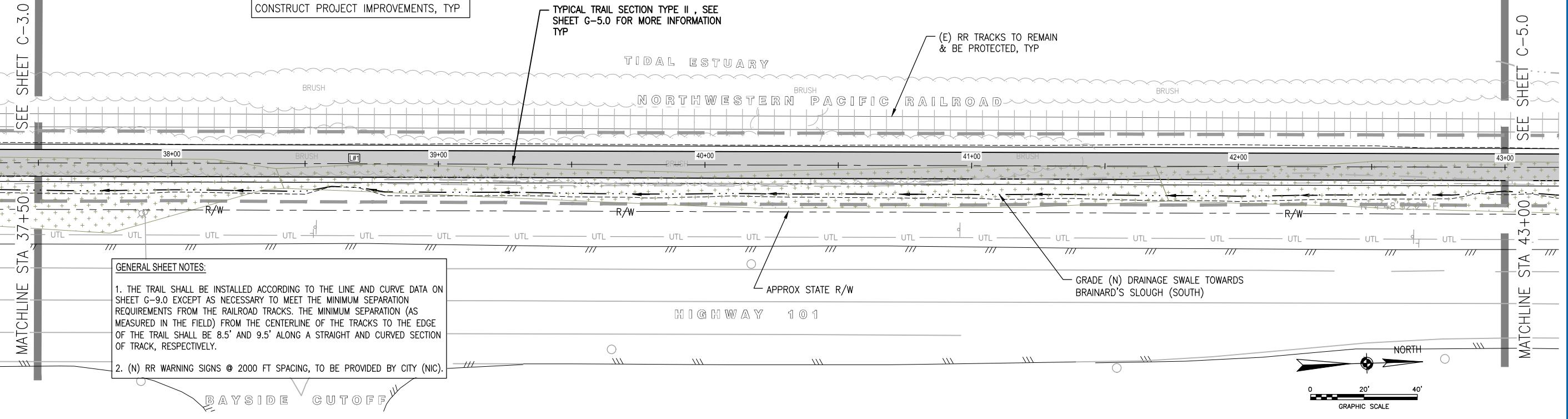
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SHEET G-5.0 FOR MORE INFORMATION
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BRUSH
NORTHWESTERN PACIFIC RAILROAD

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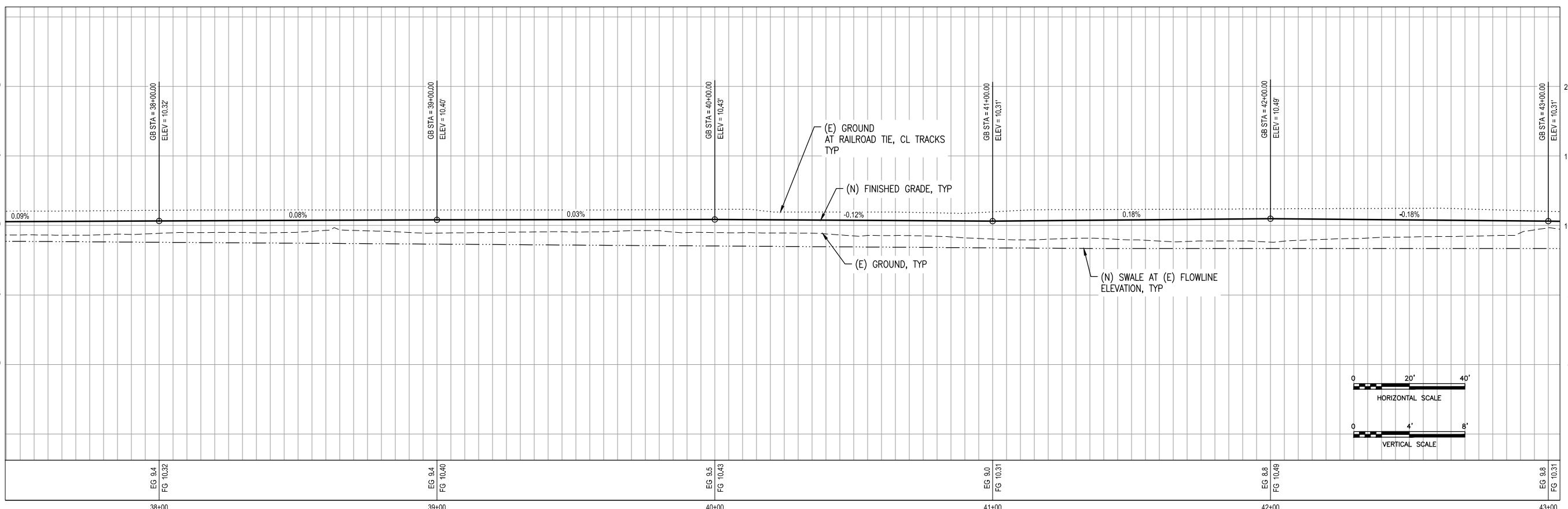
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SHEET 13 OF 52



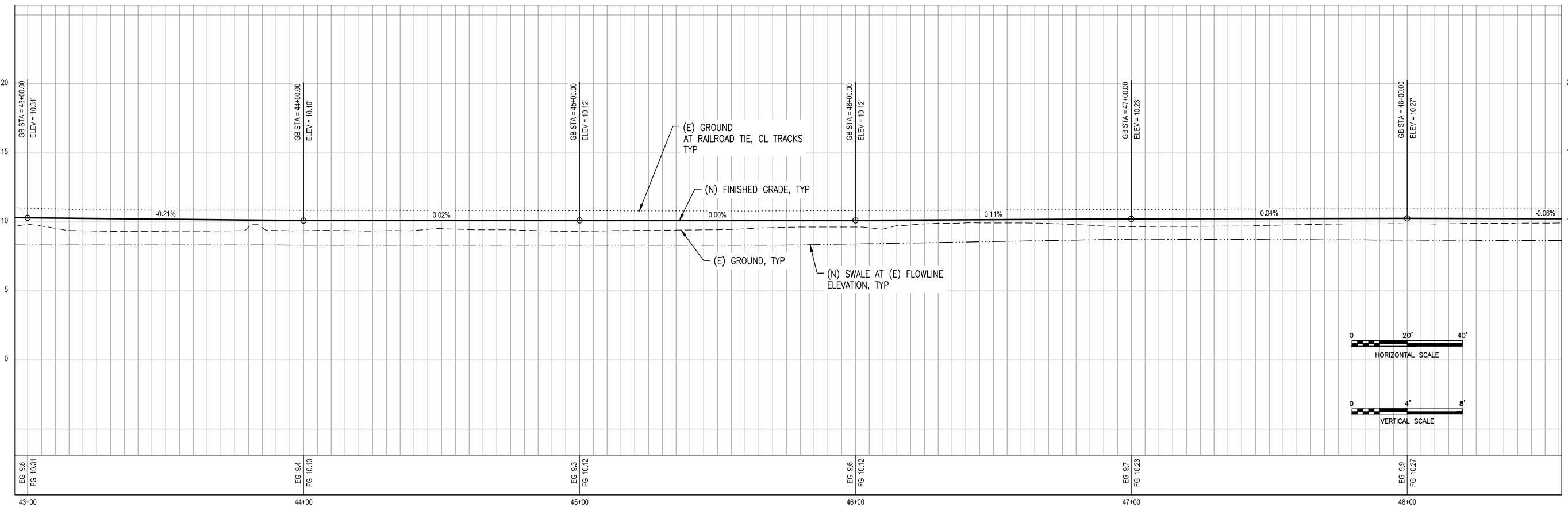
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CONSTRUCT PROJECT IMPROVEMENTS, TYP

✓ TYPICAL TRAIL SECTION TYPE II TYP, SEE SHEET G-5.0 FOR MORE INFORMATION TYP

— (E) RR TRACKS TO REMAIN
& BE PROTECTED, TYP

TRAIL - PLAN VIEW



TRAIL - PROFILE

**CITY OF ARCAT
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**CIVIL
PLAN & PROFILE**

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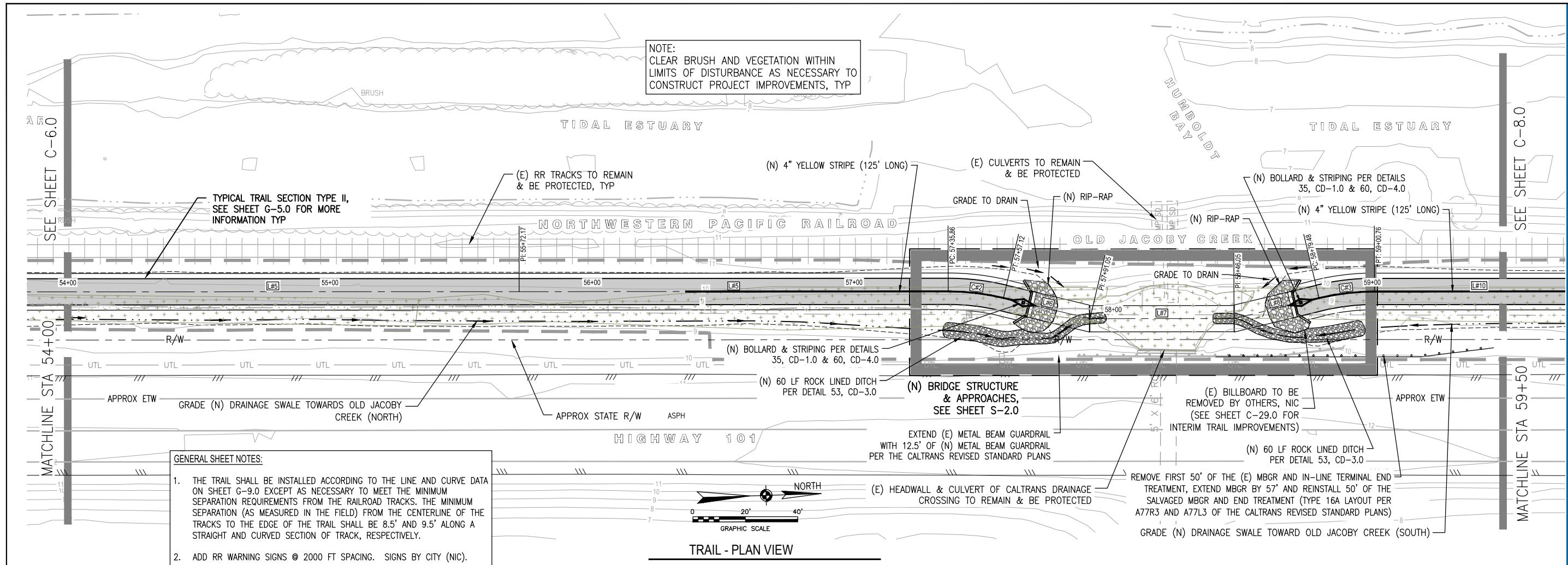
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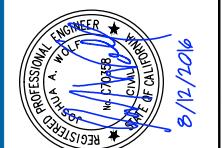
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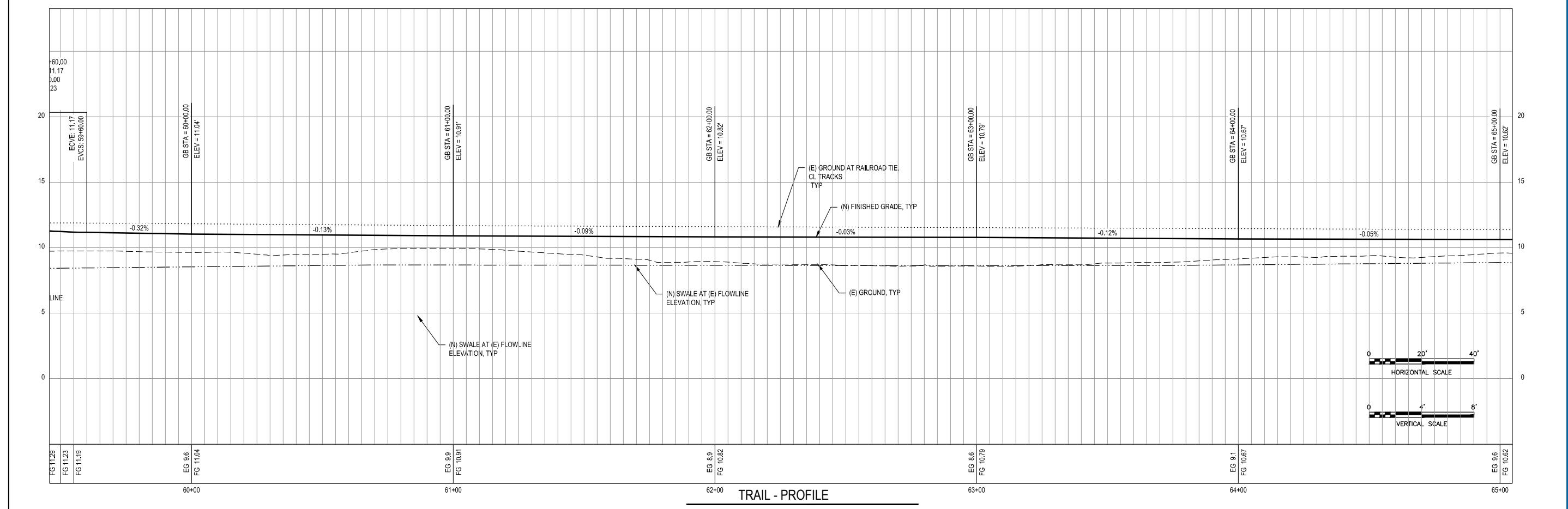
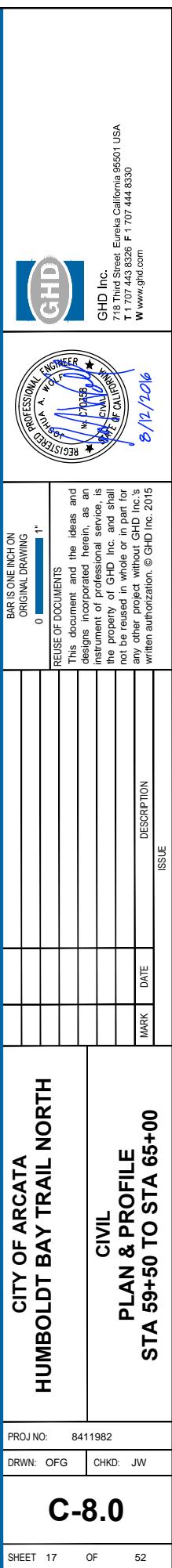
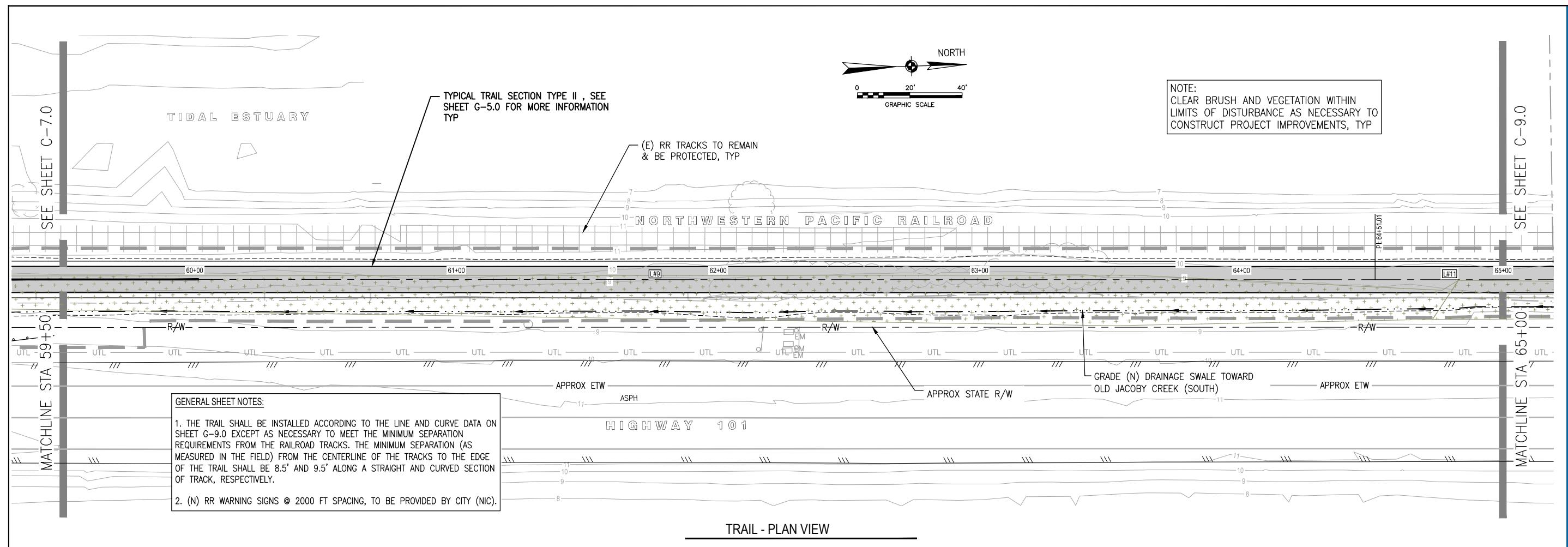
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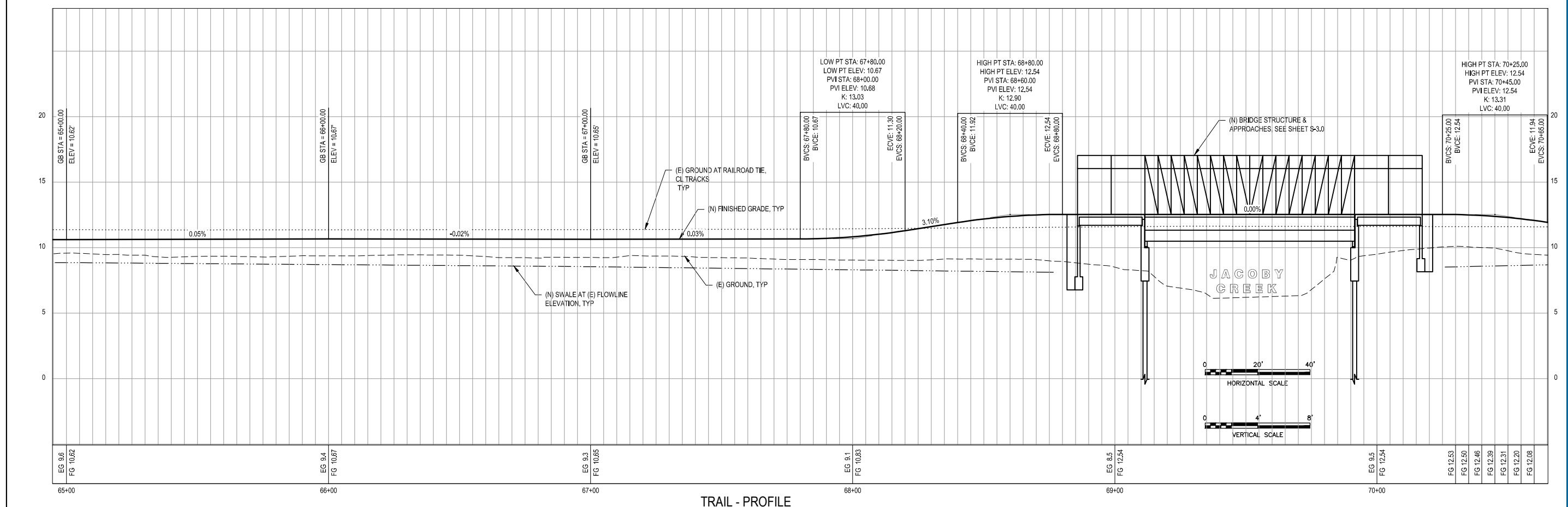
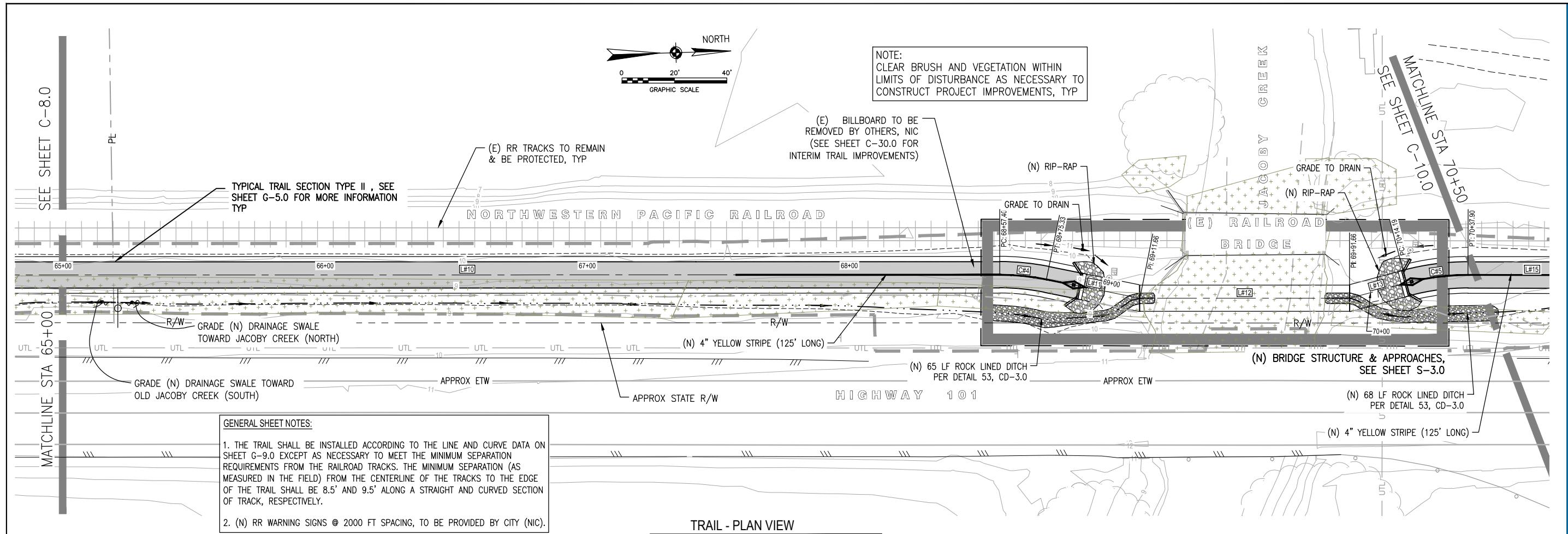
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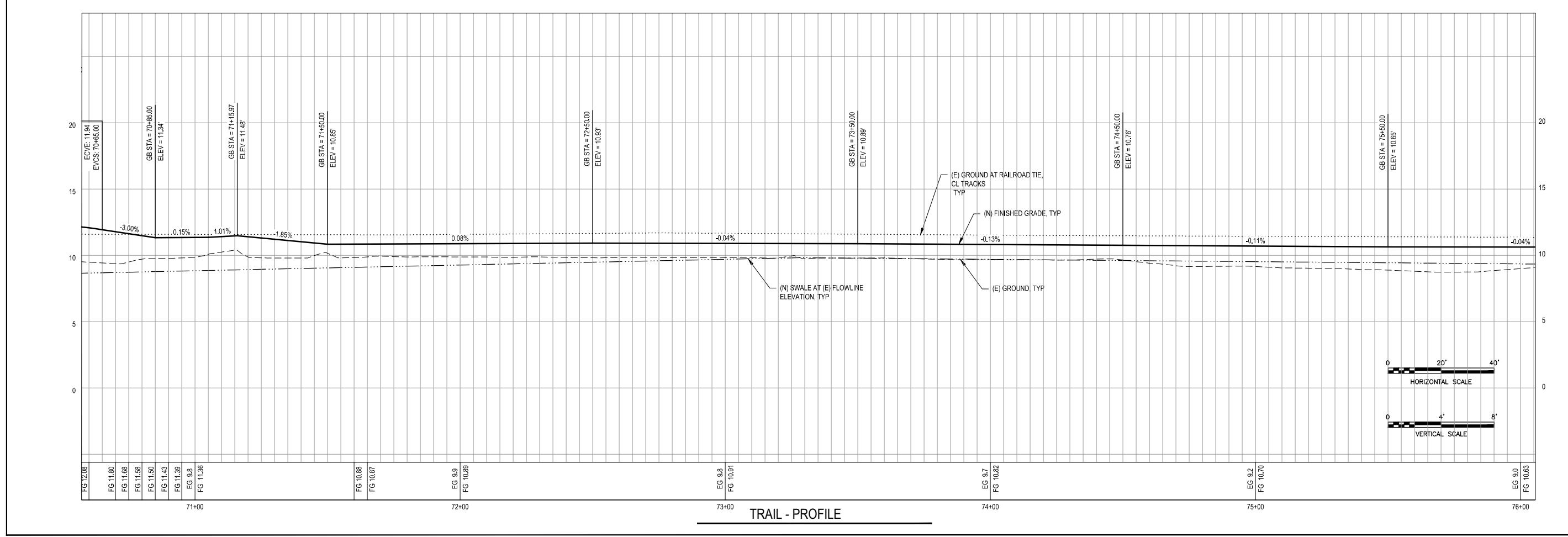
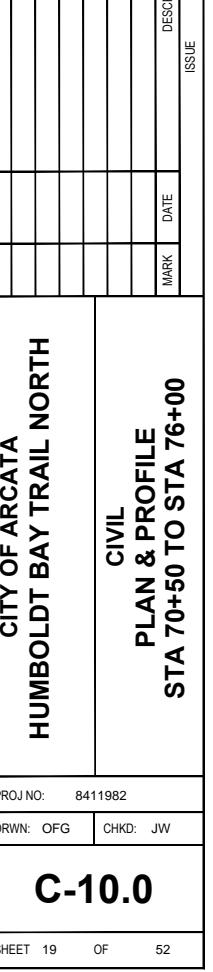
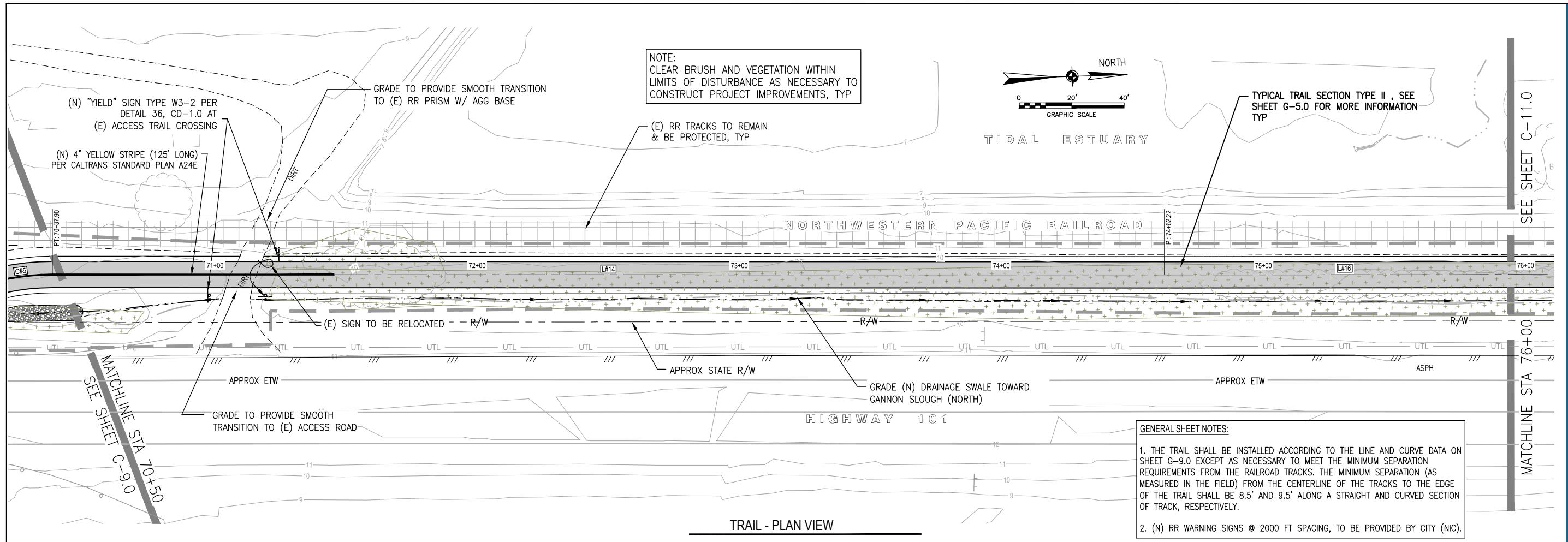
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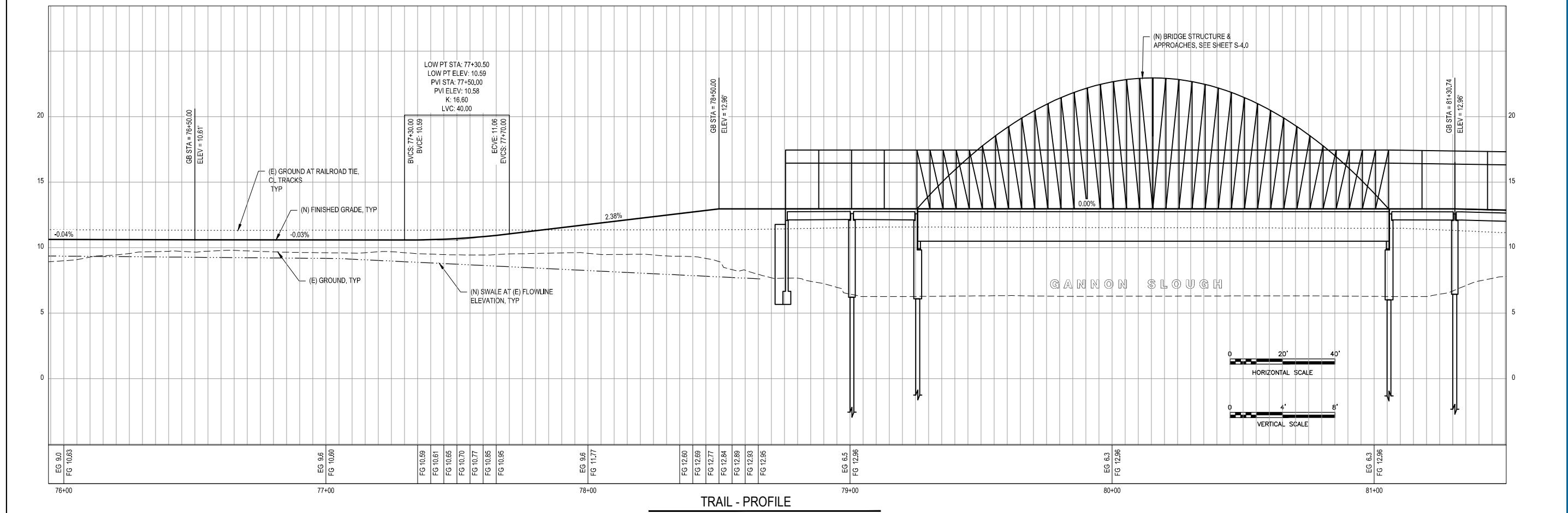
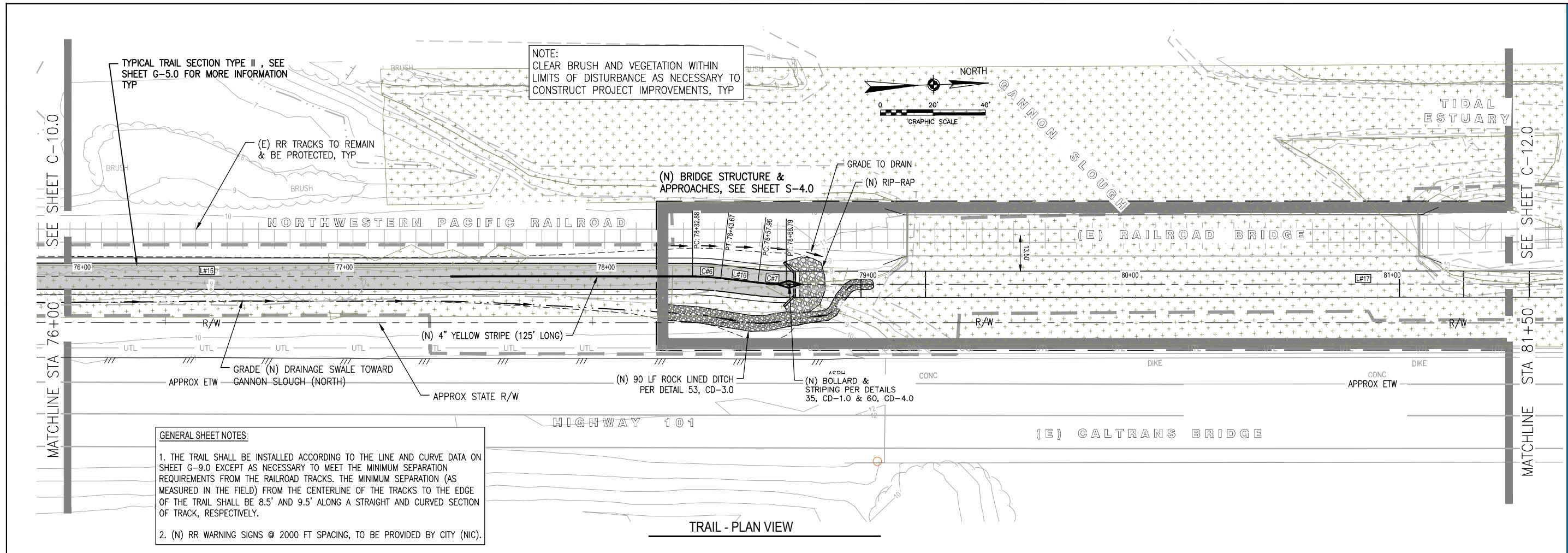
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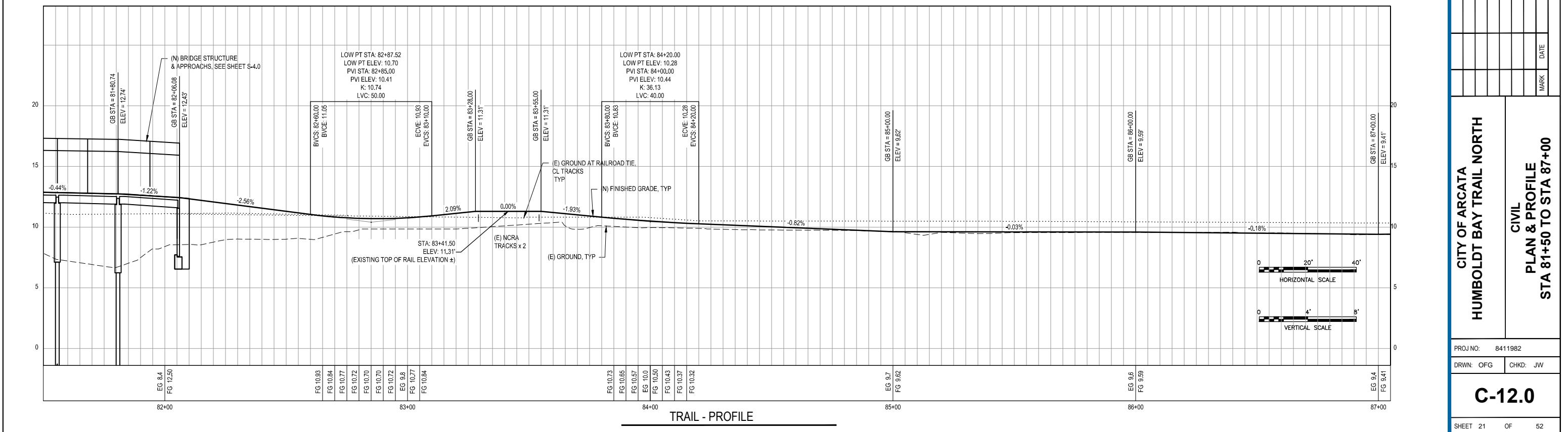
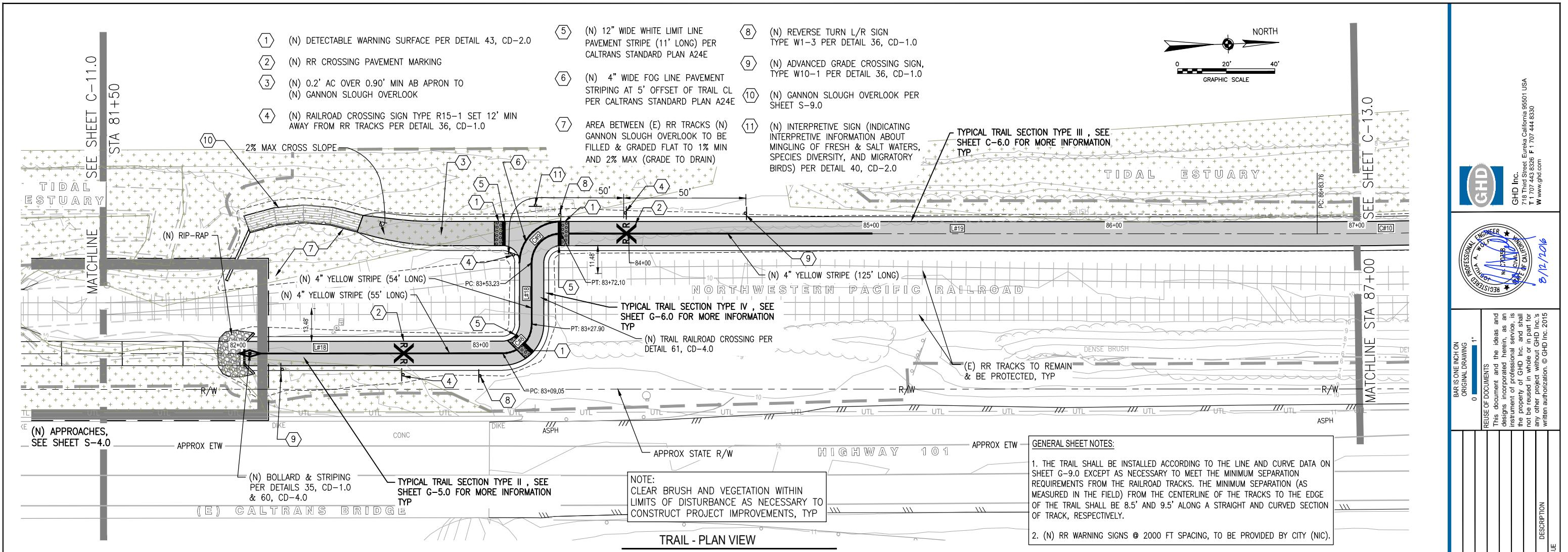
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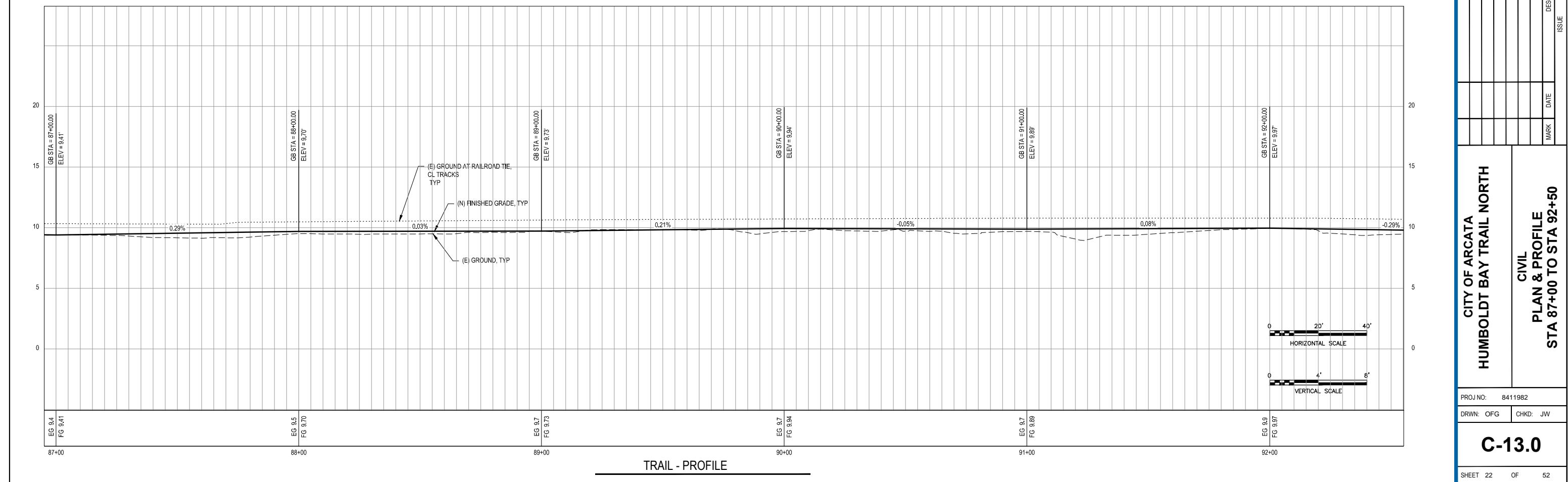
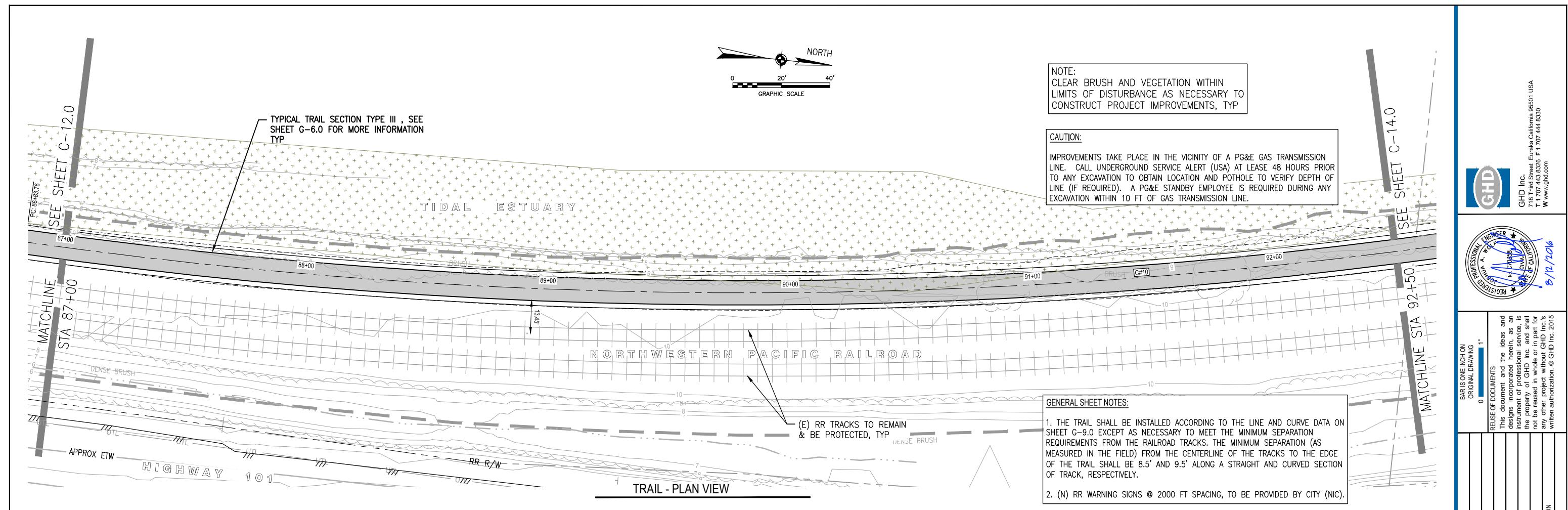


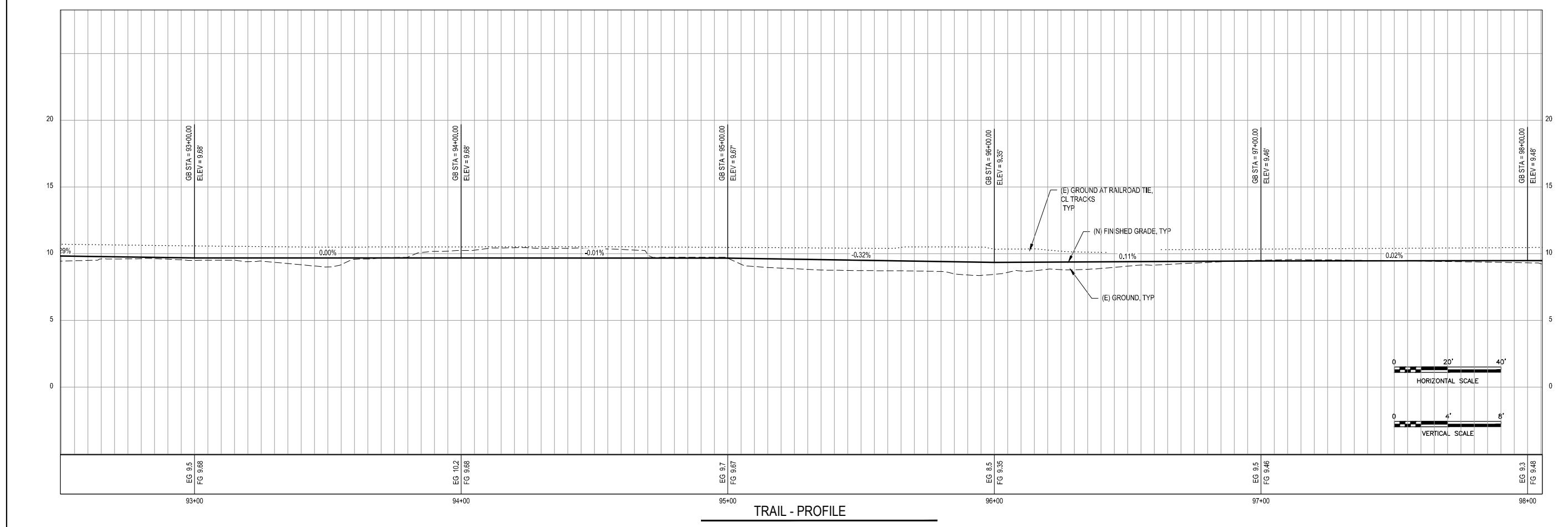
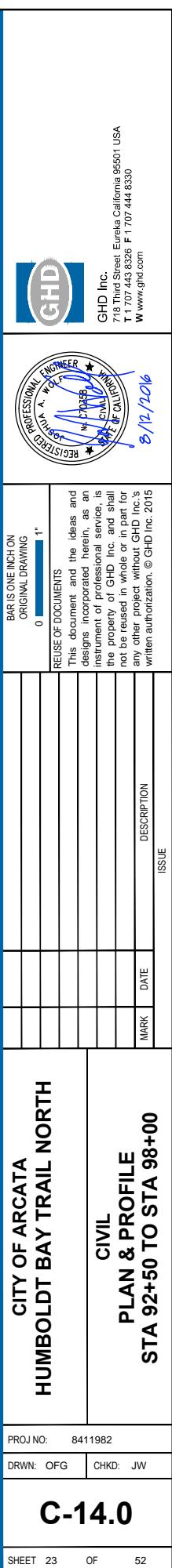
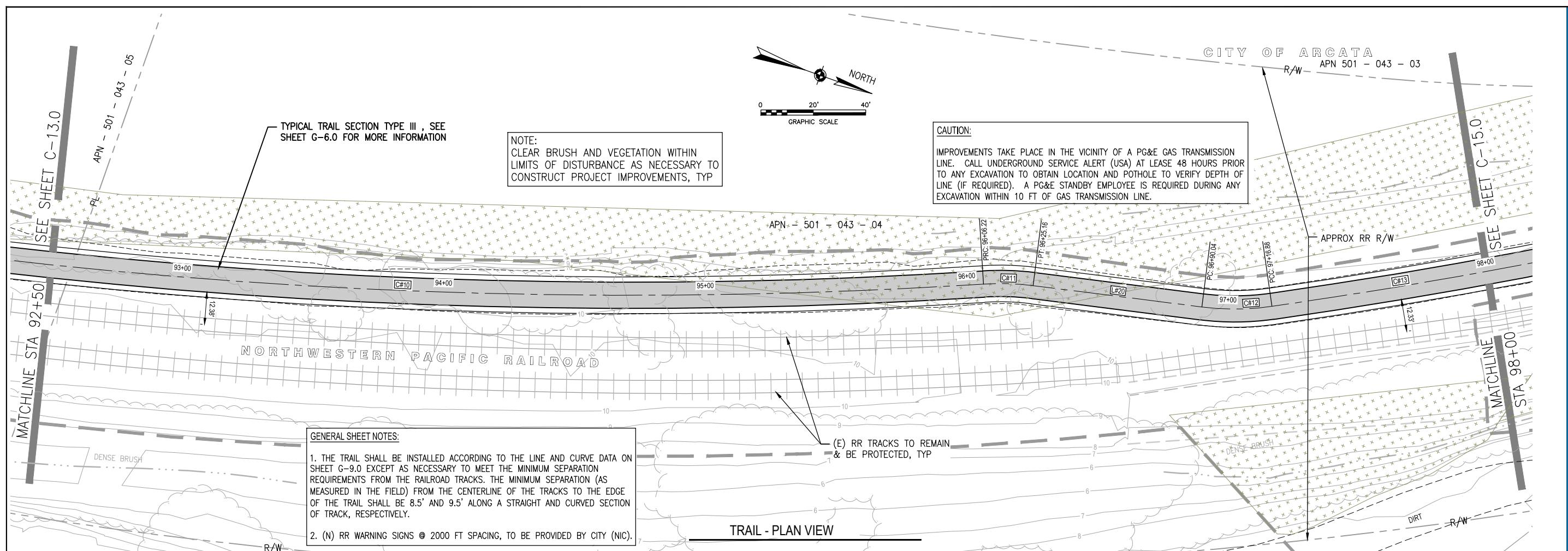


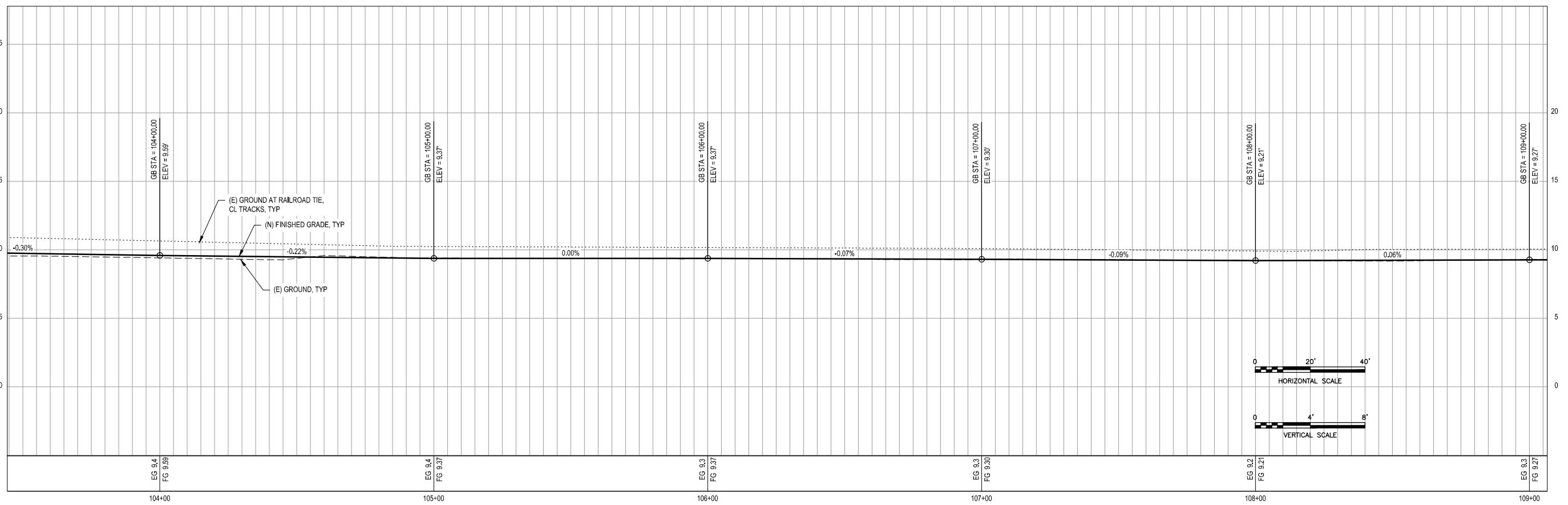
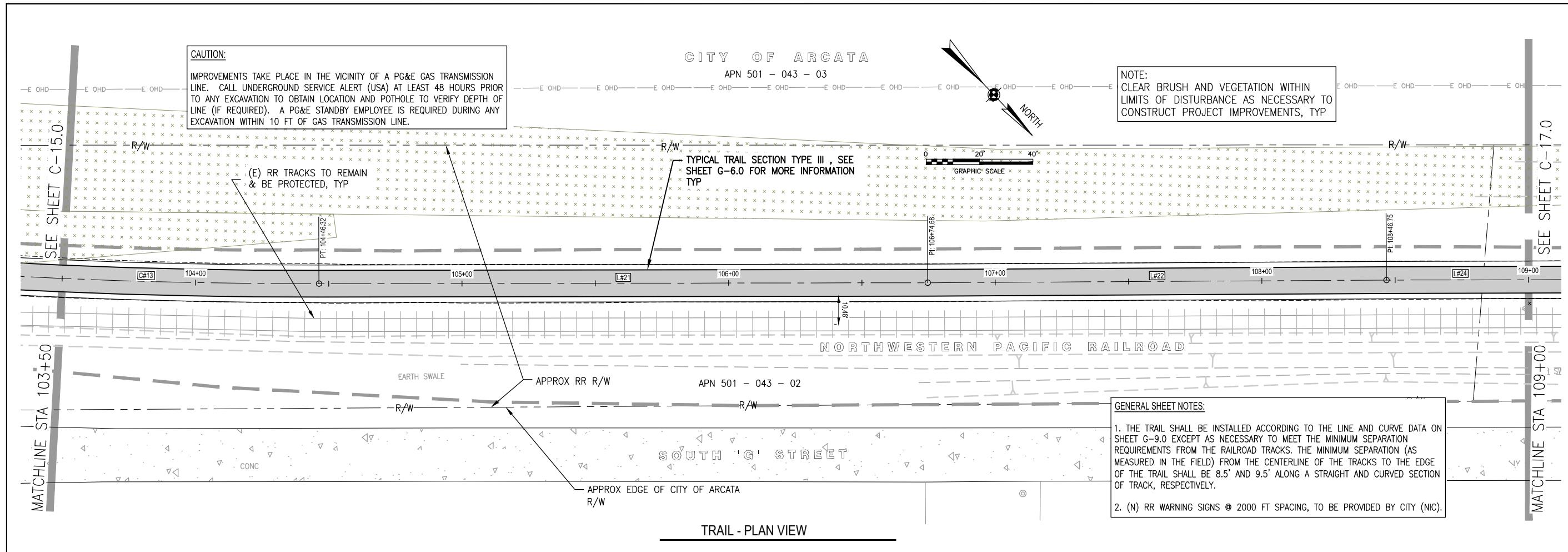




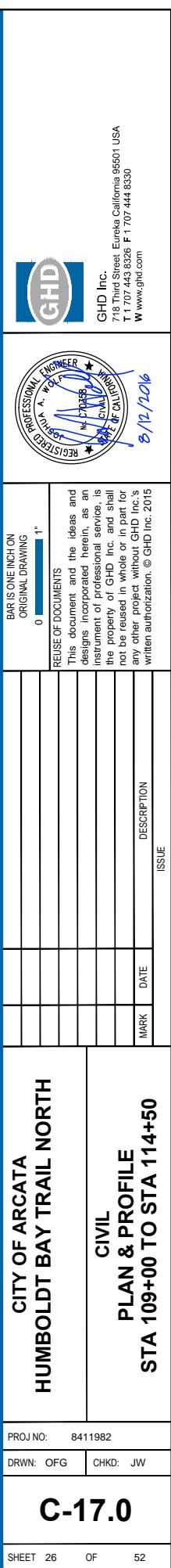
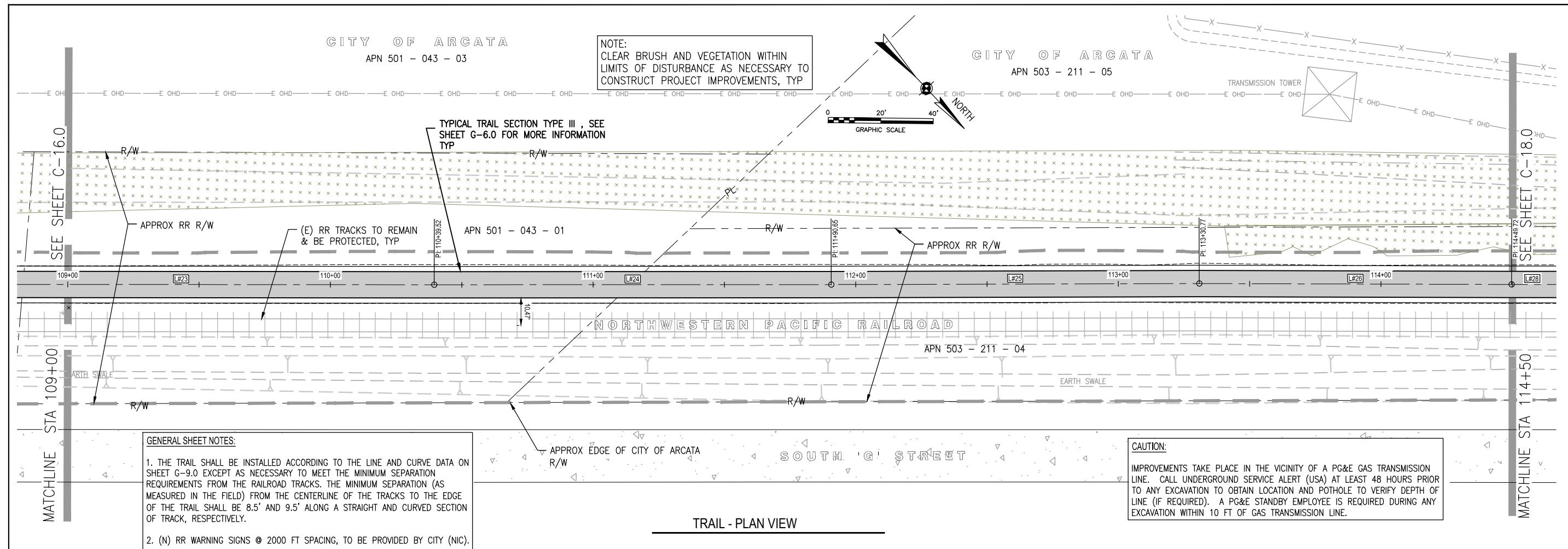


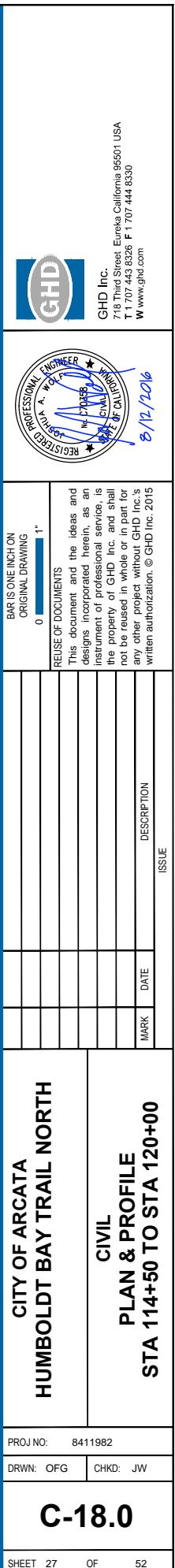
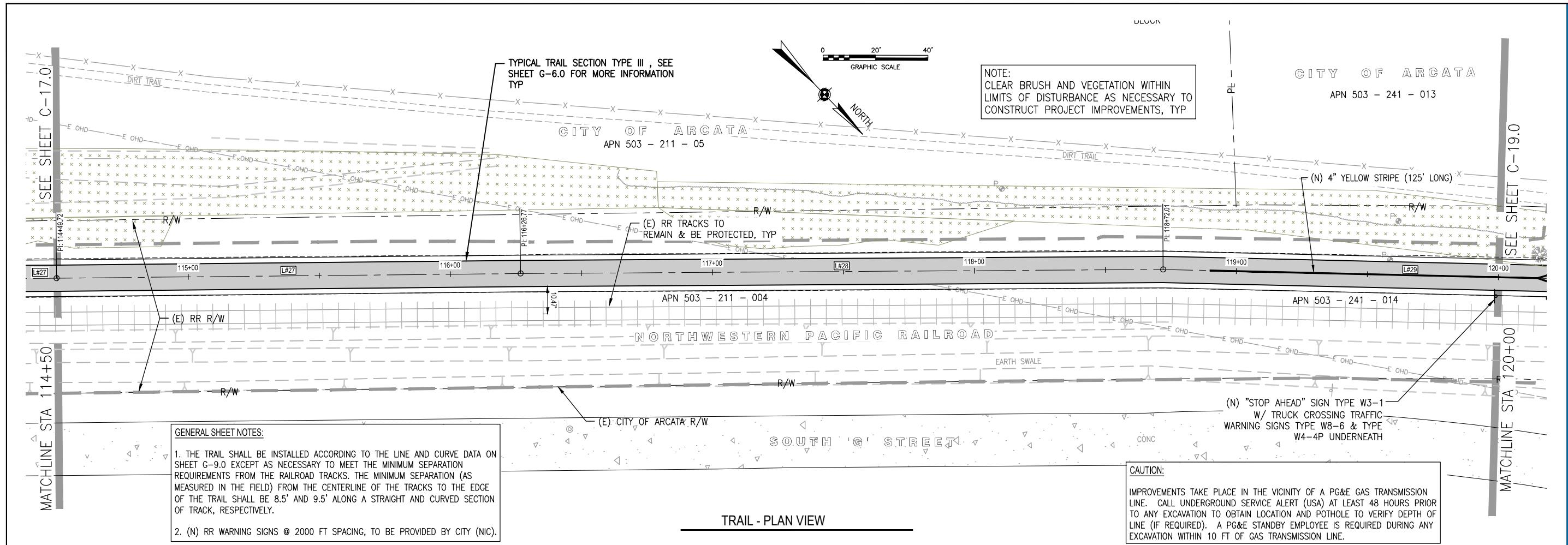


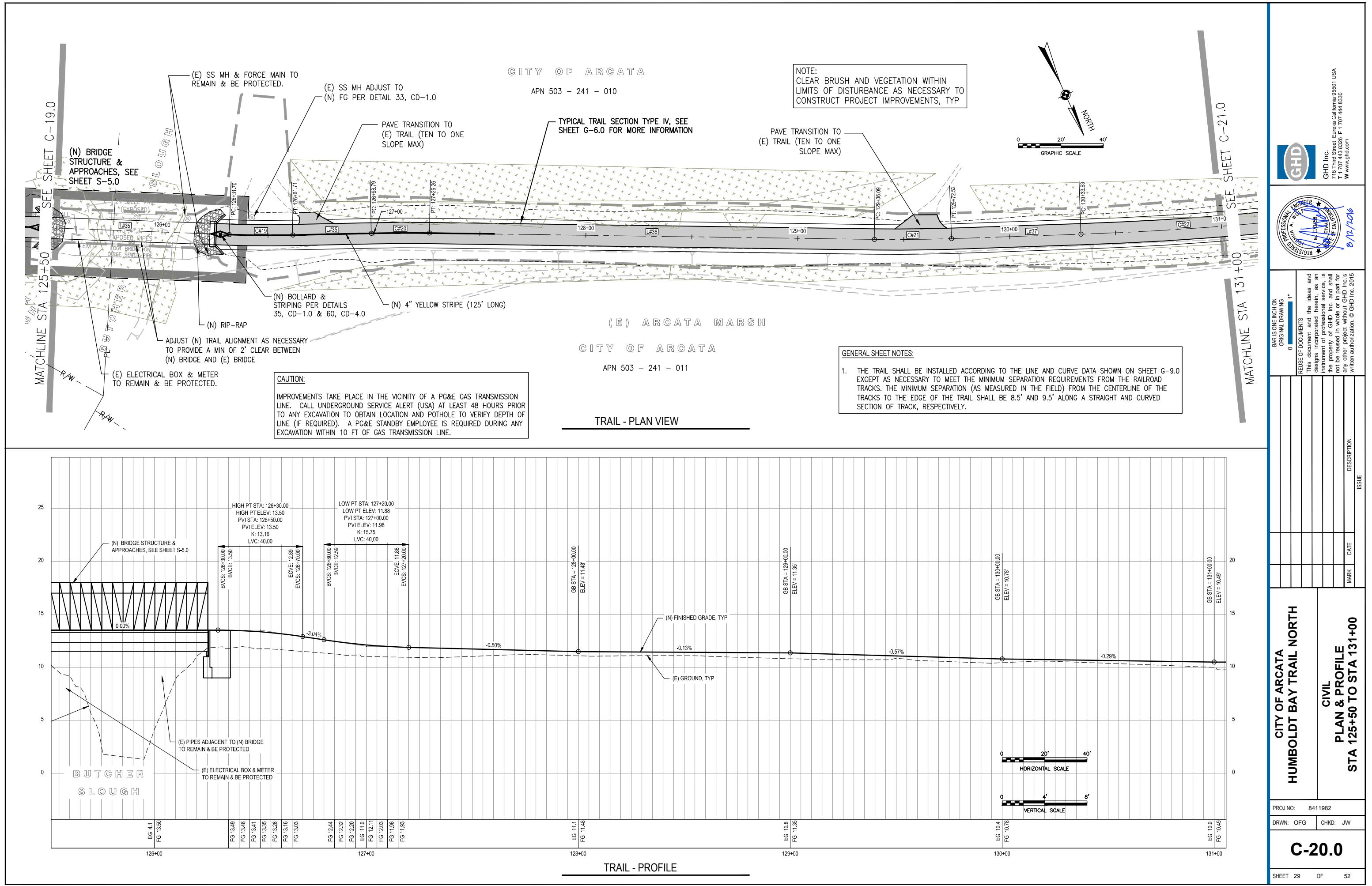


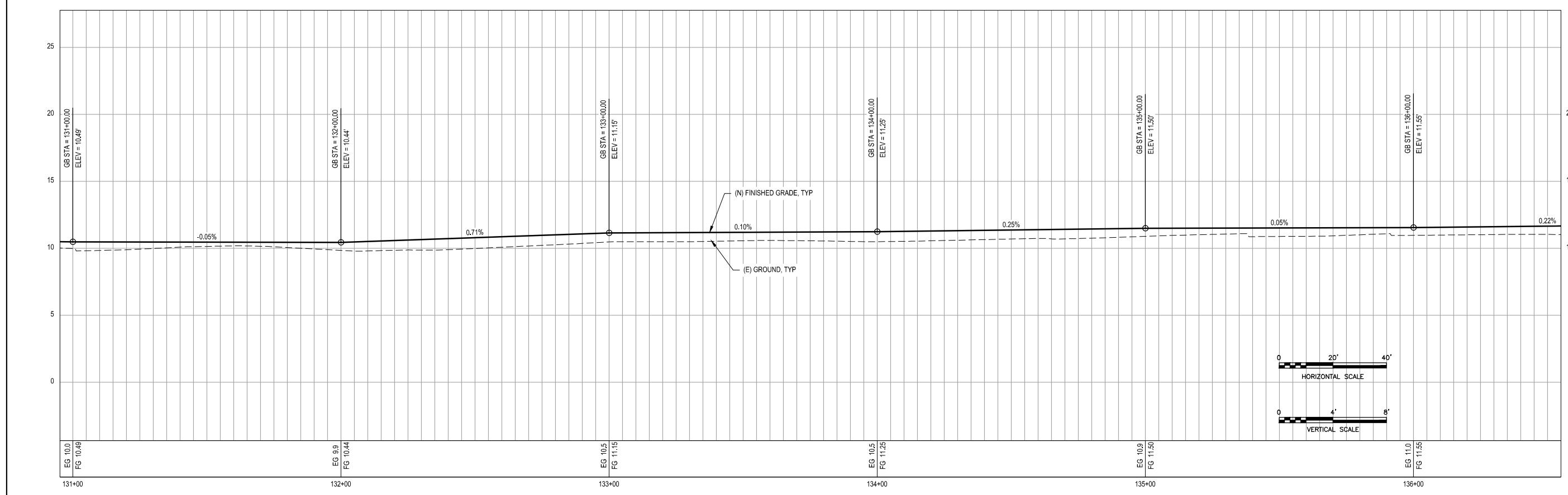
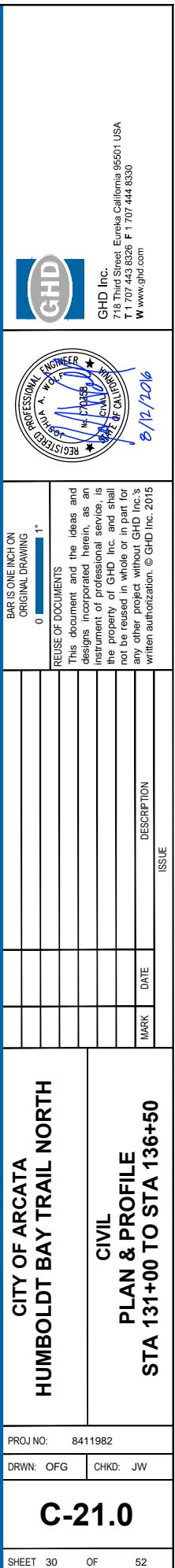
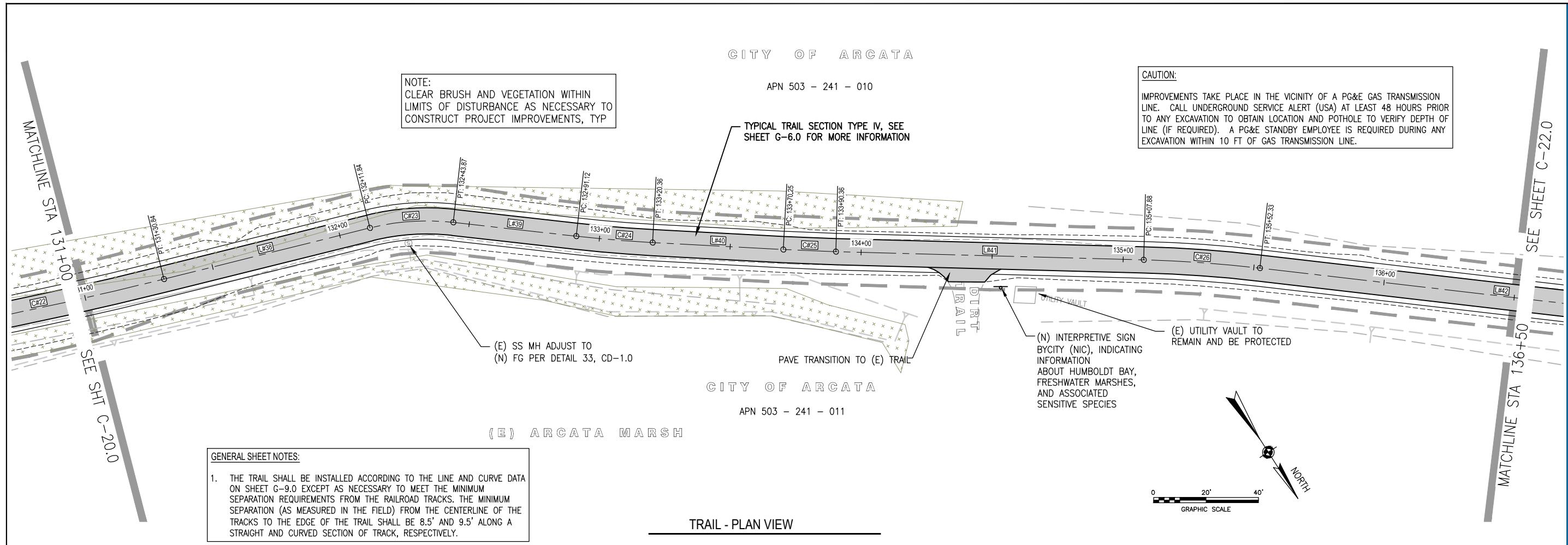


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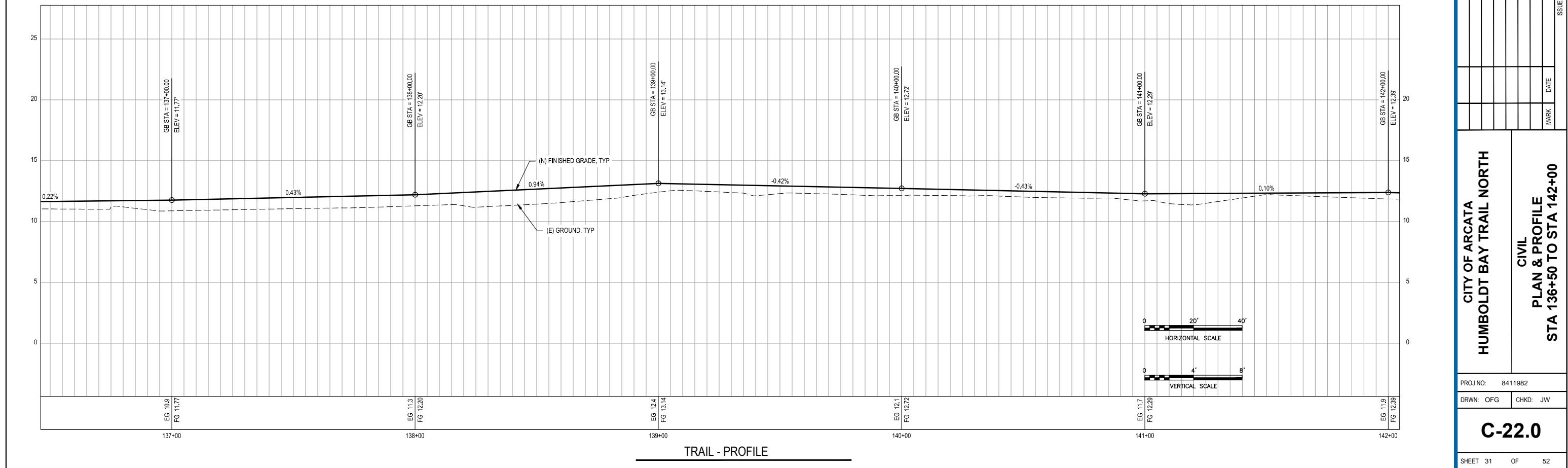
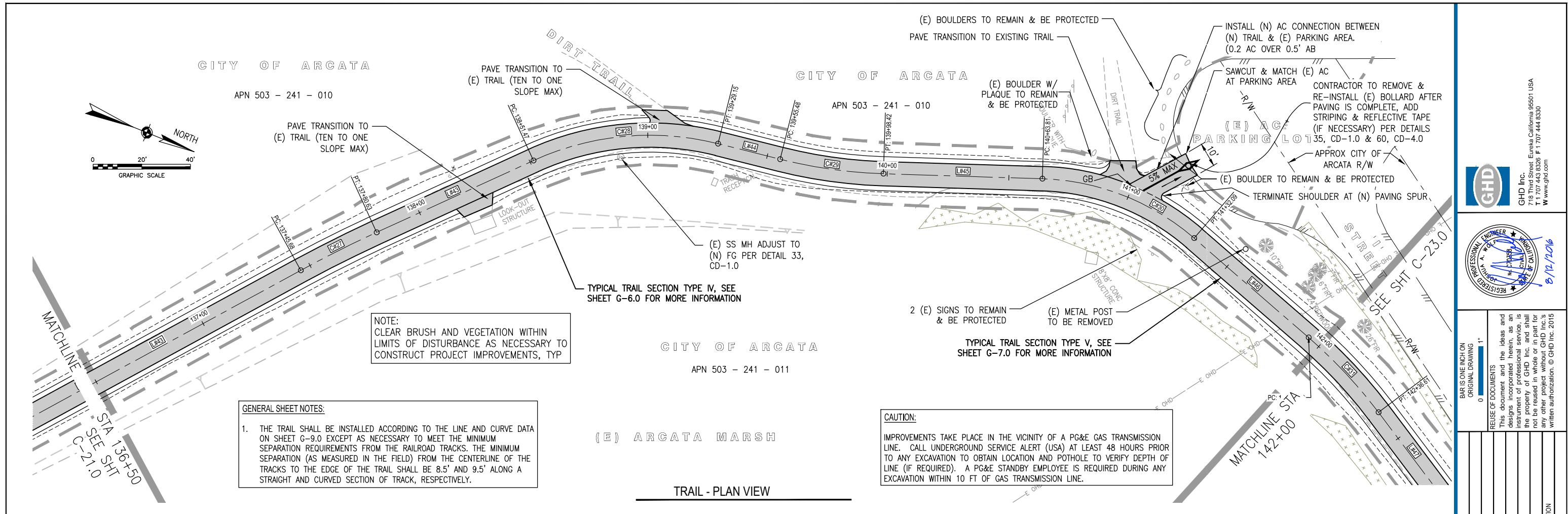


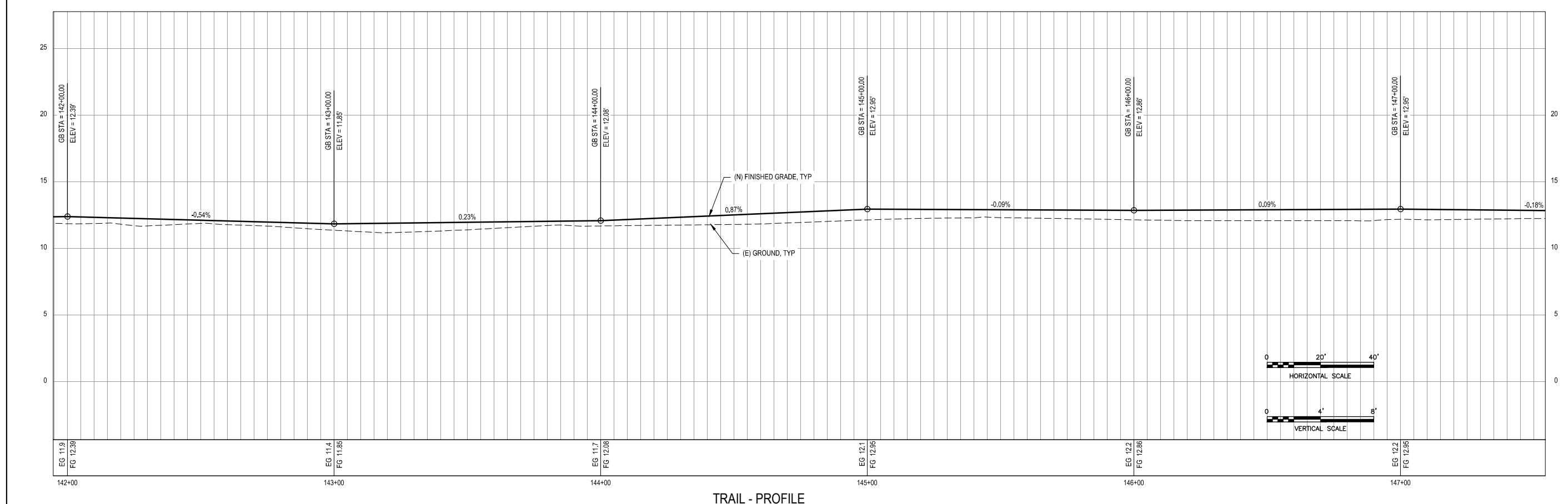
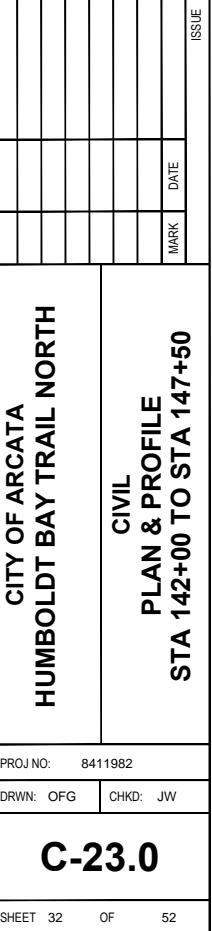
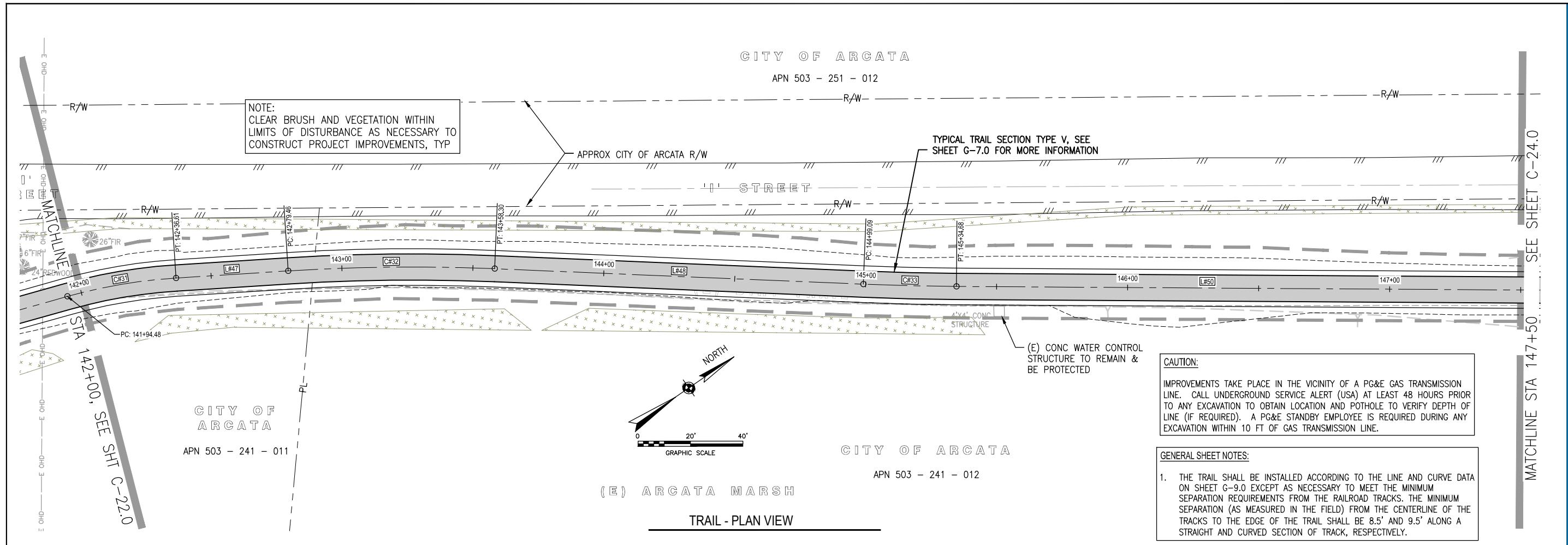


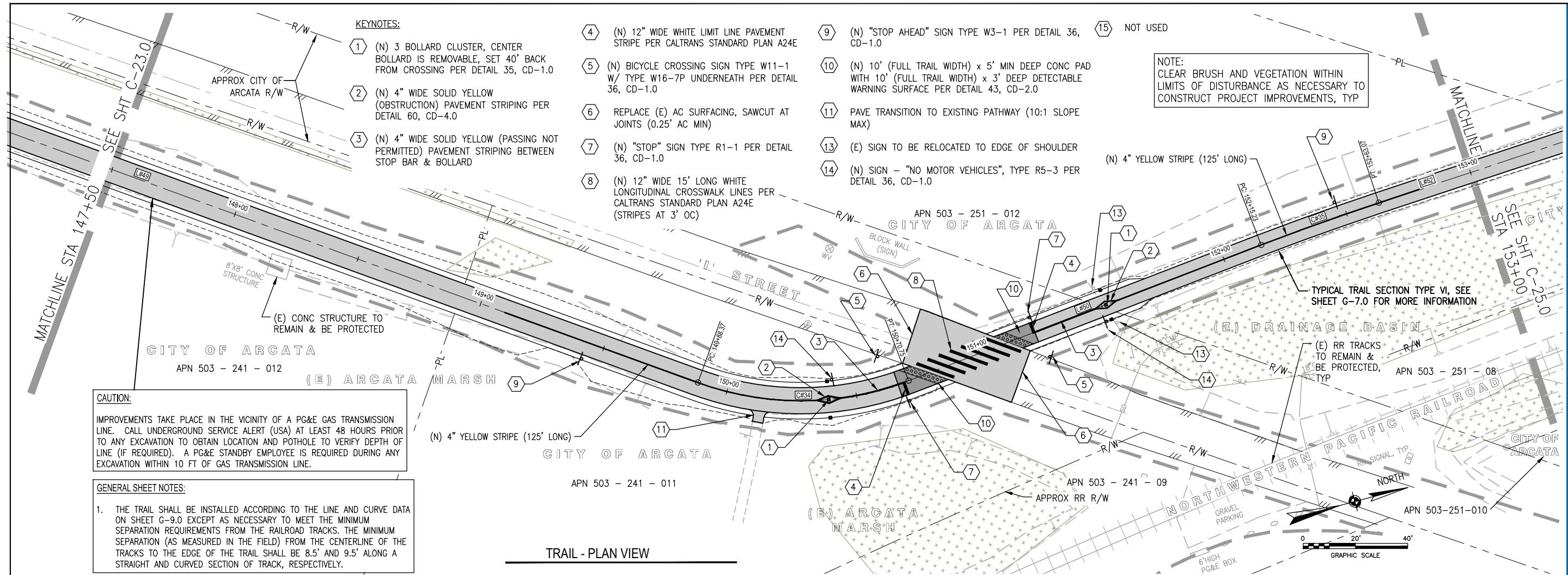




TRAIL - PROFILE







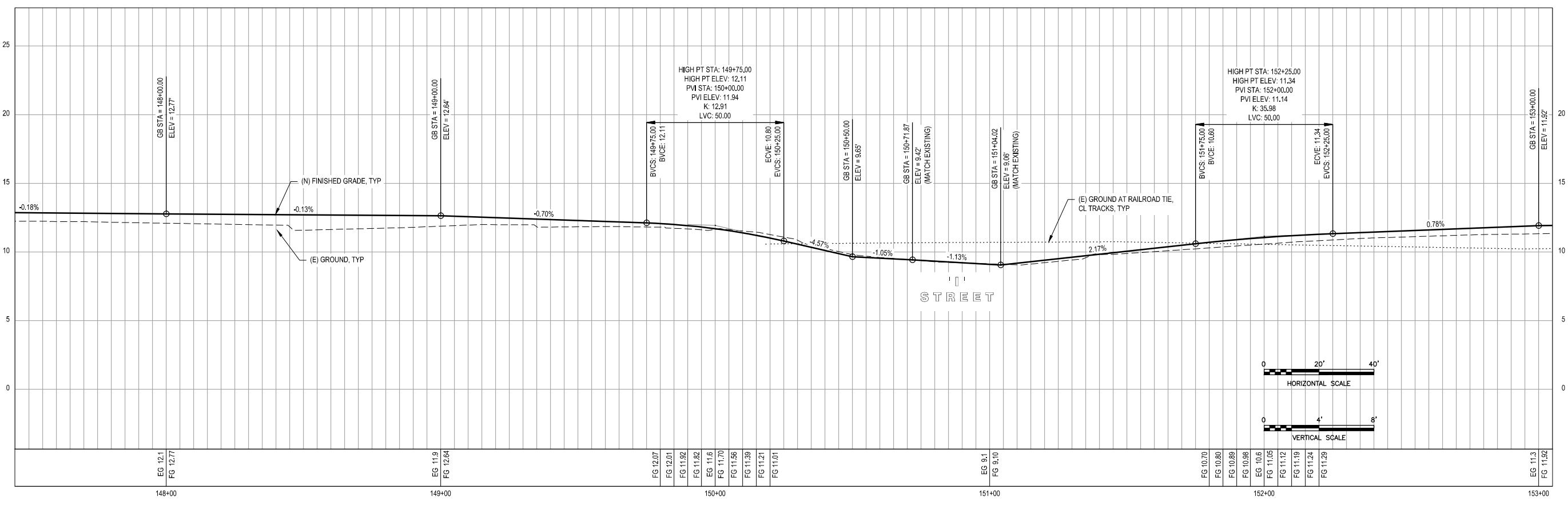
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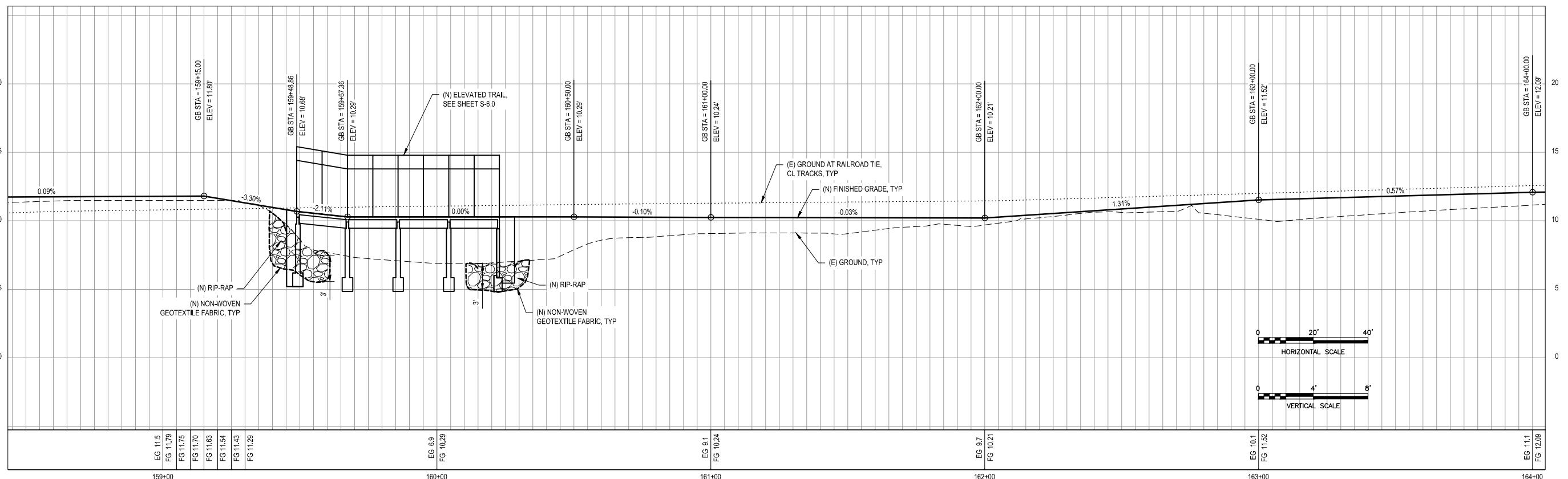
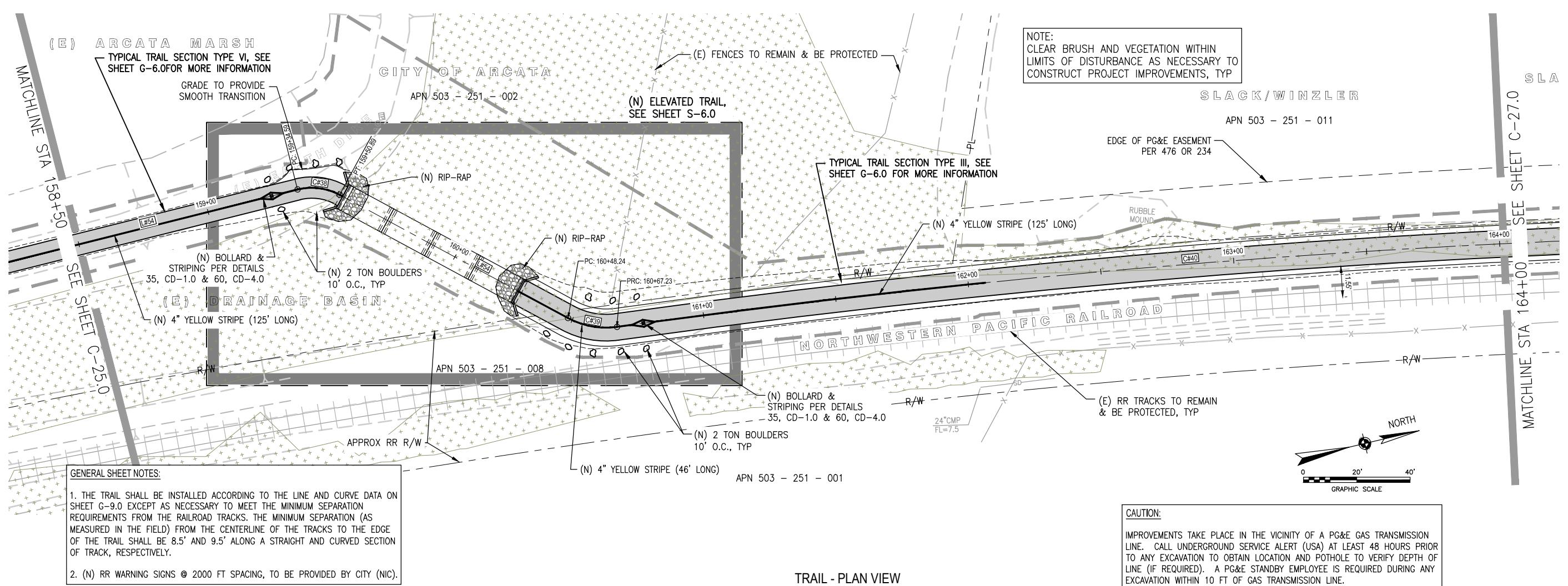
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PROJ NO:	8411982	MARK	DATE	DESCRIPTION	ISSUE
DRWN:	OFG	CHKD:	JW		
CITY OF ARCATA HUMBOLDT BAY TRAIL NORTH CIVIL PLAN & PROFILE STA 147+50 TO STA 153+00					
C-24.0					



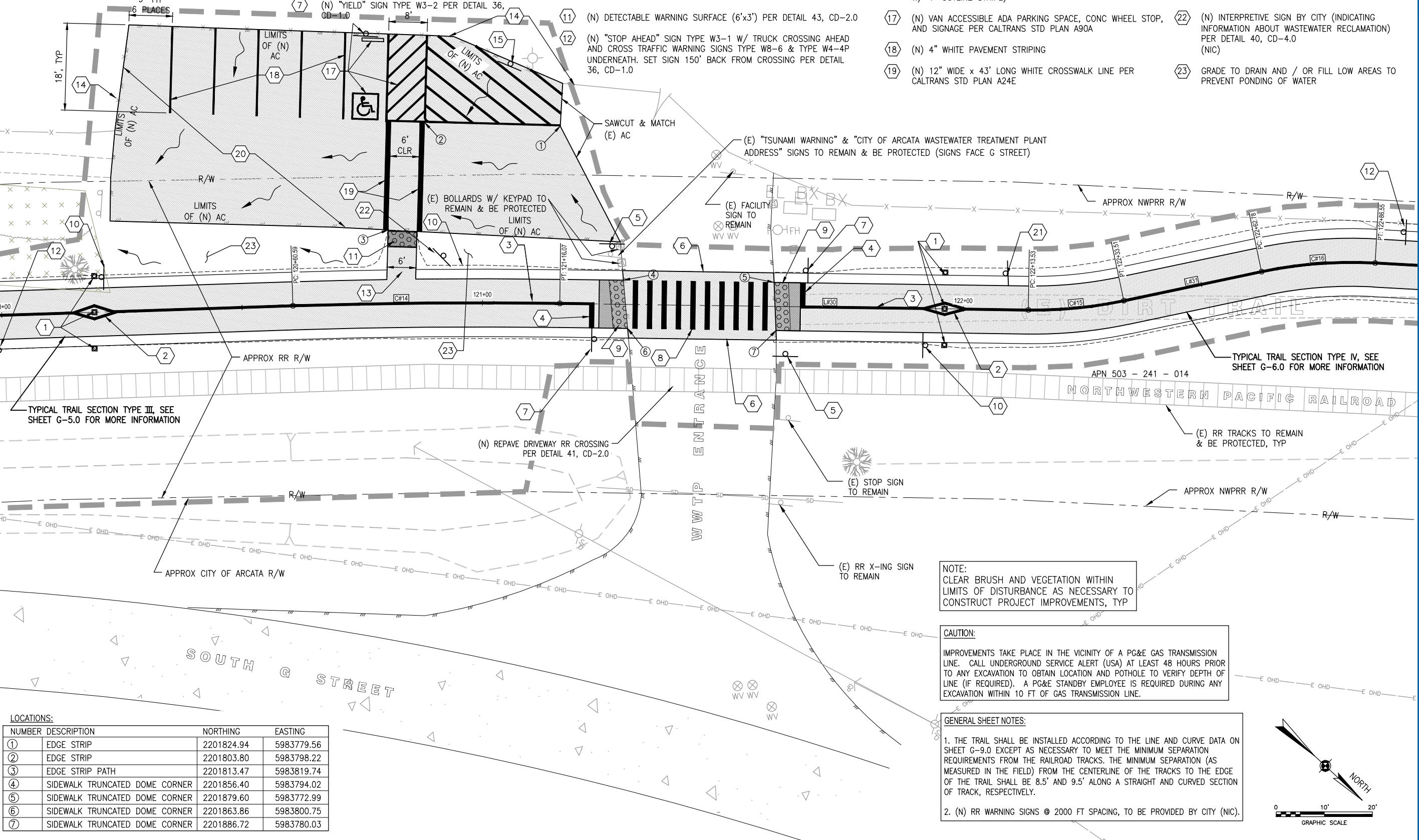


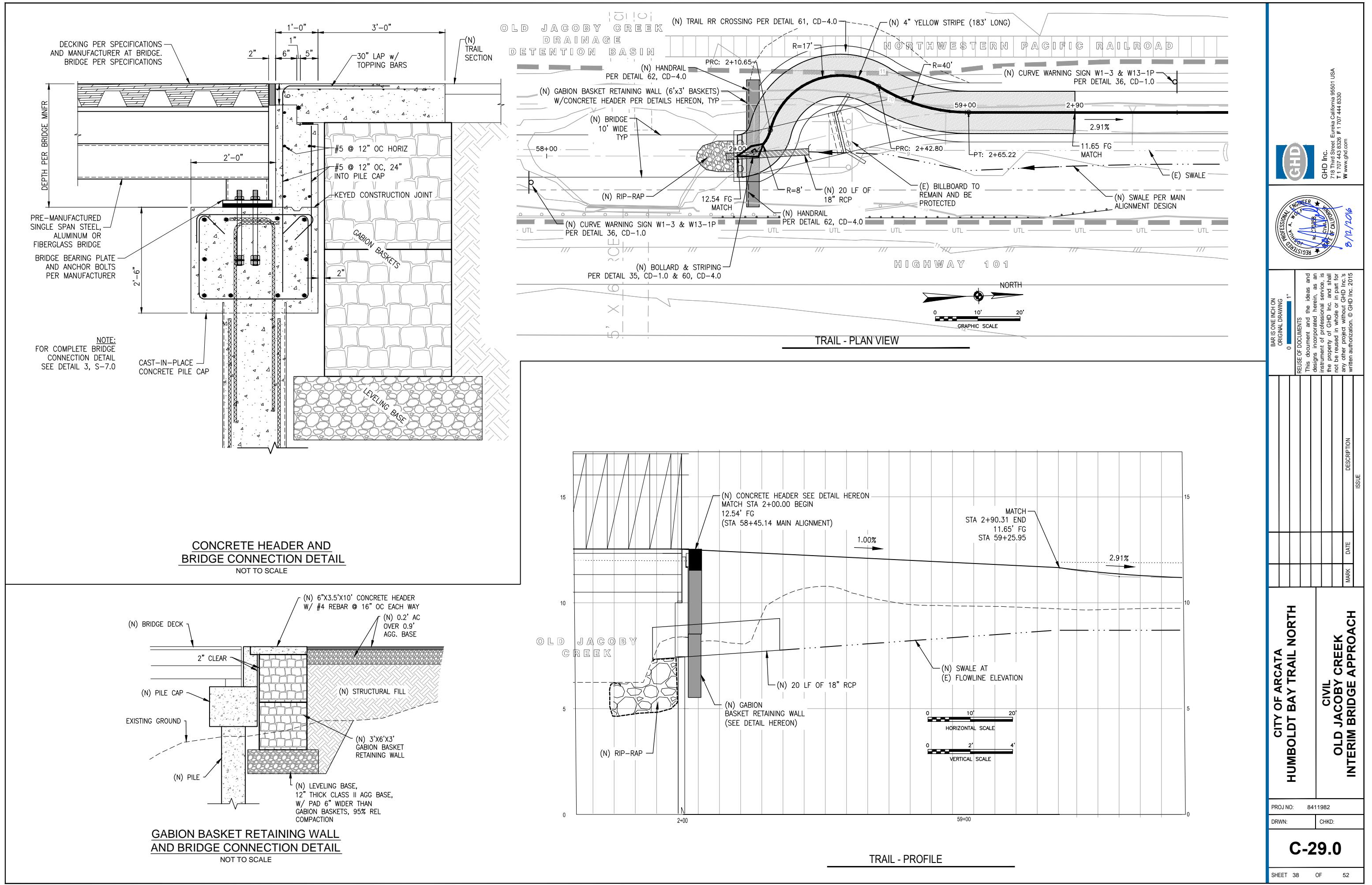
C-26.0

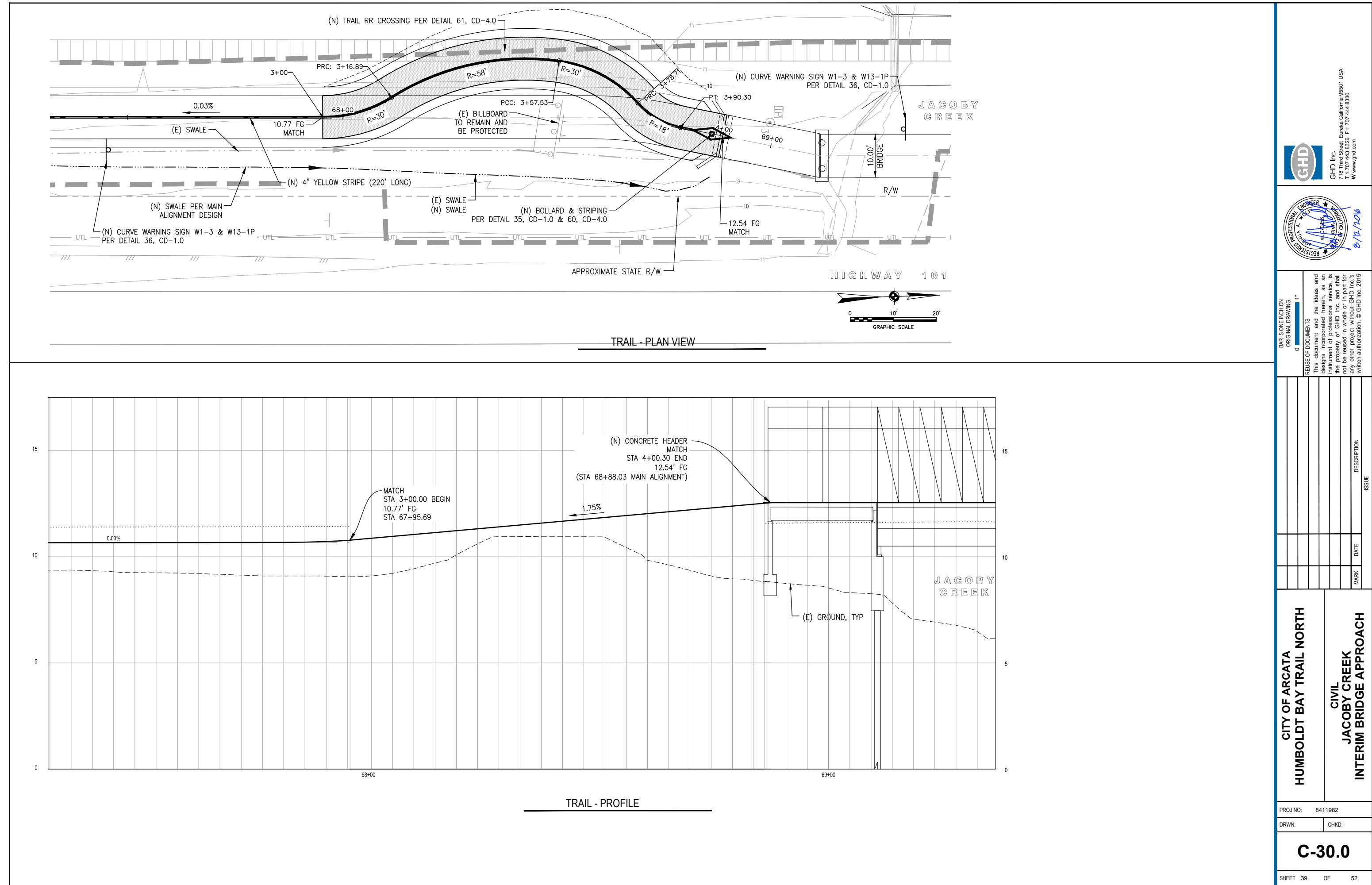
ARCATA WASTEWATER TREATMENT PLANT

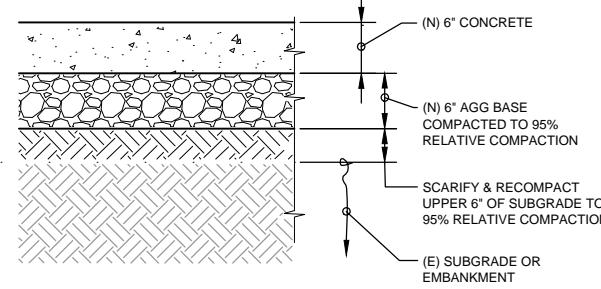
KEYNOTES:

- ① (N) 3 BOLLARD CLUSTER, CENTER BOLLARD IS REMOVABLE, SET 40' BACK FROM CROSSING PER DETAIL 35, CD-1.0
- ② (N) 4" WIDE SOLID YELLOW (OBSTRUCTION) PAVEMENT STRIPING PER DETAIL 60, CD-4.0
- ③ (N) 4" WIDE SOLID YELLOW (PASSING NOT PERMITTED) PAVEMENT STRIPING BETWEEN STOP BAR & BOLLARD
- ④ (N) 12" WIDE 10' WHITE LONGITUDINAL CROSSWALK LINE PER CALTRANS STANDARD PLAN A24E
- ⑤ (N) BICYCLE CROSSING SIGN TYPE W11-1 W/ TYPE W16-7P UNDERNEATH PER DETAIL 36, CD-1.0
- ⑥ REPLACE (E) AC SURFACING, SAWCUT AT JOINTS, (0.25' MIN AC)
- ⑦ (N) "YIELD" SIGN TYPE W3-2 PER DETAIL 36, CD-1.0
- ⑧ (N) 12" WIDE 10' WHITE LIMIT LINE PAVEMENT STRIPE PER CALTRANS STANDARD PLAN A24E (STRIPES AT 3' OC)
- ⑨ (N) 10' (FULL TRAIL WIDTH) x 5' MIN DEEP CONC PAD W/ 10' (FULL TRAIL WIDTH) x 3' DEEP DETECTABLE WARNING SURFACE PER DETAIL 43, CD-2.0
- ⑩ (N) SIGN - "NO MOTOR VEHICLES", TYPE R5-3 PER DETAIL 36, CD-1.0
- ⑪ (N) DETECTABLE WARNING SURFACE (6'x3') PER DETAIL 43, CD-2.0
- ⑫ (N) "STOP AHEAD" SIGN TYPE W3-1 W/ TRUCK CROSSING AHEAD AND CROSS TRAFFIC WARNING SIGNS TYPE W8-6 & TYPE W4-4P UNDERNEATH. SET SIGN 150' BACK FROM CROSSING PER DETAIL 36, CD-1.0
- ⑬ (N) 6' WIDE AC PATH CONNECTION TO (N) TRAIL (0.2' AC OVER 0.5' AB)
- ⑭ (E) FENCE & GATE TO REMAIN & BE PROTECTED
- ⑮ (N) "NO PARKING" SIGN TYPE R8-3 PER DETAIL 36, CD-1.0
- ⑯ (N) 4" DIAGONAL RED PAVEMENT STRIPING (STRIPES AT 3' OC W/ 4" OUTLINE STRIPE)
- ⑰ (N) VAN ACCESSIBLE ADA PARKING SPACE, CONC WHEEL STOP, AND SIGNAGE PER CALTRANS STD PLAN A90A
- ⑱ (N) 4" WHITE PAVEMENT STRIPING
- ⑲ (N) 12" WIDE x 43' LONG WHITE CROSSWALK LINE PER CALTRANS STD PLAN A24E
- ⑳ REMOVE (E) AC WITHIN LIMITS SHOWN, GRADE SUBGRADE TO DRAIN (2% MAX) AND RECOMPACT TO 95% RELATIVE COMPACTION. PLACE 0.25' AC AND GRADE TO DRAIN (2% MAX)
- ㉑ (N) WAY-FINDING SIGN (INDICATING DISTANCE AND DIRECTION TO ARCATA DOWNTOWN, ARCATA MARSH, AND PACIFIC COAST PER DETAIL 36, CD-1.0)
- ㉒ (N) INTERPRETIVE SIGN BY CITY (INDICATING INFORMATION ABOUT WASTEWATER RECLAMATION) PER DETAIL 40, CD-4.0 (NIC)
- ㉓ GRADE TO DRAIN AND / OR FILL LOW AREAS TO PREVENT PONDING OF WATER



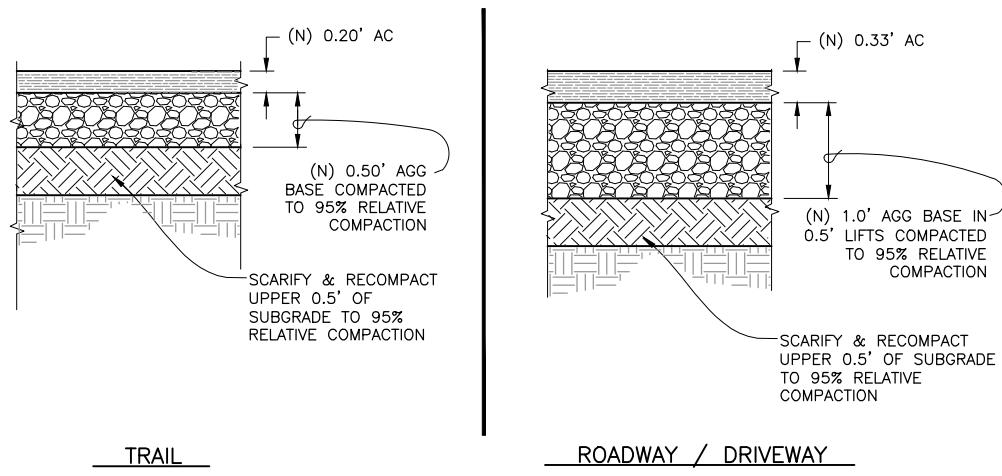






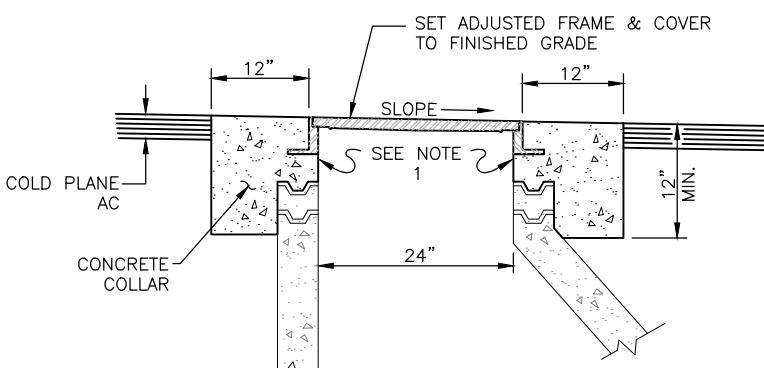
30 TYPICAL CONCRETE PAVEMENT DETAILS

TYP CD-1.0 SCALE: N.T.S.



TRAIL

ROADWAY / DRIVEWAY

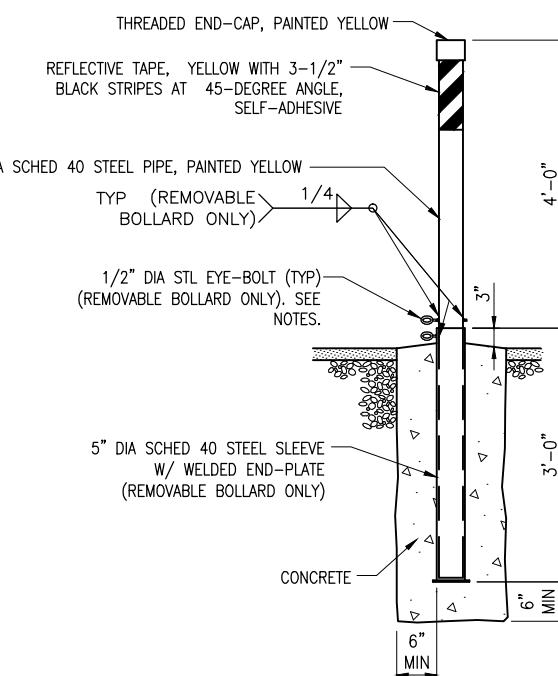


NOTES:

- SHIMS SHALL BE PLACED UNDER THE MANHOLE FRAME & LID ON TOP OF THE HIGHEST GRADE RING AS NEEDED SUCH THAT THE FRAME & LID ARE FLUSH WITH THE FINISHED GRADE OF THE STREET. THE MINIMUM SHIM HEIGHT IS ONE INCH. CONCRETE SHALL BE PLACED IN BETWEEN THE HIGHEST GRADE RING AND THE SHIMMED FRAME & LID WHEN THE CONCRETE COLLAR IS CONSTRUCTED, IN EFFECT CREATING A 'CAST-IN-PLACE' GRADE RING OF VARYING THICKNESS.
- ALL GRADE RINGS SHALL BE SEALED WITH NON-SHRINK GROUT ON BOTH INNER AND OUTER FACES OF EACH JOINT.

TYPICAL ADJUST
MANHOLE TO GRADE DETAIL

TYP CD-1.0 SCALE: N.T.S.



NOTES:

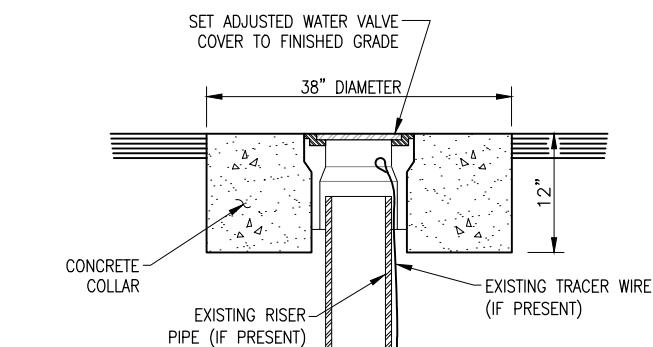
- DRILL 5/8" DIA HOLE THROUGH 4" DIA PIPE AND INSERT EYE-BOLT. WELD EYE-BOLT IN PLACE ON BOTH SIDES OF PIPE (REMOVABLE BOLLARDS ONLY).
- CUT SECOND EYE-BOLT AND WELD EYE TO 5" DIA SLEEVE (REMOVABLE BOLLARDS ONLY).
- EYE-BOLTS SHALL BE NO MORE THAN 1/2" APART TO PERMIT PAD LOCKING (REMOVABLE BOLLARDS ONLY).
- HOT DIP GALVANIZE ALL STEEL AFTER FABRICATION.
- ALL BOLLARDS SHALL BE PERMANENT (NON-REMOVABLE) UNLESS OTHERWISE NOTED ON PLANS.

35 TYPICAL BOLLARD DETAIL

TYP CD-1.0 SCALE: N.T.S.

32 NOT USED

TYP CD-1.0 SCALE: N.T.S.

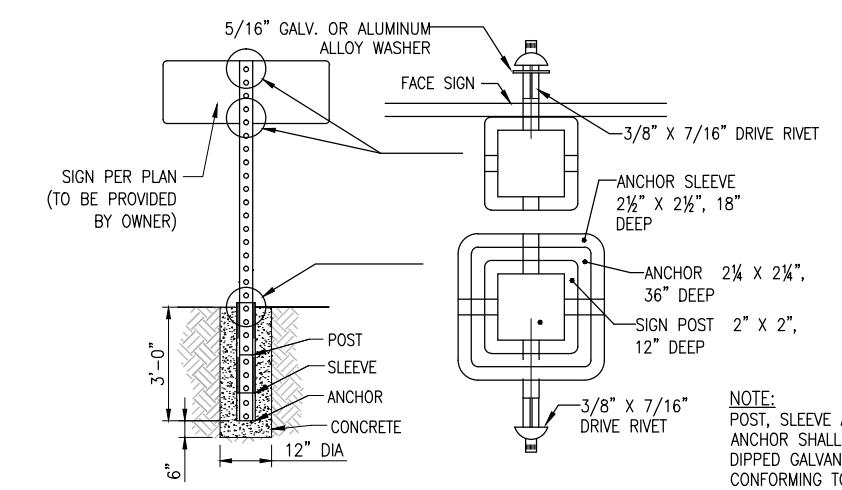
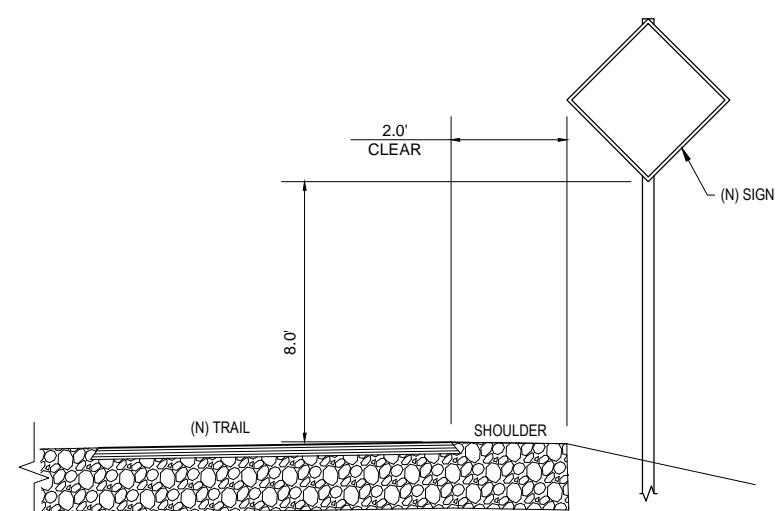


NOTES:

- THE CONTRACTOR SHALL COMPLETELY DEMOLISH EXISTING CONCRETE COLLAR TO A DEPTH OF 12 INCHES.

TYPICAL ADJUST WATER
VALVE COVER TO GRADE DETAIL

TYP CD-1.0 SCALE: N.T.S.



ELEVATION

PLAN VIEW

36 TYPICAL SIGN POST INSTALLATION, 1- POST DETAIL

TYP CD-1.0 SCALE: N.T.S.

CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH
CIVIL DETAILS
STANDARD DETAILS

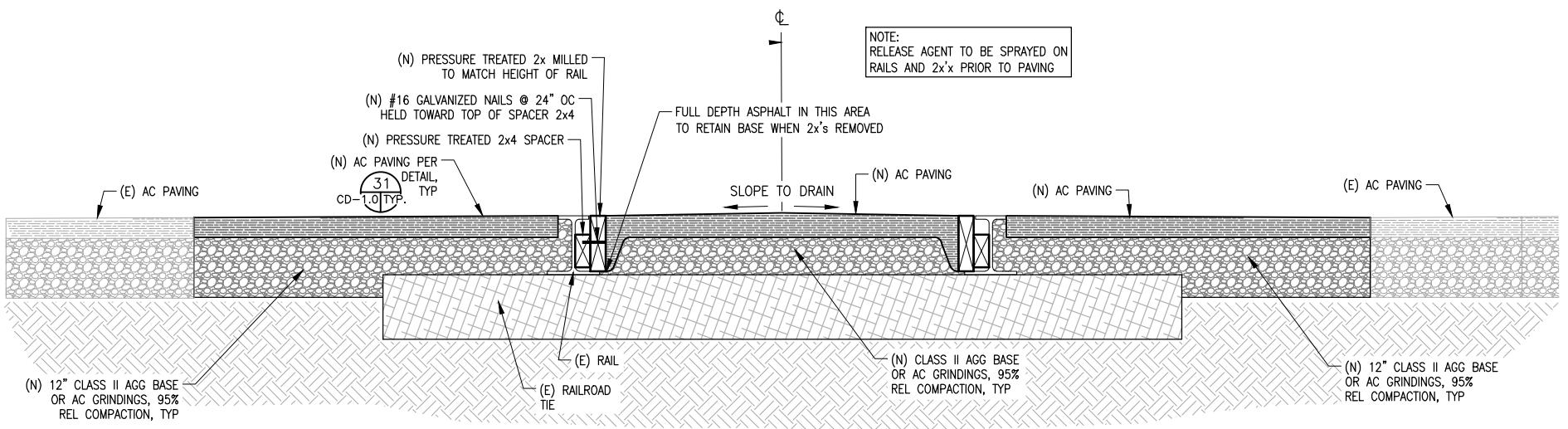
CD-1.0

SHEET 40 OF 52

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718 Third Street, Eureka, California 95501 USA
W www.ghd.com

PROFESSIONAL ENGINEER
S. COOPER
CALIFORNIA
3/12/2016

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41 DRIVEWAY AT RAILROAD CROSSING DETAIL
TYP. CD-2.0 SCALE: NTS

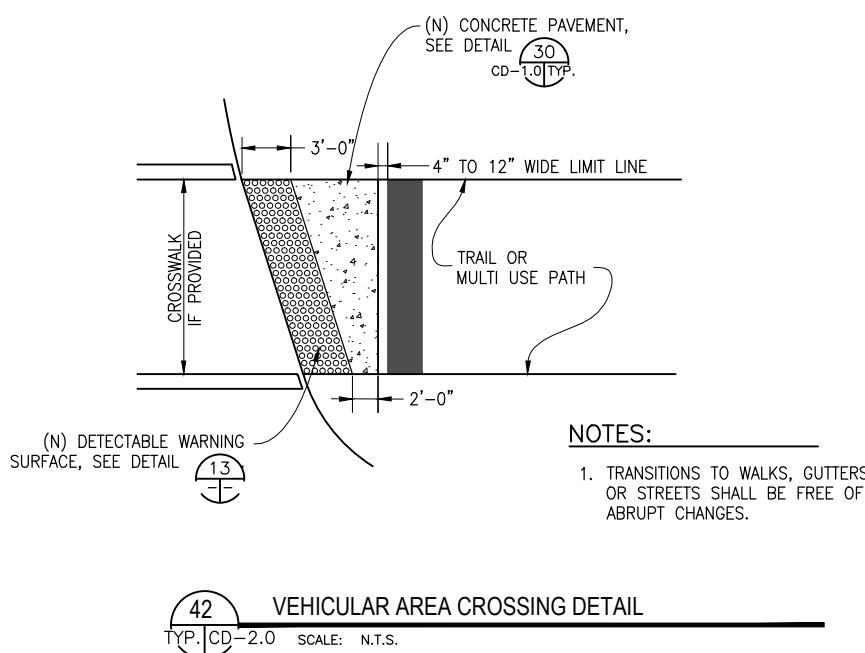
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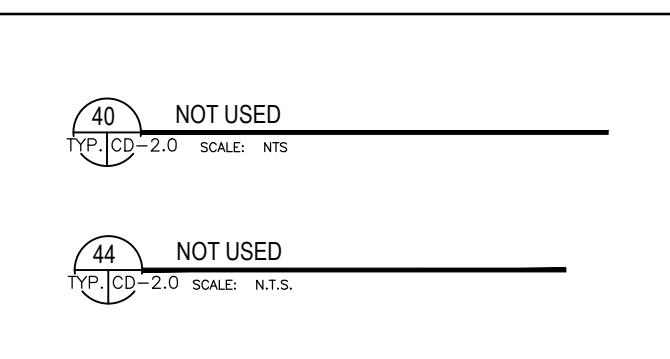
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DESCRIPTION	ISSUE

CITY OF ARCATA	HUMBOLDT BAY TRAIL NORTH	CIVIL DETAILS	STANDARD DETAILS
PROJ NO: 8411982	DRWN: OFG	CHKD: JW	CD-2.0

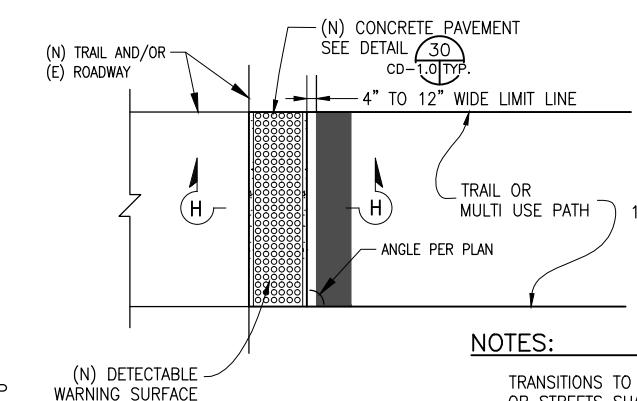


42 VEHICULAR AREA CROSSING DETAIL
TYP. CD-2.0 SCALE: NTS

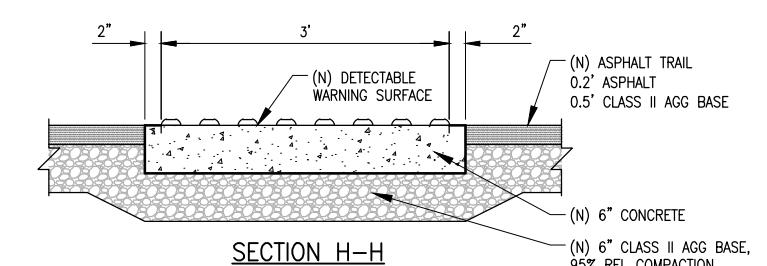


NOTES:

1. CURB RAMPS SHALL BE OUTLINED, AS SHOWN ON THE RAMP DETAILS, WITH A 1'-0" WIDE BORDER WITH 1/4" GROOVES APPROXIMATELY 3/4" ON CENTER.
2. CURB RAMPS SHALL HAVE A DETECTABLE WARNING SURFACE THAT EXTENDS THE FULL WIDTH AND 3'-0" DEPTH OF RAMP. SEE RAMP DETAILS ON THIS SHEET FOR DETECTABLE WARNING TRUNCATED DOME LAYOUT.
3. THE EDGE OF THE DETECTABLE WARNING SHALL BE BETWEEN 6" AND 8" AWAY FROM THE STREET GUTTER FLOWLINE.
4. DETECTABLE WARNING SHALL BE A PREFORMED MATERIAL AS APPROVED BY THE ENGINEER. THE DETECTABLE WARNING SHALL BE CAST-IN-PLACE (WET SET).

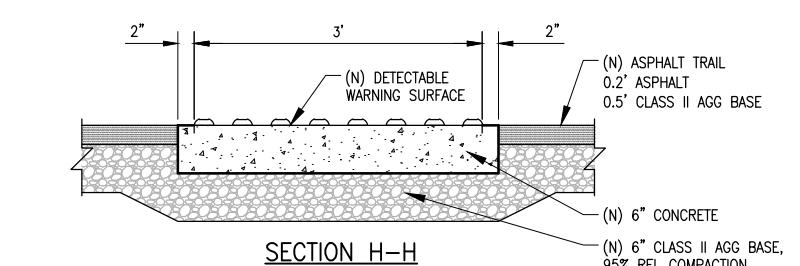


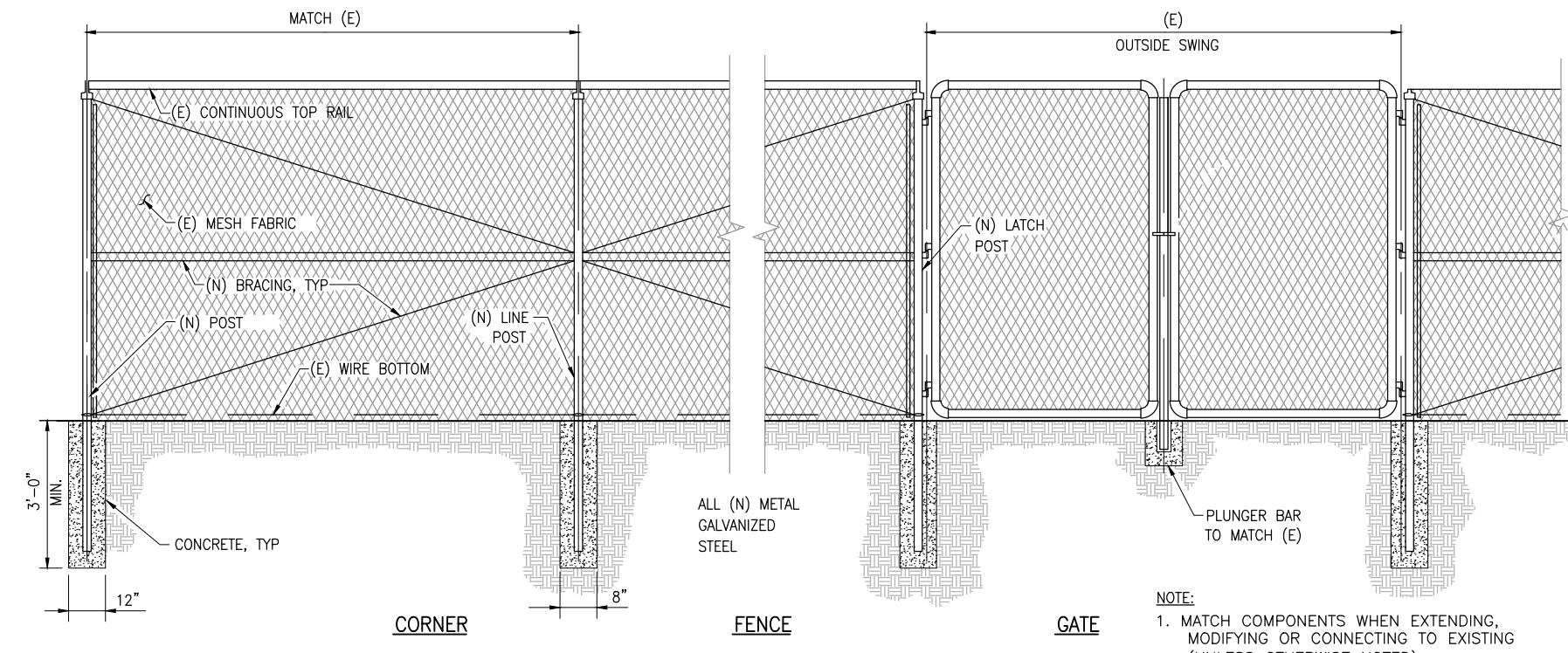
RAISED TRUNCATED DOME PATTERN (IN-LINE)
DETECTABLE WARNING SURFACE



RAISED TRUNCATED DOME

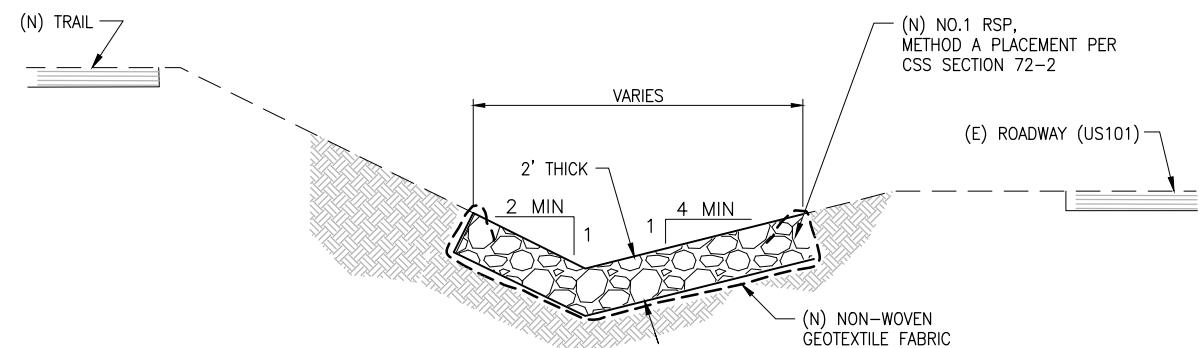
43 DETECTABLE WARNING SURFACE - TYPICAL DETAILS
TYP. CD-2.0 SCALE: NTS





51 TYPICAL RELOCATE CHAIN LINK FENCE AND GATE DETAIL
TYP. ICD-3.0 SCALE: N.T.S.

NOTE:
1. MATCH COMPONENTS WHEN EXTENDING,
MODIFYING OR CONNECTING TO EXISTING
(UNLESS OTHERWISE NOTED).



53 TYPICAL ROCK LINED DITCH DETAIL
TYP. ICD-3.0 SCALE: NTS

52 NOT USED
TYP. ICD-3.0 SCALE: NTS

54 NOT USED
TYP. ICD-3.0 SCALE: NTS

CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH
CIVIL DETAILS
STANDARD DETAILS

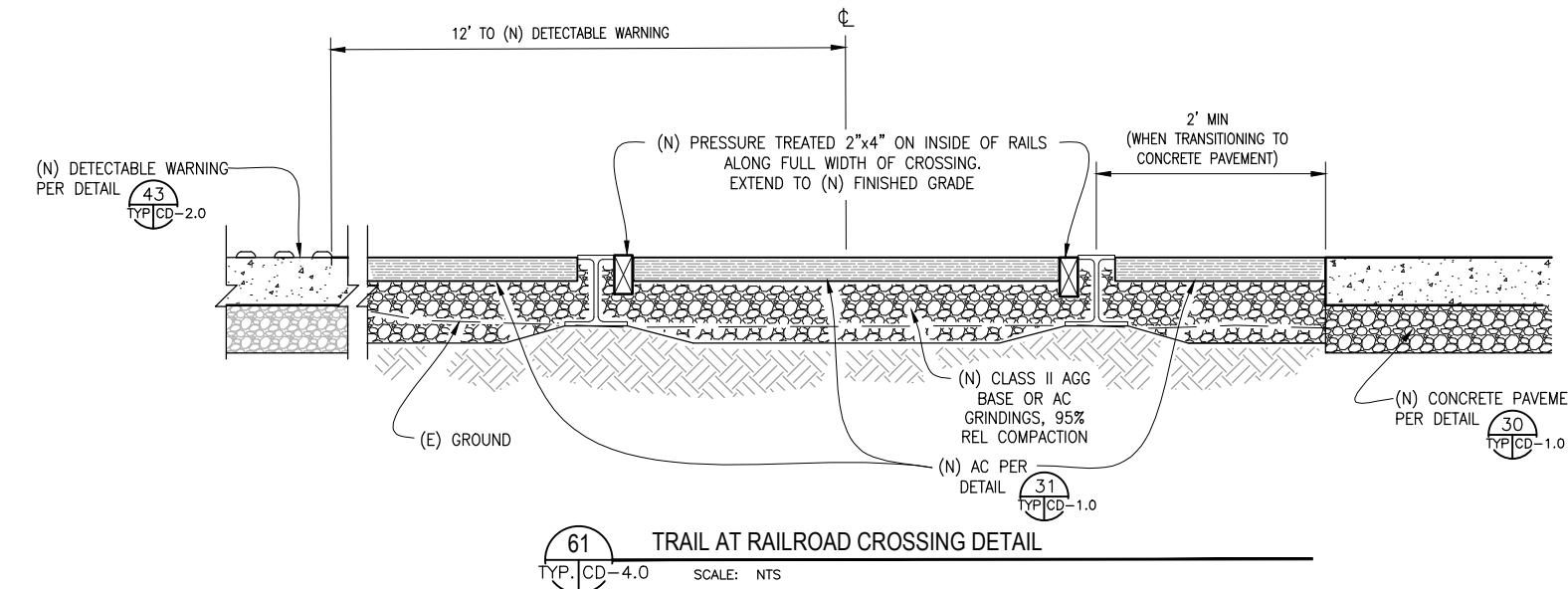
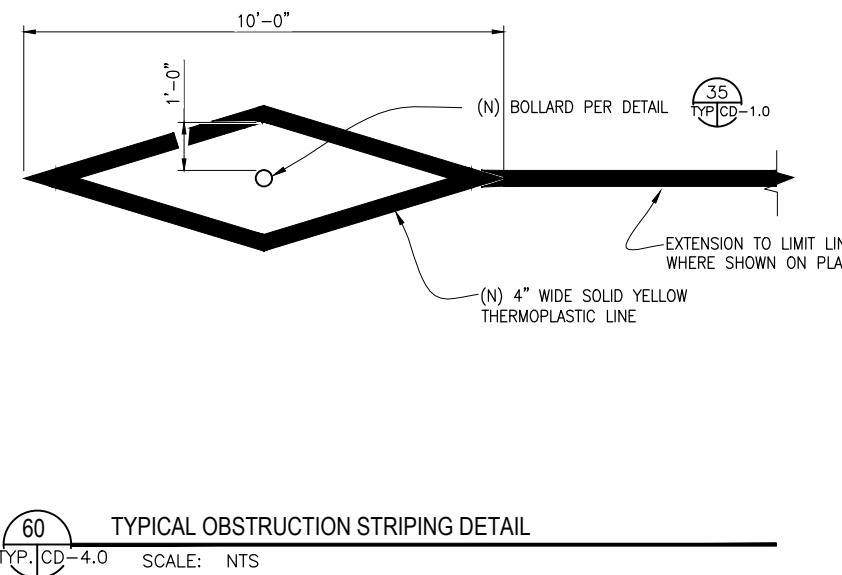
PROJ NO: 8411982
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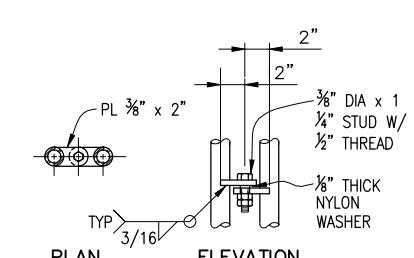
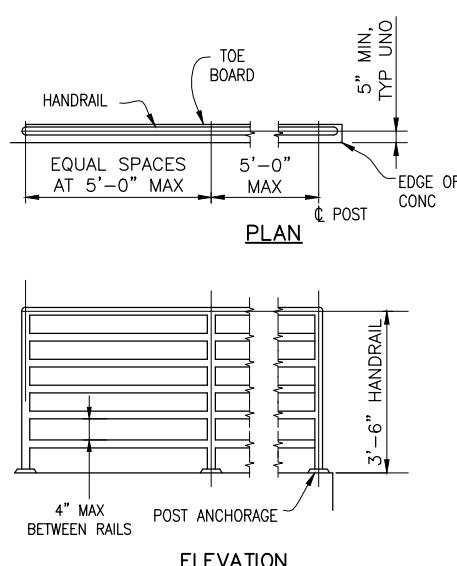


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ORIGINAL DRAWING
0 1"

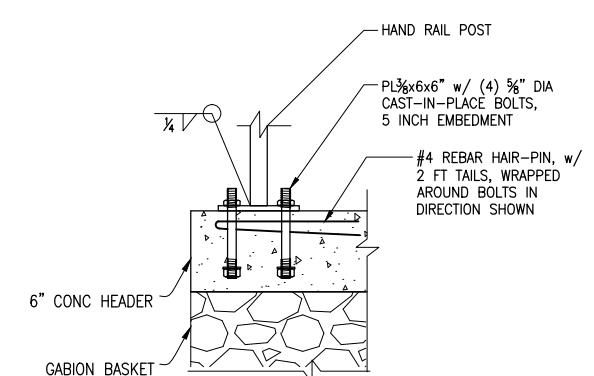
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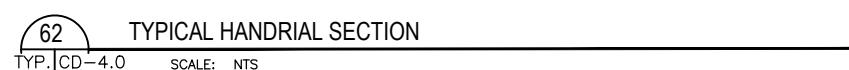
CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH
CIVIL DETAILS
STANDARD DETAILS - 4



THREE-RAIL HANDRAIL
(ALUMINUM)



HANDRAIL POST ANCHORAGE
(ALUMINUM)



1. DESIGN BASIS

- 1.1. THIS STRUCTURE HAS BEEN DESIGNED IN ACCORDANCE WITH THE CALTRANS BRIDGE DESIGN SPECIFICATIONS AND GEOTECHNICAL REPORT "ARCATA RAIL WITH TRAIL CONNECTIVITY PROJECT, ARCATA, CALIFORNIA" BY BLACKBURN CONSULTING. REFER TO THIS REPORT FOR ALL GEOTECHNICAL INFORMATION AND RECOMMENDATIONS.
- 1.2. STRUCTURE DESIGNED FOR SEISMIC LOADS BASED ON THE FOLLOWING PARAMETERS: ARS CURVE PER GEOTECHNICAL REPORT
- 1.3. DESIGN LIVE LOADS: 90 PSF PEDESTRIAN LIVE LOAD

2. GENERAL NOTES

- 2.1. ALL WORK TO CONFORM TO REQUIREMENTS OF ALL PUBLICATIONS AND NOTES LISTED UNDER "DESIGN BASIS".
- 2.2. CIVIL DRAWINGS AND ALL OTHER DRAWINGS AS REQUIRED SHALL BE USED IN CONJUNCTION WITH STRUCTURAL DRAWINGS TO DEVELOP DETAILS AND DIMENSIONS FOR SHOP DRAWINGS, FABRICATION, ERECTION AND CONSTRUCTION.
- 2.3. THE CONTRACTOR IS TO VERIFY ALL EXISTING CONDITIONS AND ALL DIMENSIONS IN FIELD PRIOR TO START OF CONSTRUCTION AND PROTECT AND MAINTAIN ALL EXISTING CONSTRUCTION AND ITS CONTENTS IN FULL.
- 2.4. THE CONTRACTOR SHALL MAINTAIN A SET OF LATEST REVIEWED SHOP DRAWINGS ON JOB SITE.
- 2.5. THE STRUCTURE HAS BEEN DESIGNED TO BE STABLE AND SELF SUPPORTING AFTER THE CONSTRUCTION IS COMPLETE. IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY FOR THE STRUCTURE'S STABILITY DURING CONSTRUCTION. THIS RESPONSIBILITY ALSO INCLUDES BUT IS NOT LIMITED TO METHOD AND SEQUENCE OF ERECTION, TEMPORARY SHORING AND TEMPORARY BRACING.
- 2.6. IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO FOLLOW ALL APPLICABLE SAFETY CODES AND REGULATIONS DURING ALL PHASES OF CONSTRUCTION.
- 2.7. SHOULD ANY INFORMATION ON THE STRUCTURAL DRAWINGS CONFLICT WITH THE SPECIFICATIONS OR ANY OTHER PART OF THE DRAWINGS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND AN INTERPRETATION WILL BE GIVEN.
- 2.8. ALL SECTIONS, DETAILS, NOTES, DIMENSIONS AND CONDITIONS ARE APPLICABLE AT ANY OTHER LOCATION WHERE CONDITIONS AND DETAILS ARE SIMILAR BUT ARE NOT SPECIFICALLY NOTED AS SUCH OR ARE NOT SHOWN.
- 2.9. FOR QUALITY CONTROL INSPECTION AND TESTING REQUIREMENTS DURING CONSTRUCTION, SEE SPECIFICATIONS AND "SCHEDULE OF SPECIAL INSPECTIONS".

3. FOUNDATION NOTES

- 3.1. SOIL BEARING CAPACITY USED IN THE DESIGN OF FOUNDATIONS:
2000 PSF AS RECOMMENDED BY THE GEOTECHNICAL REPORT BY BCI INC. JUNE 2010.
- 3.2. ALL FOOTINGS TO BEAR ON NATURAL UNDISTURBED SOIL OR COMPAKTED STRUCTURAL FILL HAVING MINIMUM BEARING CAPACITY AS INDICATED.
- 3.3. ALL COMPAKTED STRUCTURAL FILL SHALL CONFORM TO ALL RECOMMENDATIONS CONTAINED IN THE PROJECT GEOTECHNICAL REPORT.
- 3.4. PLACEMENT OF ALL COMPAKTED FILL AND COMPAKTION OF SUBGRADE SHALL BE UNDER FULL TIME DIRECTION OF A QUALIFIED GEOTECHNICAL ENGINEER. CONCRETE SLABS AND FOOTINGS SHALL NOT BE PLACED UNTIL FILL AND SUBGRADE HAVE BEEN CHECKED IN PLACE AND APPROVED BY THE GEOTECHNICAL ENGINEER.
- 3.5. IF VERY SOFT "BAY MUDS" OR ORGANICS ARE PRESENT BELOW BRIDGE FOUNDATIONS OR WHERE COMPAKTION IS UNACHIEVABLE DUE TO PUMPING SOILS, A LAYER OF GEOTEXTILE (TENSAR BX1200 OR EQUIVALENT) SHALL BE PLACED BEFORE PLACING FILL.

4. CONCRETE NOTES

- 4.1. ALL WORK TO CONFORM TO THE REQUIREMENTS OF THE FOLLOWING PUBLICATIONS:
 - 4.1.1. ACI "BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE" (ACI 318-LATEST EDITION) AND "DETAILS AND DETAILING OF CONCRETE REINFORCEMENT" (ACI 315-LATEST EDITION).
 - 4.1.2. "SPECIFICATION FOR WELDED STEEL WIRE FABRIC FOR CONCRETE REINFORCEMENT" (LATEST EDITION) BY THE WIRE REINFORCEMENT INSTITUTE, INC.
- 4.2. MINIMUM COMPRESSIVE STRENGTH OF CONCRETE AT 28 DAYS:
5000 PSI (NORMAL WEIGHT) AT PILES
4000 PSI (NORMAL WEIGHT) AT ALL OTHER ELEMENTS

PRIOR TO PLACING CONCRETE, MIX DESIGNS SHALL BE SUBMITTED FOR REVIEW.

4.3. ALL BAR REINFORCING FOR CONCRETE SHALL CONFORM TO ASTM A 615 GRADE 60 (DEFORMED). REINFORCING SHOWN ON DRAWING TO BE WELDED SHALL CONFORM TO ASTM A 706 GRADE 60 (DEFORMED) BARS SPECIALLY FORMULATED TO BE WELDABLE.

4.4. WELDED WIRE FABRIC SHALL CONFORM TO ASTM A 1064.

4.5. UNLESS OTHERWISE SHOWN, LOCATE REINFORCING BARS WITH FOLLOWING CLEAR DIMENSION TO FACE OF CONCRETE:

CONCRETE CAST ON GROUND: 3" CLEAR

CONCRETE TOPPING SLAB AT PRE-CAST BRIDGES: 1½" CLEAR

ALL OTHER LOCATIONS: 2" CLEAR, UNO

4.6. CONCRETE ACCESSORIES MUST BE ADEQUATE TO MAINTAIN REINFORCING ACCURATELY IN PLACE AND BE NON-CORROSIVE, NON-STAINING TYPE.

4.7. LAP ALL BAR REINFORCING IN CONCRETE ELEMENTS AS SHOWN IN "REINFORCING CONCRETE LAP SCHEDULE". IN BEAMS, GRADE BEAMS AND GIRDERS, SPLICE TOP BARS AT MIDDLE OF SPAN BETWEEN SUPPORTS AND SPLICE BOTTOM BARS AT SUPPORTS (UNLESS OTHERWISE SHOWN).

4.8. SAW-CUT CONTROL JOINTS IMMEDIATELY AFTER CONCRETE HAS SET SUFFICIENTLY SO THAT CUTTING DOES NOT PRODUCE SHREDDING OF THE CONCRETE, BUT BEFORE CONCRETE HAS HAD A CHANCE TO CRACK DUE TO INITIAL SHRINKAGE. THE CUTTING PERIOD WILL VARY ACCORDING TO THE RATE OF SETTING OF THE CONCRETE. IT IS THE CONTRACTOR'S SOLE RESPONSIBILITY TO CUT THESE JOINTS AT THE PROPER TIME AND USING THE PROPER PROCEDURE TO MINIMIZE SHRINKAGE CRACKING AND TO PRODUCE CLEAN, STRAIGHT JOINTS.

4.9. REFERENCE SPECIFICATIONS FOR CONCRETE CURING AND PROTECTION. BEGIN CONCRETE CURING AS SOON AS FINISHING OPERATIONS ARE COMPLETE (WITHIN TWO HOURS).

4.10. SHORE CONCRETE BEAMS, CANTILEVERS AND SUPPORTED SLABS DIRECTLY TO FIRM BEARING FOR AS LONG AS PRACTICAL (MINIMUM 28 DAYS) TO MINIMIZE CREEP AND DEFLECTION.

4.11. LIQUID MEMBRANE FORMING CURING COMPOUNDS SHALL COMPLY WITH ASTM C 309, TYPE I, CLASS A.

5. STEEL NOTES

5.1. ALL WORK TO CONFORM TO THE REQUIREMENTS OF THE FOLLOWING PUBLICATIONS:

5.1.1. AISC 360-05 "DESIGN SPECIFICATION FOR STRUCTURAL STEEL BUILDINGS" (MANUAL OF STEEL CONSTRUCTION - 13TH EDITION)

5.2. ALL STEEL TUBES, ROUND AND SHAPED, SHALL CONFORM TO ASTM A500, GRADE B.

5.3. ALL STEEL PILINGS SHALL CONFORM TO ASTM A252.

5.4. ALL PLATES AND MISCELLANEOUS IRON SHALL CONFORM TO ASTM A36 (EXCEPT AS NOTED).

5.5. ALL BOLTED CONNECTIONS TO USE 3/4" DIAMETER A307 BOLTS UNLESS NOTED OTHERWISE.

5.6. ALL WELDS TO STRUCTURAL STEEL TO CONFORM TO "STRUCTURAL WELDING CODE - STEEL" (AWS D1.1 - LATEST EDITION). ALL WELDS TO COLD-FORMED SHEET STEEL TO CONFORM TO "STRUCTURAL WELDING CODE - SHEET STEEL" (AWS D1.3- LATEST EDITION).

5.7. ALL STEEL EXCEPT PILINGS SHALL BE GALVANIZED AND THEN PAINTED. STEEL PILINGS FILLED WITH CONCRETE SHALL BE PAINTED ABOVE THE MUD LINE.

5.8. GALVANIZING SHALL CONFORM TO ASTM A 123 OR ASTM A 153, AS APPLICABLE.

5.5. FIELD WELDING TO GALVANIZED STEEL: PRIOR TO FIELD WELDING CONNECTION, ZINC COATING AT ALL WELD CONNECTION AREAS SHALL BE REMOVED BY BURNING WITH OXYGEN FUEL GAS TORCH, SHOT BLASTING OR GRINDING TO BARE STEEL. APPLY A MINIMUM OF 2 COATS OF ZINC-RICH PAINT AFTER CLEANING COMPLETED WELD.

6. PRE-MANUFACTURED BRIDGE

6.1. PROVIDE AND INSTALL FULLY FUNCTIONAL PRE-MANUFACTURED BRIDGE PER THE STRUCTURAL DRAWINGS AND SPECIFICATIONS.

6.2. BRIDGES TO BE DESIGNED BY A CALIFORNIA PROFESSIONAL ENGINEER AND CONSTRUCTED IN ACCORDANCE WITH SPECIFICATIONS AND TO SUPPORT OWN WEIGHT, IMPOSED LIVE, WIND, AND SEISMIC LOADS. SEE "DESIGN BASIS" IN STRUCTURAL GENERAL NOTES FOR STRUCTURAL CRITERIA. SUBMIT DESIGN FOR REVIEW PRIOR TO FABRICATION. CONTRACTOR TO INCLUDE MULTIPLE STRUCTURAL REVIEWS IN PROJECT SCHEDULE. ALLOW 2 WEEKS FOR EACH REVIEW.

CONCRETE REINFORCEMENT CLASS B LAP SPLICE SCHEDULE (inches)									
F _c (psi)	#3	#4	#5	#6	#7	#8	#9	#10	
2800	23	31	39	47	68	78	88	99	
3000	21	28	36	43	62	71	80	90	
4000	18	25	31	37	54	62	70	78	
5000	17	22	28	33	48	55	62	70	
6000	15	20	25	30	44	50	57	64	

BASE FLOOD ELEVATION NOTE:

100 YR WATER SURFACE ELEVATION OF HUMBOLDT BAY ESTIMATED TO BE 10.2 FT NAVD 88 BASED ON PRELIMINARY RESULTS OF FEMA FLOOD MODEL PROVIDED TO GHD BY THE CITY OF ARCATA (1/21/2016). FOR PURPOSES OF DESIGN THE SOFFIT ELEVATION WAS ROUNDED UP TO 10.5 FT.

CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH
STRUCTURAL
GENERAL NOTES

PROJ NO: 8411982
DRWN: CHKD:

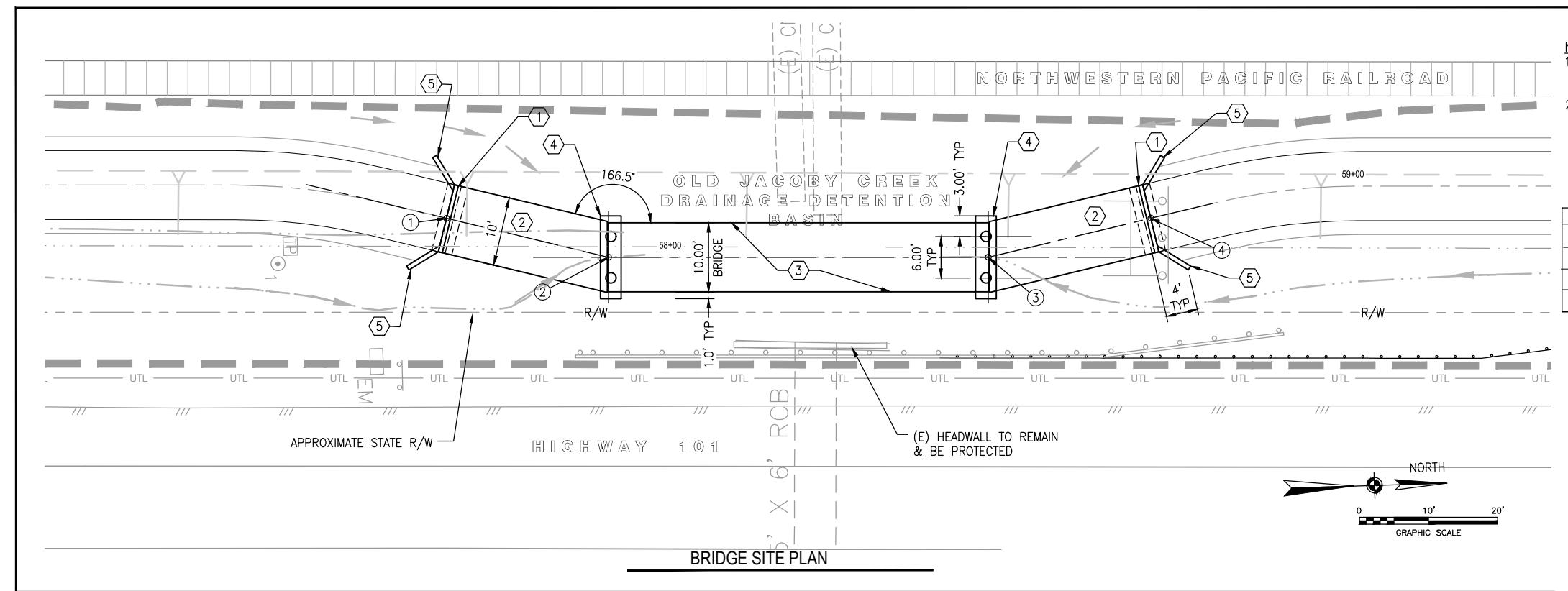
S-1.0

SHEET 44 OF 52

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MARK	DATE	DESCRIPTION	ISSUE



KEYNOTES:

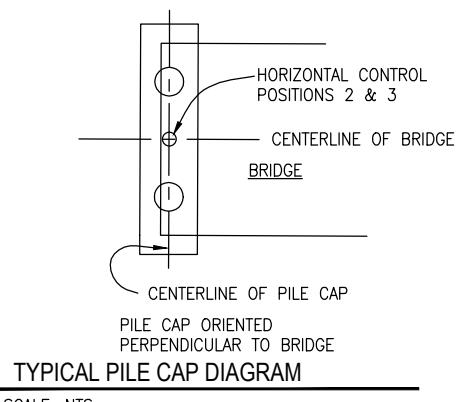
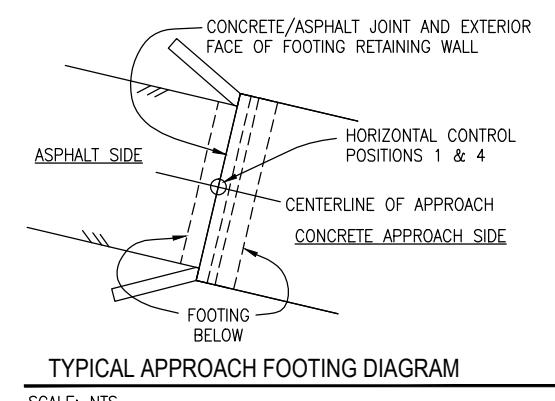
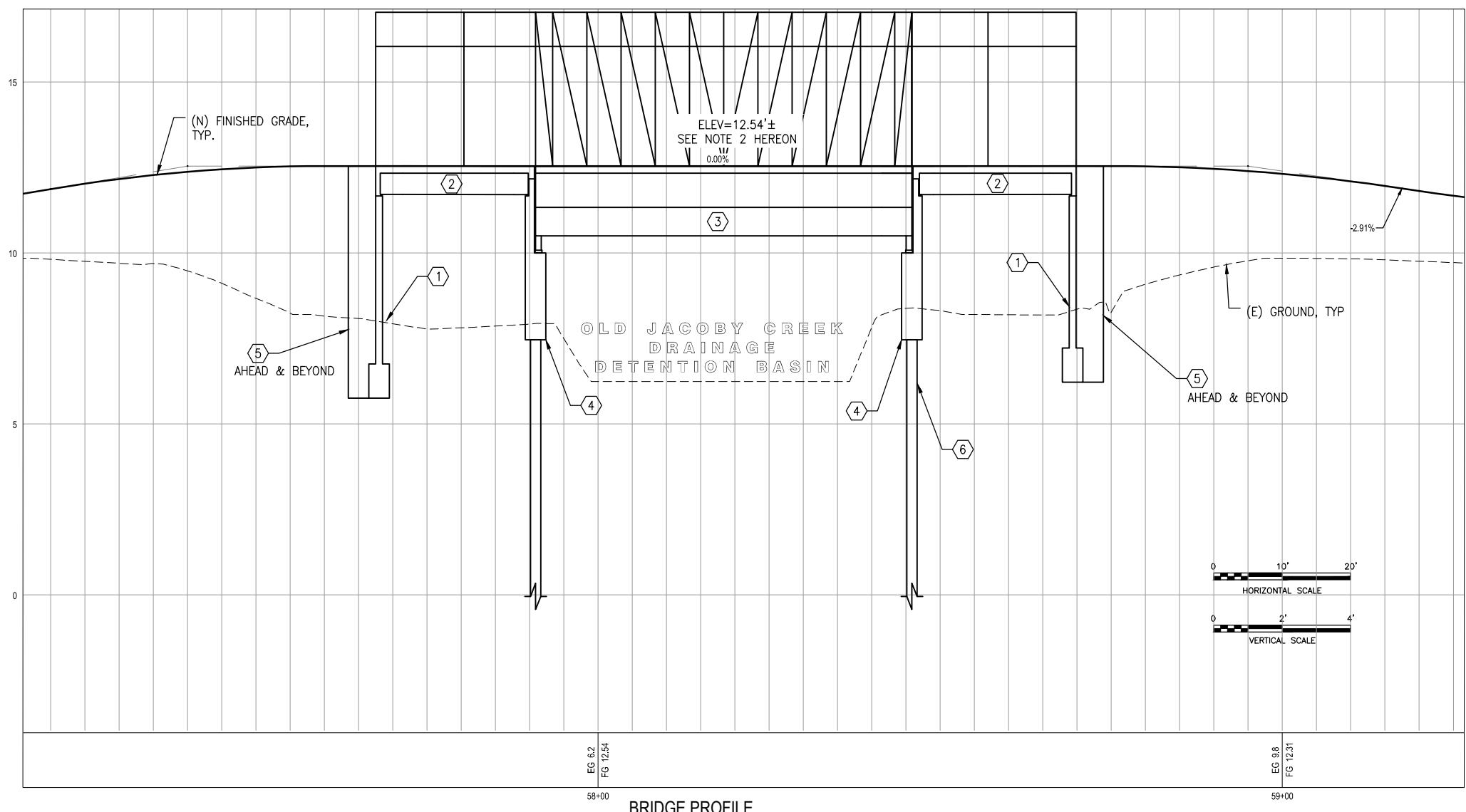
- ① (N) CAST-IN-PLACE CONC RETAINING FTG PER DETAIL ¹ TYP S-7.0
- ② (N) PRECAST CONC BRIDGE DECKS W/ PEDESTRIAN GUARDRAILS AND CONCRETE OVERLAY, TYP ² TYP S-8.0
- ③ (N) PRE-MANUFACTURED 55 FT LONG BY 10 FT WIDE ALUMINUM BRIDGE W/ PEDESTRIAN GUARDRAILS AND ALUMINUM DECKING ¹ TYP S-8.0
- ④ (N) CAST-IN-PLACE PILE CAP ON DRIVEN PILES PER ³ TYP S-7.0
- ⑤ (N) CAST-IN-PLACE WING WALL PER DETAIL ⁵ TYP S-8.0
- ⑥ (N) 18" STEEL SHELL PILE PER ⁶ TYP S-7.0

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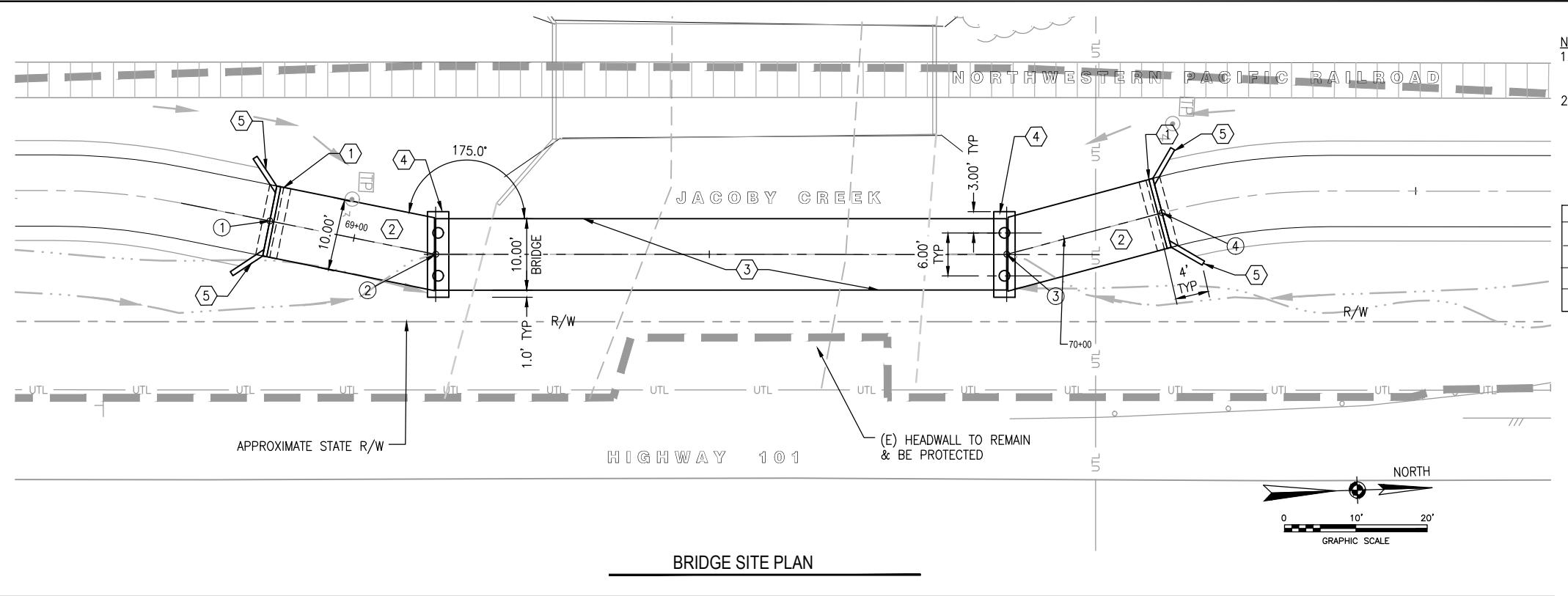
**CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH
STRUCTURAL
OLD JACOBY CREEK CROSSING
SITE PLAN & PROFILE**

PROJ NO: 8411982

DRWN: CHKD:

S-2.0

SHEET 45 OF 52



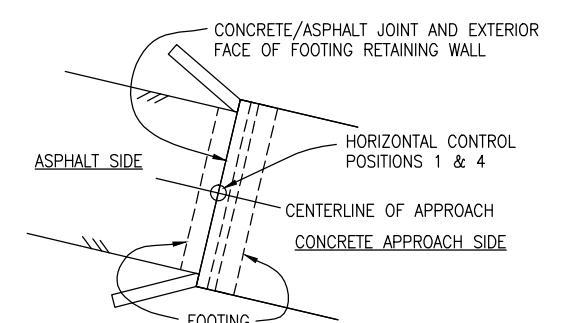
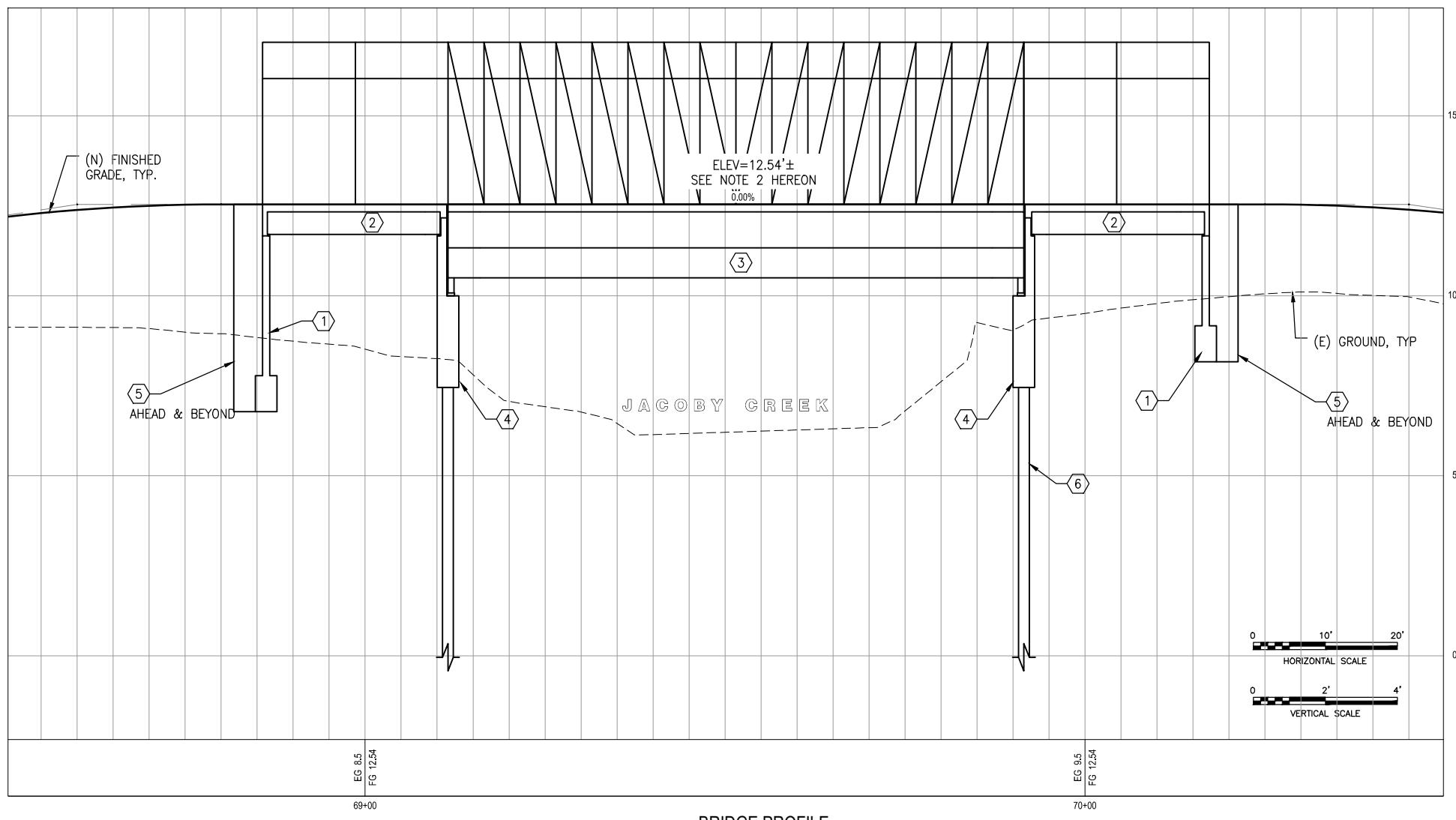
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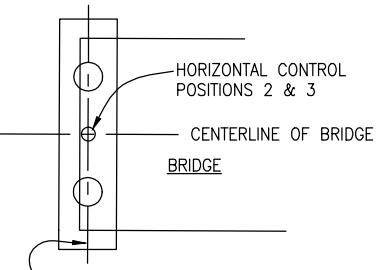
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TYPICAL APPROACH FOOTING DIAGRAM
SCALE: NTS



TYPICAL PILE CAP DIAGRAM
SCALE: NTS

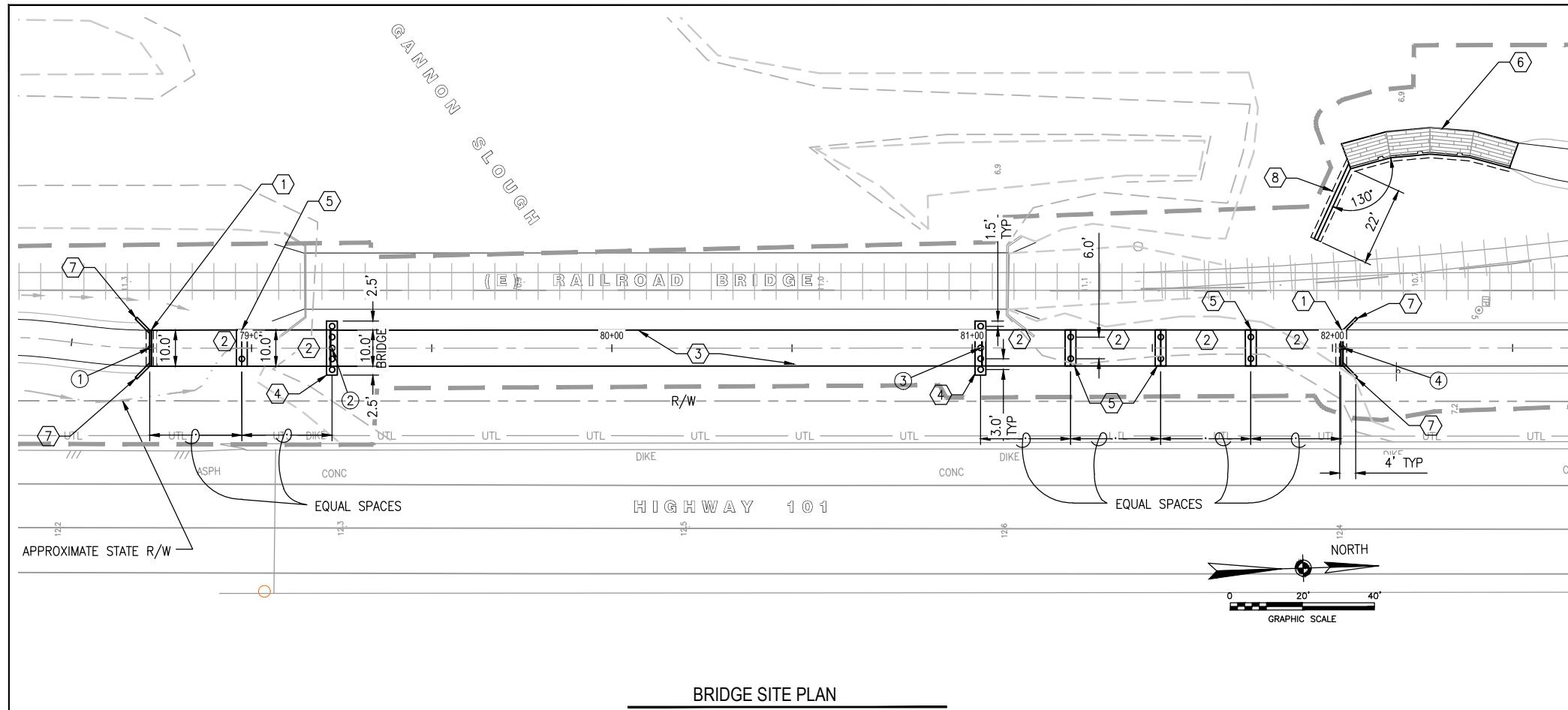
CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH
STRUCTURAL
JACOBY CREEK CROSSING
SITE PLAN & PROFILE

PROJ NO: 8411982

DRWN: CHKD:

S-3.0

SHEET 46 OF 52



NOTES:

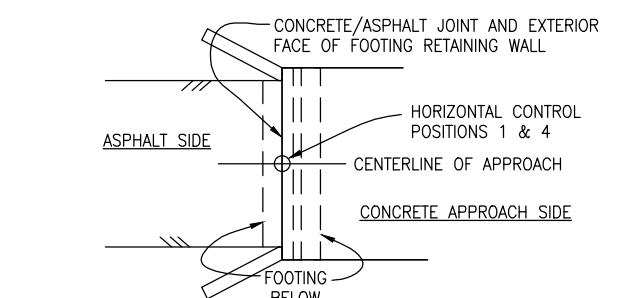
1. DIMENSIONS SHOWN ARE TYPICAL. APPROACH SPANS & FOOTINGS ARE SYMMETRICAL ABOUT CENTER LINE OF TRAIL.
2. THE BRIDGE SOFFIT SHALL BE SET AT OR ABOVE ELEVATION 10.5 FT (NAVD 88).

LOCATIONS:

NUMBER	DESCRIPTION	NORTHING	EASTING
①	CL OF APPROACH (SEE FOOTING DIAGRAM)	2198340.08	5985673.94
②	CL OF BRIDGE (SEE PILE CAP DIAGRAM)	2198390.64	5985675.53
③	CL OF BRIDGE (SEE PILE CAP DIAGRAM)	2198570.55	5985681.14
④	CL OF APPROACH (SEE FOOTING DIAGRAM)	2198671.09	5985684.28

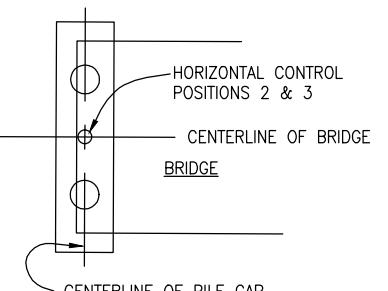
KEYNOTES:

- ① (N) CAST-IN-PLACE CONC RETAINING FTG PER DETAIL 1 TYP S-7.0
- ② (N) PRECAST CONC BRIDGE DECKS W/ PEDESTRIAN GUARDRAILS AND CONCRETE OVERLAY, TYP 2 TYP S-8.0
- ③ (N) PRE-MANUFACTURED 180 FT LONG BY 10 FT WIDE STEEL BRIDGE W/ PEDESTRIAN GUARDRAILS AND AC SURFACE 1 TYP S-8.0
- ④ (N) CAST-IN-PLACE PILE CAP ON DRIVEN PILES PER 3 TYP S-7.0
- ⑤ (N) CAST-IN-PLACE PILE CAP ON DRIVEN PILES PER 2 TYP S-7.0
- ⑥ (N) GANNON SLOUGH OVERLOOK PER S-4.0 S-9.0
- ⑦ (N) CAST-IN-PLACE WING WALL PER DETAIL 5 TYP S-8.0
- ⑧ (N) CAST-IN-PLACE FTG SIMILAR TO DETAIL EXCEPT DECK NOTCH 3 TYP S-9.0
- ⑨ (N) 18" STEEL SHELL PILE PER 6 TYP S-7.0



TYPICAL APPROACH FOOTING DIAGRAM

SCALE: NTS



TYPICAL PILE CAP DIAGRAM

SCALE: NTS

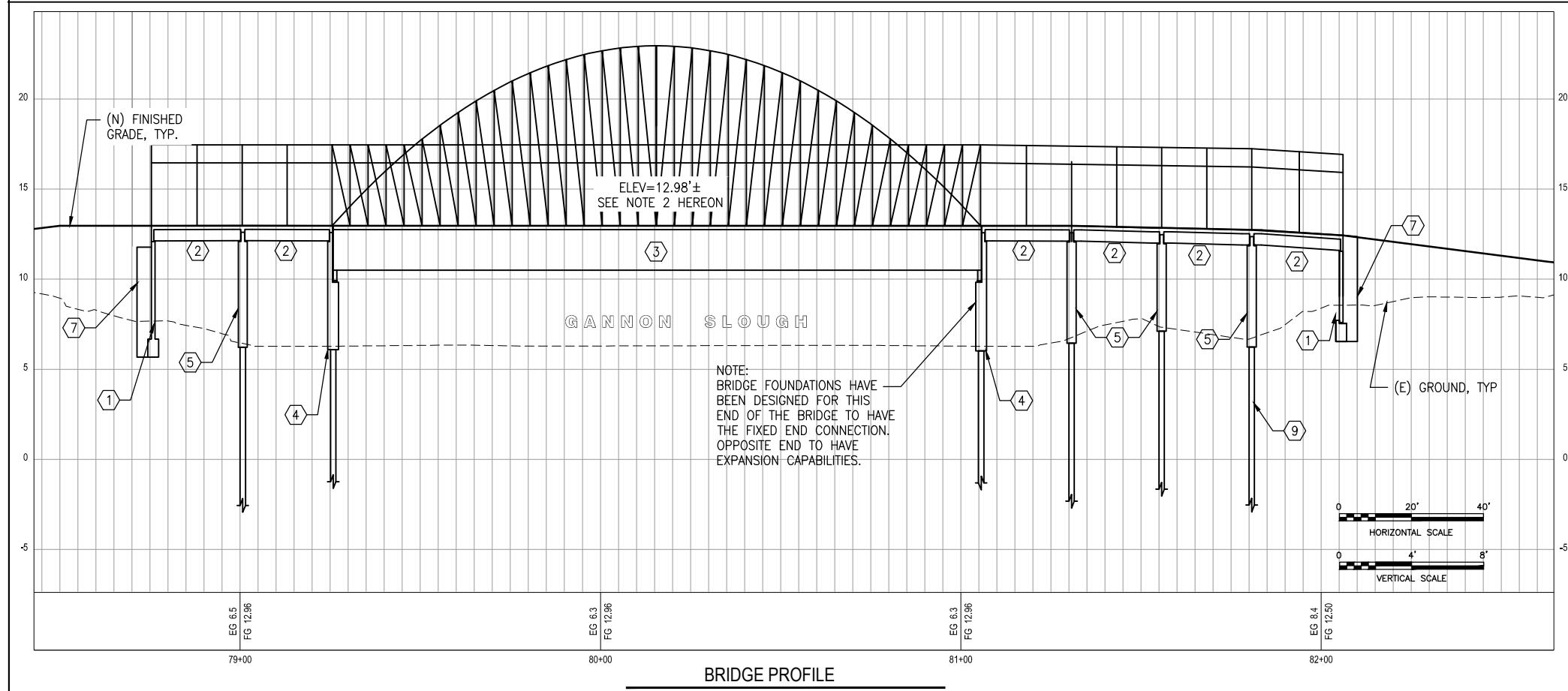
**CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH
STRUCTURAL
GANNON SLOUGH BRIDGE
SITE PLAN & PROFILE**

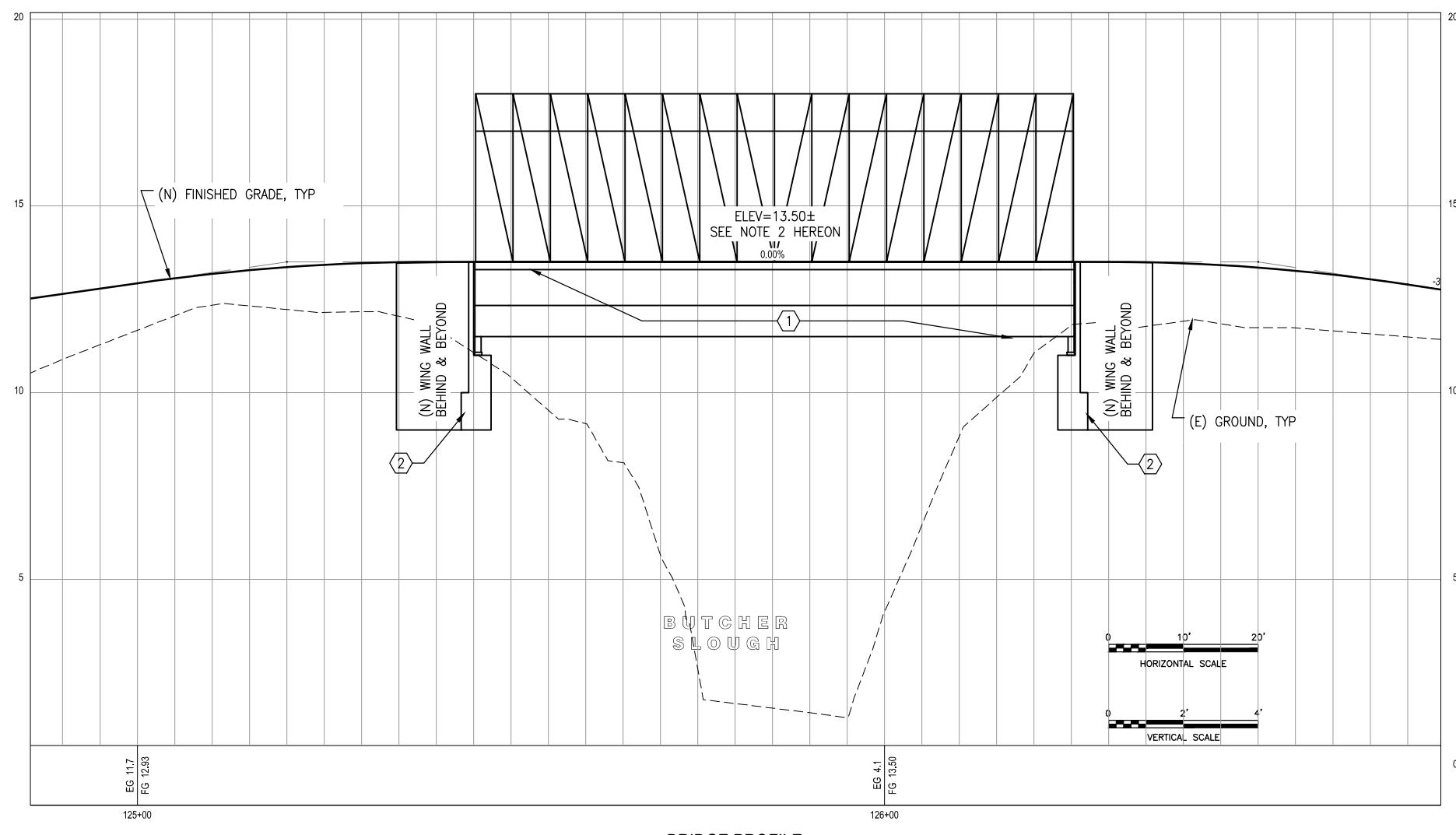
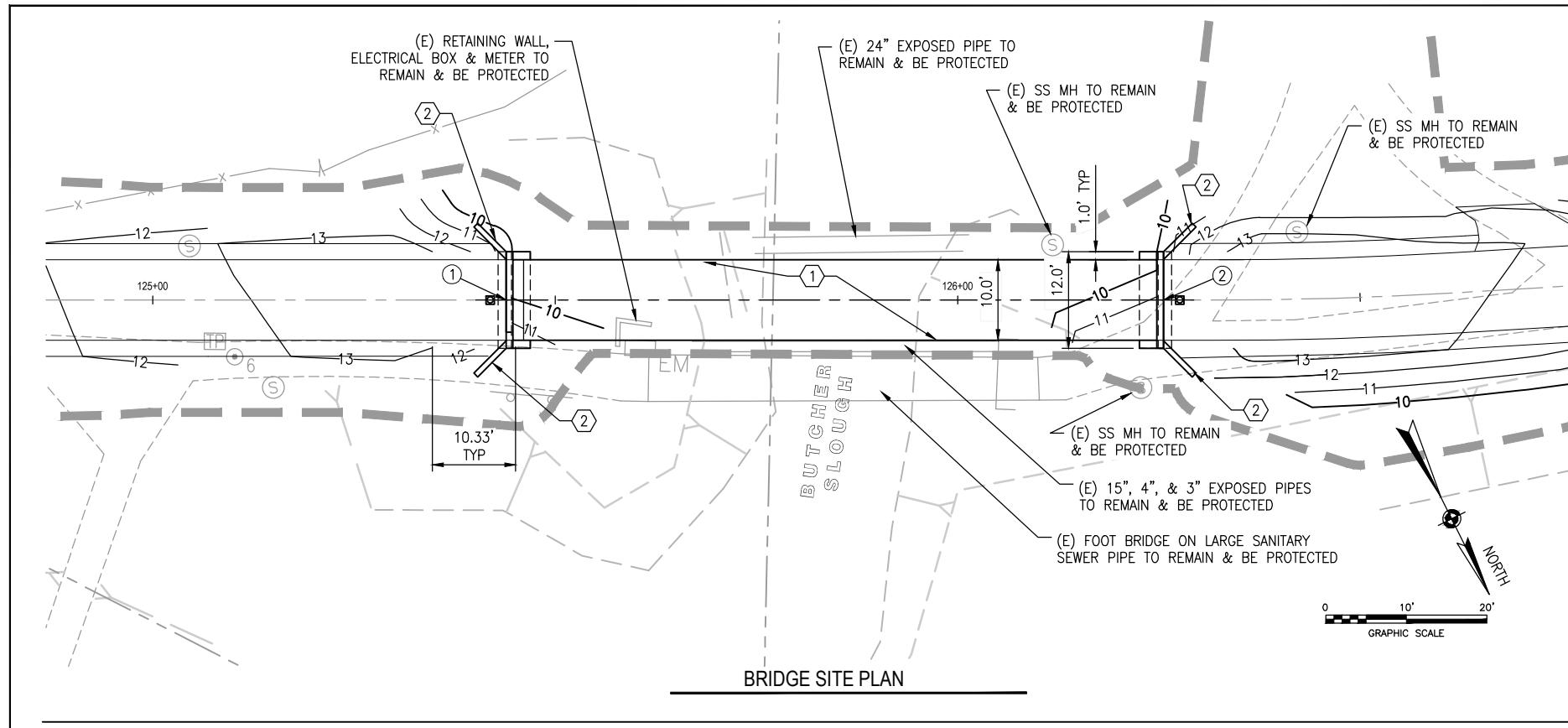
PROJ NO: 8411982

DRWN: CHKD:

S-4.0

SHEET 47 OF 52





NOTES:

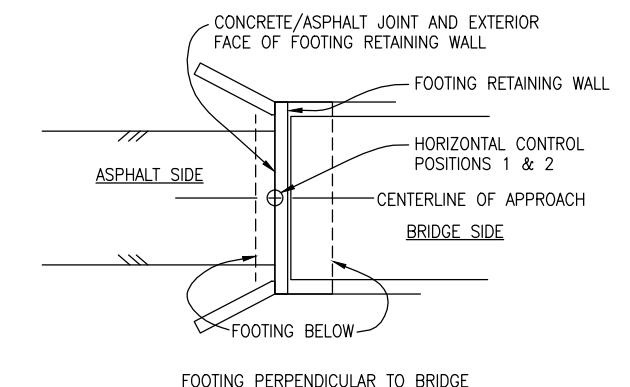
1. DIMENSIONS SHOWN ARE TYPICAL. APPROACH SPANS AND FOOTINGS ARE SYMMETRICAL ABOUT CENTER LINE OF TRAIL.
2. THE BRIDGE SOFFIT SHALL BE SET AT OR ABOVE ELEVATION 10.5 FT (NAVD 88).

KEYNOTES:

- (1) (N) PRE-MANUFACTURED 80 FT LONG BY 10 FT WIDE ALUMINUM BRIDGE W/ PEDESTRIAN GUARDRAILS AND ALUMINUM DECKING TYP S-8.0
- (2) (N) CAST-IN-PLACE BRIDGE SPREAD FOOTING PER 5 TYP S-8.0 WITH WING WALLS PER 7 TYP S-8.0

LOCATIONS:

NUMBER	DESCRIPTION	NORTHING	EASTING
(1)	CL OF BRIDGE (SEE FOOTING DIAGRAM)	2202118.36	5983482.64
(2)	CL OF BRIDGE (SEE FOOTING DIAGRAM)	2202154.79	5983409.55



TYPICAL FOOTING DIAGRAM

SCALE: NTS

CAUTION:

IMPROVEMENTS TAKE PLACE IN THE VICINITY OF A PG&E GAS TRANSMISSION LINE. CALL UNDERGROUND SERVICE ALERT (USA) AT LEAST 48 HOURS PRIOR TO ANY EXCAVATION TO OBTAIN LOCATION AND POTHOLE TO VERIFY DEPTH OF LINE (IF REQUIRED). A PG&E STANDBY EMPLOYEE IS REQUIRED DURING ANY EXCAVATION WITHIN 10 FT OF GAS TRANSMISSION LINE.

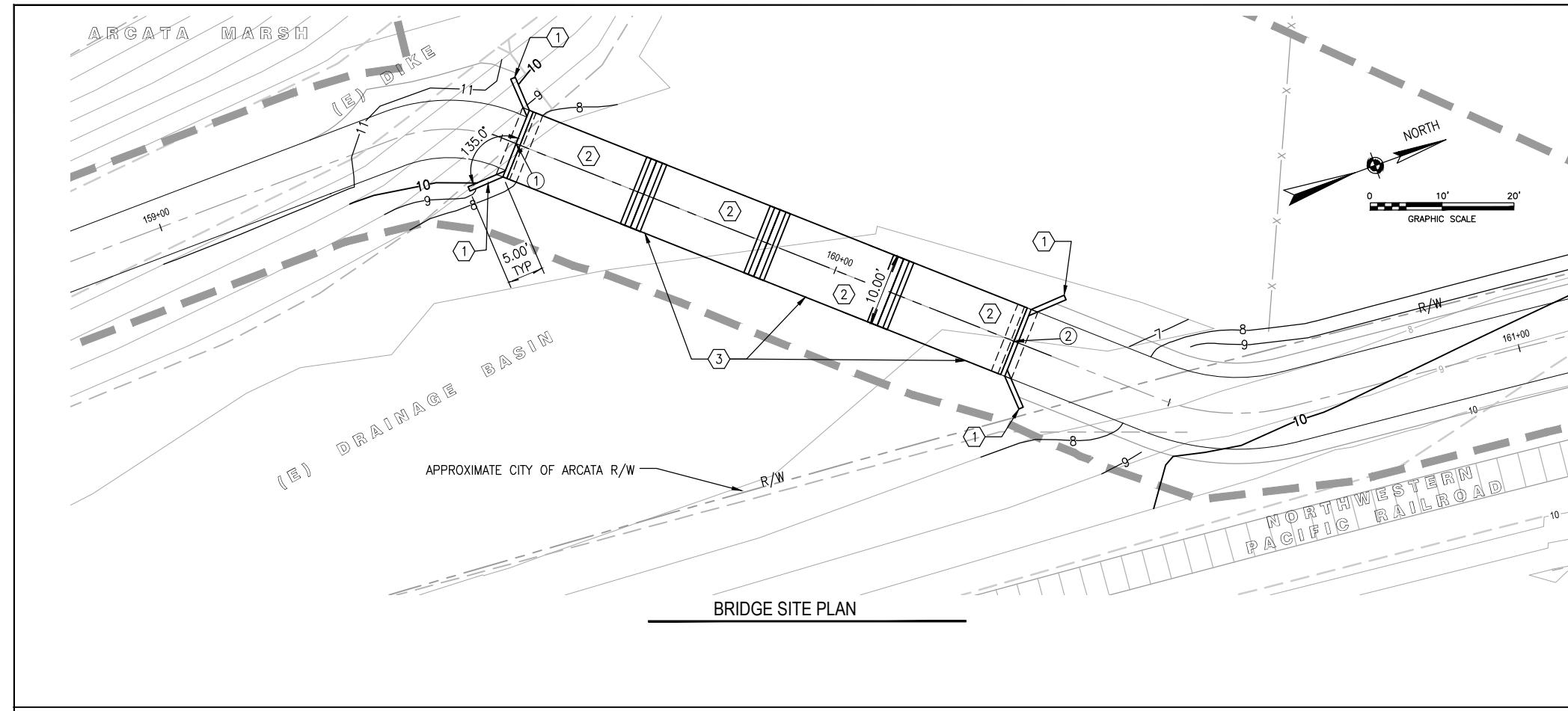
**CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH
STRUCTURAL
BUTCHER SLOUGH BRIDGE
SITE PLAN & PROFILE**

PROJ NO: 8411982

DRWN: CHKD:

S-5.0

SHEET 48 OF 52



NOTES:
1. DIMENSIONS SHOWN ARE TYPICAL. APPROACH SPANS AND FOOTINGS ARE SYMMETRICAL ABOUT CENTER LINE OF TRAIL.

LOCATIONS:

NUMBER	DESCRIPTION	NORTHING	EASTING
①	CL OF BRIDGE (SEE FOOTING DIAGRAM)	2204706.20	5982746.22
②	CL OF BRIDGE (SEE FOOTING DIAGRAM)	2204761.91	5982795.80



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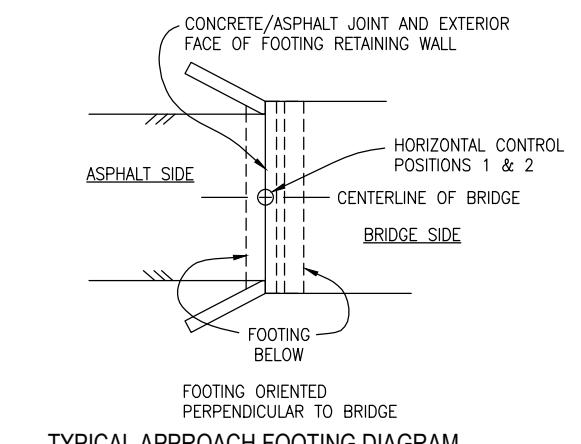
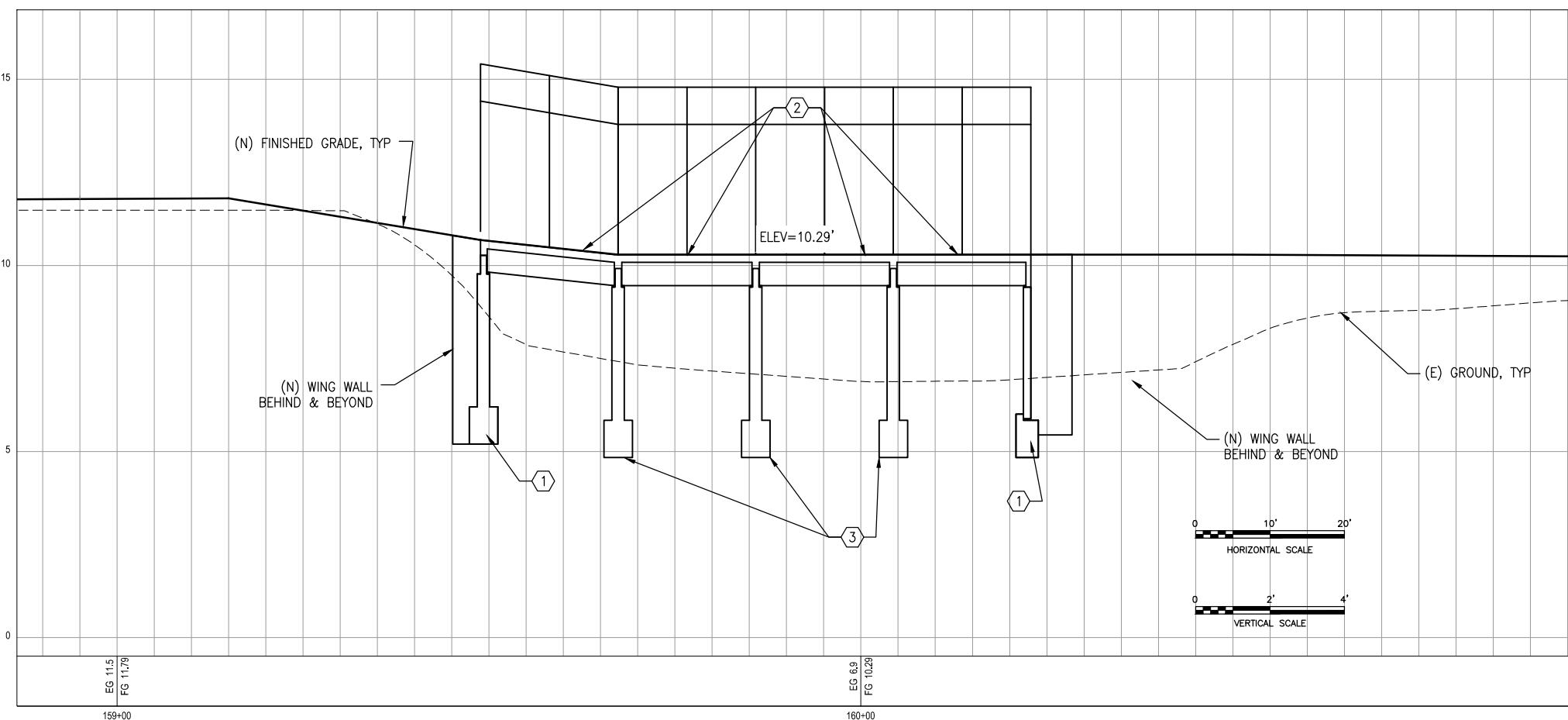
MARK	DATE	DESCRIPTION	ISSUE

CITY OF ARCATA
HUMBOLDT BAY TRAIL NORTH
STRUCTURAL
PERIMETER LEVEE ELEVATED
TRAIL - SITE PLAN & PROFILE

PROJ NO: 8411982
DRWN: CHKD:

S-6.0

SHEET 49 OF 52

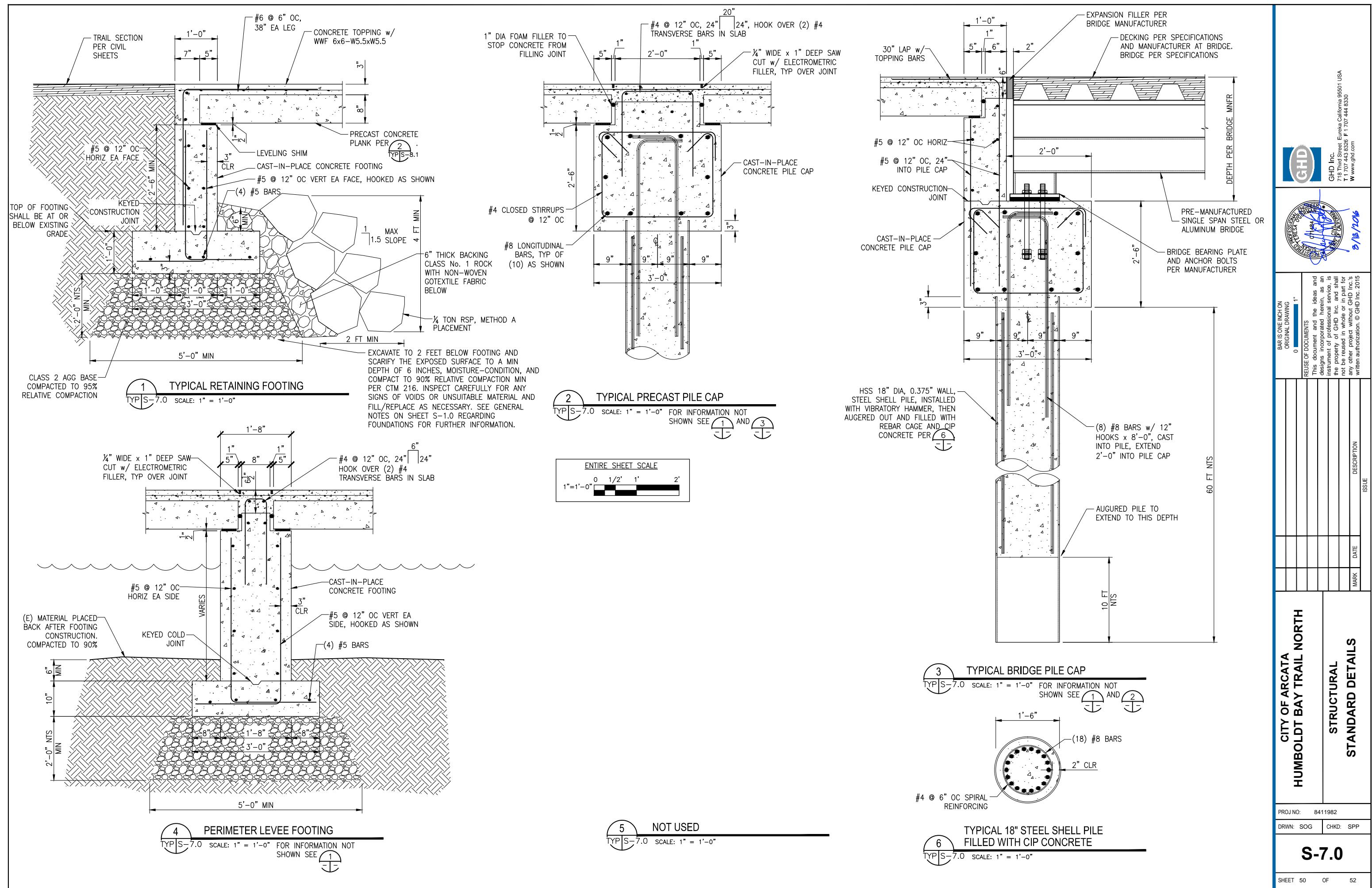


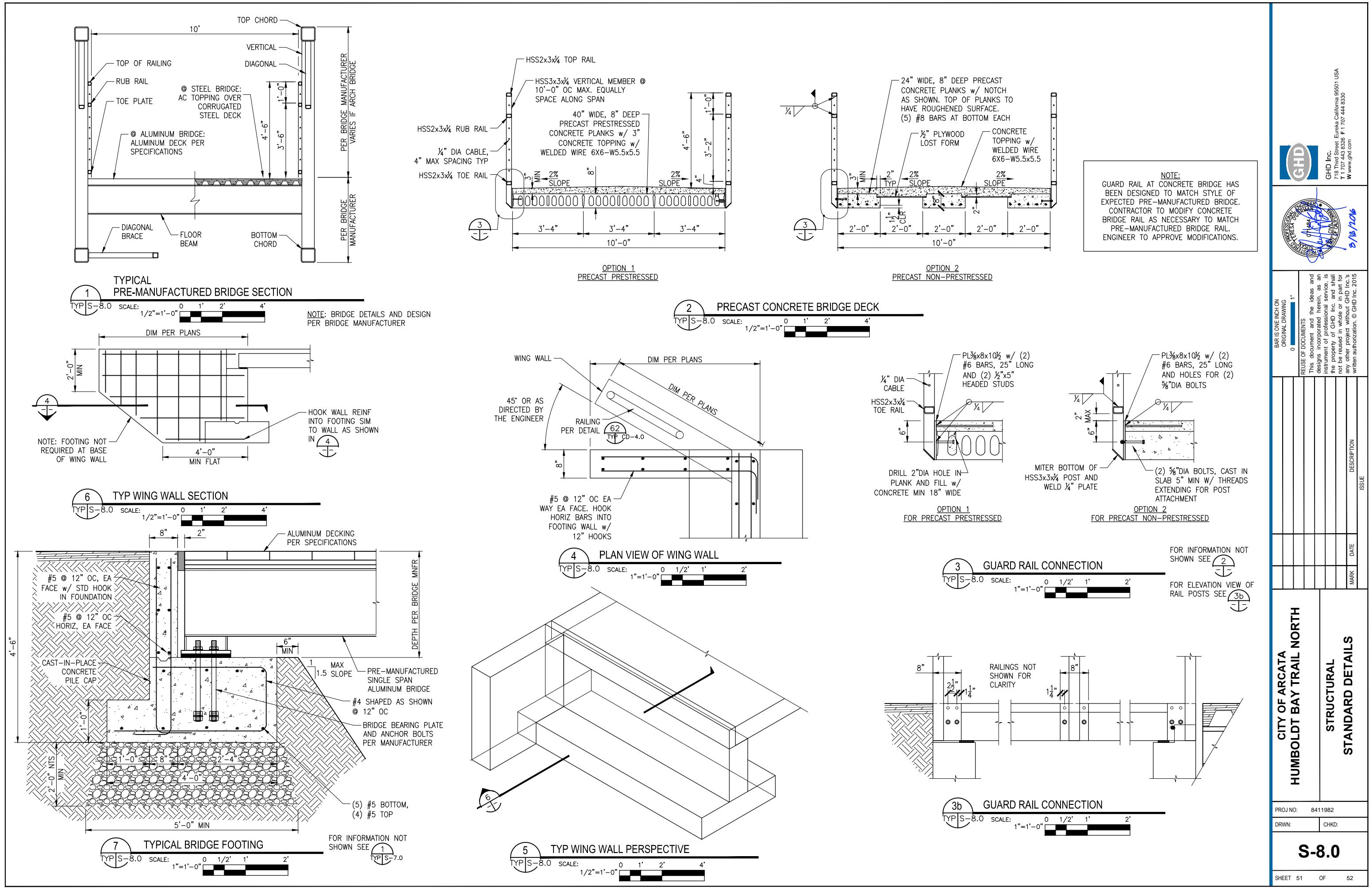
TYPICAL APPROACH FOOTING DIAGRAM

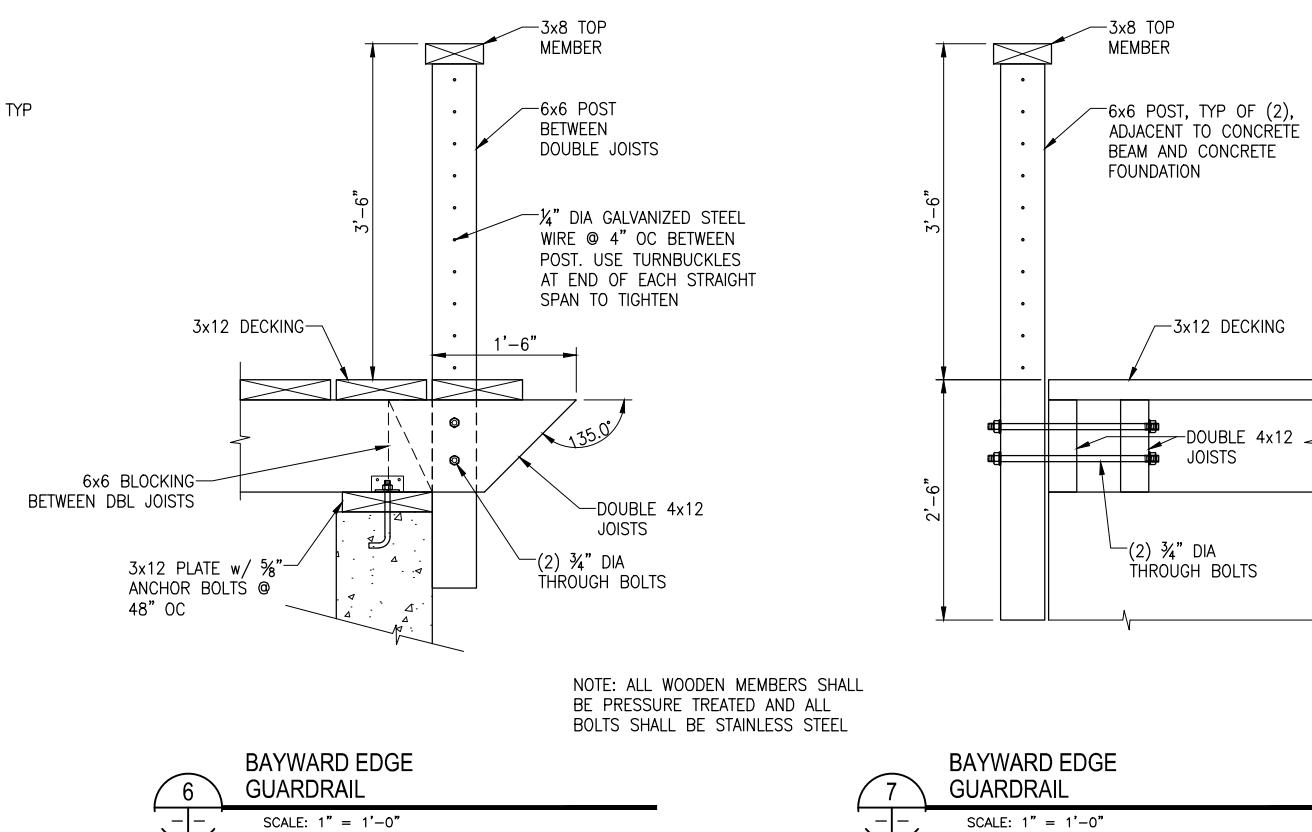
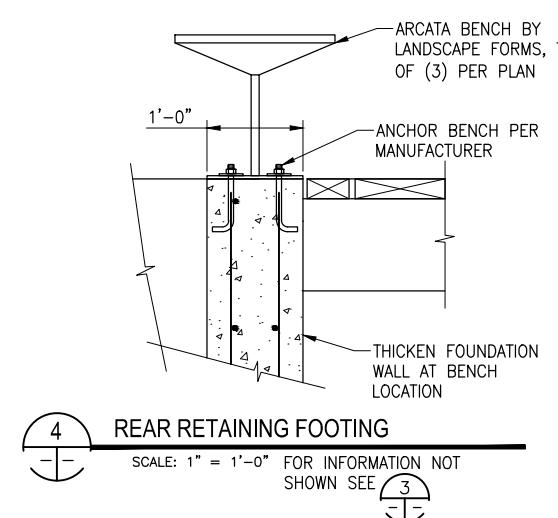
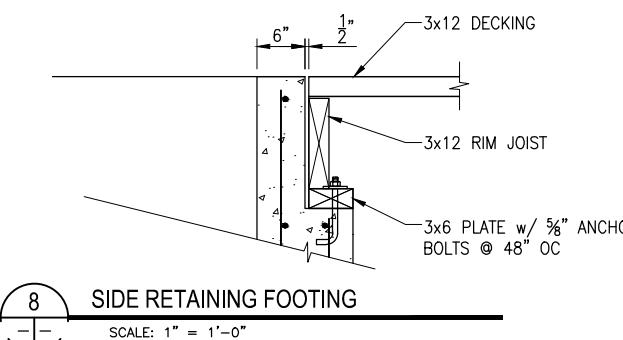
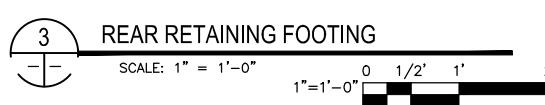
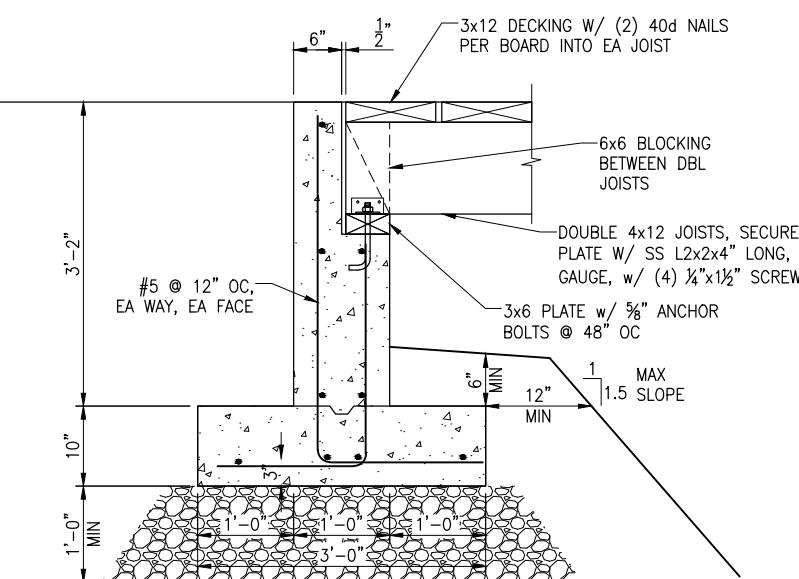
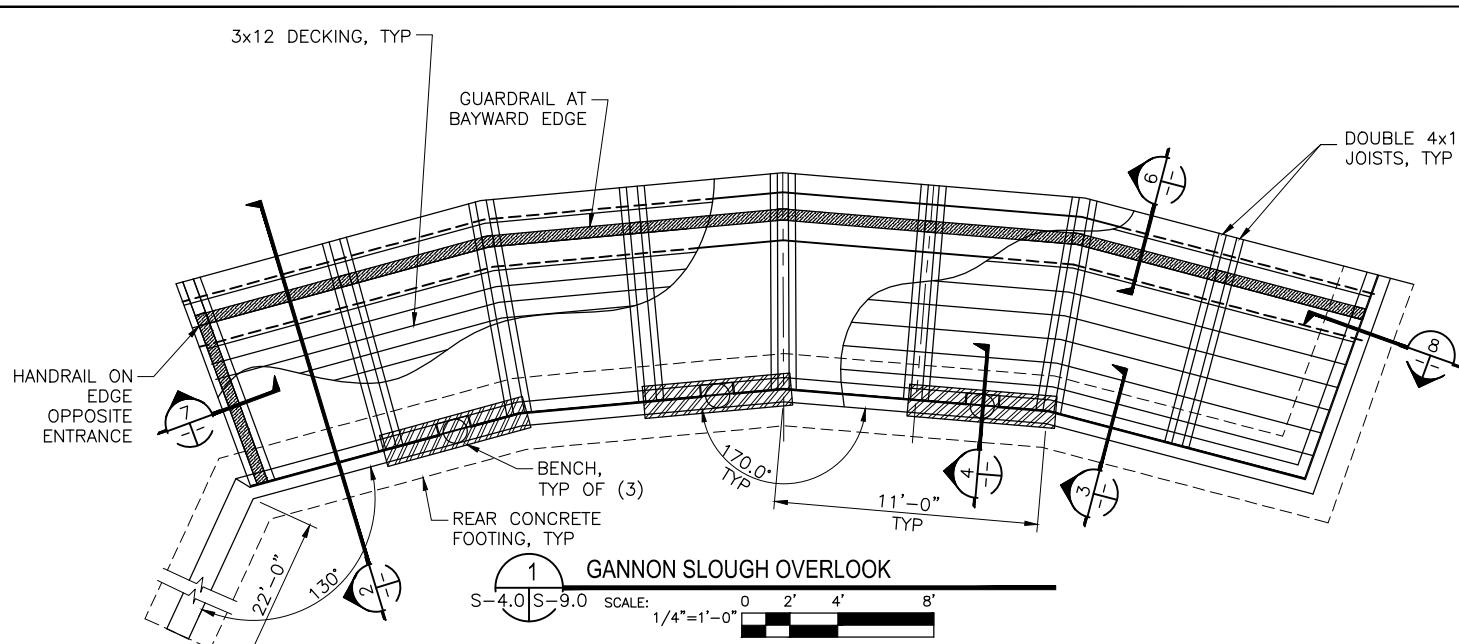
SCALE: NTS

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NOTE: ALL WOODEN MEMBERS SHALL BE PRESSURE TREATED AND ALL BOLTS SHALL BE STAINLESS STEEL

**CITY OF ARCAT
HUMBOLDT BAY TRAIL NORTH**

STRUCTURAL

GANNON SLOUGH OVERLOOK

PROJ NO: 8411982

FEET 52 OF 52